

# BEACH AND EDINGER CORRIDORS



## SPECIFIC PLAN





CITY OF HUNTINGTON BEACH, CALIFORNIA

# BEACH AND EDINGER CORRIDORS SPECIFIC PLAN

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LOOKING NORTH ON BEACH BOULEVARD FROM 150 FEET SOUTH OF TALBERT AVENUE C.1931



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i.1 PURPOSE

The *Beach and Edinger Corridors Specific Plan* is established to orchestrate private and public investment activities along the Beach Boulevard and Edinger Avenue Corridors, and to support and promote investment that will enhance the beauty and vitality of these, the City’s primary commercial corridors. This Specific Plan presents the community’s vision for the evolution and continued growth of the two corridors, and it establishes the primary means of regulating land use and development within the Specific Plan Area (see section i.3 - Specific Plan Area). Finally, the Plan contains a program of planned actions and investments that the community intends to implement to stimulate and complement private investment along the corridors.

i.2 AUTHORITY AND PROCEDURE

The Specific Plan is established by the Huntington Beach City Council in accordance with Chapter 215 of the *Huntington Beach Zoning & Subdivision Ordinance*, which establishes Specific Plans as an authorized mechanism for regulating land use and development in the City; and as enabled by the State of California Government Code Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457.

This Specific Plan implements the broad policies established in *The City of Huntington Beach General Plan* to guide growth and change along the Beach Boulevard and Edinger Avenue Corridors and is consistent with the General Plan. The Development Code contained within the *Specific Plan* replaces previous land use and development regulations contained within the *Huntington Beach Zoning and Subdivision Ordinance* for these portions of the City.

The specific plan document is based on community input resulting from six community meetings, several study sessions with City Council and the public hearing process with the Planning Commission and City Council. A specific plan may either be adopted by ordinance or resolution (Government Code Section 65507). Should the legislative body wish to change a proposed Specific Plan recommended by the Planning Commission, the change must first be referred back to the Commission for consideration, if not previously considered (Government Code Section 65504).

1) CEQA Compliance

Adoption or amendment of a Specific Plan constitutes a project under the California Environmental Quality Act (CEQA). If the initial environmental review shows that the proposed or amended plan could significantly affect the environment, the jurisdiction must prepare an environmental impact report (EIR).

Pursuant to CEQA, an Initial Study was prepared as part of the Specific Plan approval process. Based on the results of the Initial Study, the City of Huntington Beach determined that a Program EIR would be the appropriate level of environmental review for the Specific Plan. As part of the Specific Plan Program EIR a traffic study, noise study, air quality study and utilities analysis, including a Water Supply Assessment, was completed. The Program EIR also evaluated land use, aesthetics, biological resources, cultural resources, hydrology and water quality, public services, recreation and hazards and hazardous materials. Mitigation measures included as part of the Program EIR have been incorporated in Appendix D and will be included as requirements on development projects within the Specific Plan area.

All subsequent approvals necessary to develop property within the Specific Plan Area must be consistent with the Specific Plan, the Maximum Amount of New Development established in section 2.1.1 of this Specific Plan and be within the scope of the Program EIR. Additional environmental documentation may be required in the future if significant changes are found to have occurred pursuant to CEQA guidelines.

2) Specific Plan Amendments

Specific Plan Amendments shall be made through the Zoning Text Amendment process; subject to consideration and approval of the Planning Commission and City Council in accordance with the provisions of the Huntington Beach Zoning and Subdivision Ordinance.

3) Severability

If any section, subsection, sentence, clause, phrase, or portion of this title, or any future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court or competent jurisdiction, such decision shall not affect the validity of the remaining portions of this title, or any future amendments or additions hereto. The City hereby declares that it would have adopted these titles and each sentence, subsection, clause, phrase, or portion or any future amendments or additions thereto, irrespective of the fact that any one or more sections, subsections, clauses, phrases, portions or any future amendments or additions thereto may be declared invalid or unconstitutional.

### i.3 SPECIFIC PLAN AREA

The Specific Plan refers to all private and public properties that come under the purview of the Specific Plan as indicated in the Specific Plan Area Map presented in Figure i.1. The total acreage of the Area is approximately 459 acres. The Beach & Edinger Specific Plan Area is composed of portions of two primary development corridors, the Beach Boulevard Corridor, and the Edinger Avenue Corridor, as follows:

*The Edinger Corridor Plan Area.* The portion of the Specific Plan centering on Edinger Avenue extends westward from the Beach Boulevard intersection to the west of Goldenwest Street intersection. Development Standards contained in this Specific Plan will apply solely to properties formally located in the Specific Plan Area. The properties that will be included in that formal Specific Plan Area are illustrated in Figure i.1 - Specific Plan Area.

Notwithstanding these formal policy-area boundaries, the primary developments influencing the optimum potential future for all properties in this portion of the City are Bella Terra shopping center, Golden West College, Goldenwest Transit Center, and the properties adjacent to these three located between Edinger Avenue and Interstate 405. Although these properties are outside of the formal Beach and Edinger Specific Plan Area for regulatory purposes, the first section of this Specific Plan - Book I, Community Intent reflects the interconnectedness of the area. The Specific Plan refers to this cohesive portion of the City as *the Edinger Avenue Corridor and Environs*. The Edinger Corridor and Environs includes all properties lining Edinger Avenue between the intersection of Goldenwest Street and Beach Boulevard, as well as the triangle of properties formed by the eastern edge of Golden West College, Interstate 405, and Edinger Avenue – see Figure i.2 - The Edinger Avenue Corridor and Environs. For specific development regulations

pertaining to properties in the Edinger Avenue corridor and Environs that are outside of the Specific Plan area presented in Figure i.1, refer to *The Crossings at Huntington Beach Specific Plan* (SP13), as well as to the *Huntington Beach Zoning & Subdivision Ordinance*. Figure i.2 illustrates the location of properties that are regulated by *The Crossings at Huntington Beach Specific Plan*.

*The Beach Boulevard Corridor Plan Area.* The Beach Boulevard Corridor portion of this Specific Plan extends from Edinger Avenue to the north, to the northern edge of the Coastal Zone, which is located south of Atlanta Avenue (see Figure i.1 – Specific Plan Area).

### i.4 DOCUMENT ORGANIZATION

The *Beach and Edinger Corridors Specific Plan* is organized into three mutually reinforcing “Books” or primary sections,” as follows:

**Book I: Community Intent** describes the community objectives that the Specific Plan is intended to achieve and the primary means by which the community intends to support the emergence of those desired outcomes.

**Book II: Development Code** establishes the primary means of regulating land use and development on properties located within the Plan Area.

**Book III: Public Improvements** describes the planned investment of City resources and infrastructure needs to stimulate, promote and support the desired growth and change in the Plan Area.

In addition to these three primary sections, additional information upon which the Specific Plan is founded is located in the Beach and Edinger Corridors Reference Volume.

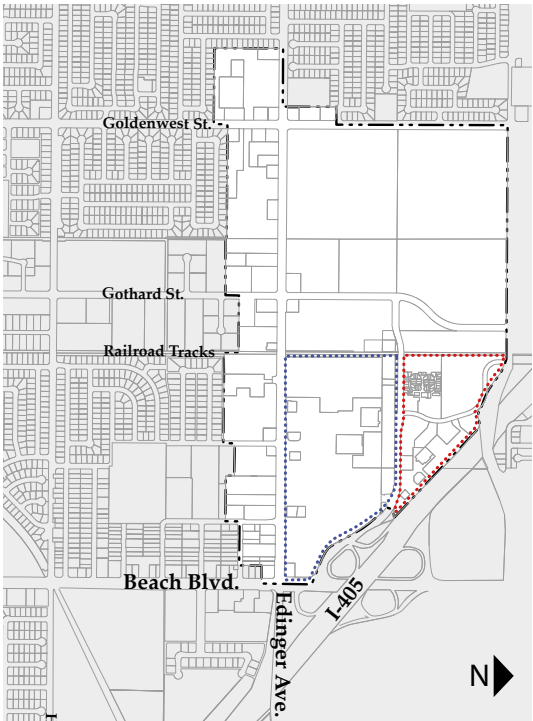


FIG.i.2. EDINGER AVENUE CORRIDOR AND ENVIRONS

- • • Edinger Avenue Corridor and Environs Boundary
- • • • • The Crossings Specific Plan Area Boundary
- • • • • North Huntington Center Specific Plan Area Boundary

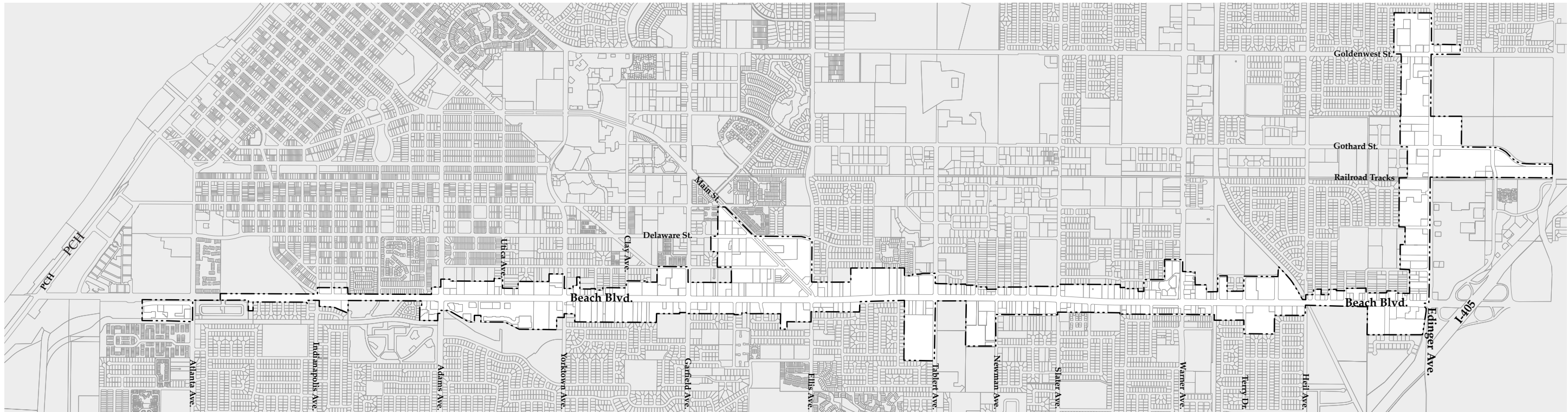


FIG.i.1. PLAN AREA MAP

- — — Coastal Zone Boundary
  - • • Specific Plan Area Boundary
- N 1,000 500 0 1,000 2,000 Feet



This Specific Plan establishes a planning and design framework to enhance the economic performance, functionality and beauty of the Beach and Edinger Corridors in accordance with the forces of the free market and the community’s vision for its primary commercial corridors. This first of the three “Books” that contain the primary contents of this Specific Plan, describes the community objectives that the Plan is intended to achieve as new investment creates change in the Plan Area and its environs. This section of the Specific Plan also describes the overall development concept to instigate new investment and to guide the form of that investment to the mutual benefit of private and public interests. These intended outcomes and revitalization strategies form the basis of the regulations and planned public actions contained in the two subsequent Books of this Plan. By making the community’s intent clear, Book I is also intended to provide supplementary guidance for instances or opportunities not specifically covered by the development regulations or public improvements contained in Books II and III.

## 1.1 COMMUNITY OBJECTIVES

**Enhance the overall economic performance, physical beauty and functionality of the Corridors. More specifically:**

- 1) Orchestrate new public and private investment toward the establishment of a more lasting framework for growth and development - a framework of clearly defined districts, centers, street patterns, and local architectural and landscape identity - upon which new development can reliably respond to, build upon and draw value from.
- 2) Re-position disinvested corridor properties to capture value in the contemporary marketplace.
- 3) Begin the transformation of the visual character of Beach Boulevard from “anywhere strip” to its proper role as the iconic gateway to and from the beach, and as the city’s most visible north-south thoroughfare.
- 4) Promote new investment that supports the growth and success of Bella Terra and Golden West College
- 5) Build on the presence of Golden West College, Bella Terra, and the existing transit infrastructure to instigate the emergence of a vital and attractive urban district characterized by a synergistic mix of students, customers, residents, pedestrians, transit-riders, office workers, and visitors.
- 6) Instigate the development of a network of pedestrian-oriented streets, promenades and other public open spaces that encourage walking, and ultimately, walking in combination with transit ridership.
- 7) Enhance pedestrian, bicycle and vehicular connections between Golden West College, Bella Terra, Golden West Transportation Center, and development along Edinger Avenue.
- 8) Balance mobility and community development objectives: enable continued market-driven growth and development while 1) maintaining minimum community mobility standards, and 2) furthering patterns of land use and development that contribute toward long-term regional mobility and livability.
- 9) Make the most of each increment of new development to build toward a more environmentally sustainable future city and region.
- 10) Insure that new buildings and landscaping contribute to the emergence of an increasingly visible and memorable visual identity appropriate to the unique history and character of the City.
- 11) Incorporate Crime Prevention Through Environmental Design (CPTED) guidelines into designs to reduce the incidence and fear of crime and create an environment to maximize public safety.

## 1.2 STARTING POINT: SUMMARY OF PLAN INPUTS

The primary conditioning factors in the determination of the optimum achievable future for the Corridors are the physical and economic conditions present at the inception of the Plan. More specifically, the development concept and revitalization strategy described in the subsequent sections of Book I, and the regulations and public improvements that follow in Books II and III to implement the community’s intent for the corridors, are conditioned by the following factors: Existing Pattern of Development, Market Trends, Infrastructure Capacity, Principles of Sustainability, and Established Principles of Good Place-Making (see Figure 1.1 – Conditioning Factors of the Specific Plan).

Existing conditions along the Beach Boulevard and Edinger Avenue Corridors at the time of adoption of this Specific Plan are detailed in the Beach and Edinger Corridors Reference volume, in combination with separately bound technical reports. Ultimately, the implementation of the planning framework contained herein will result in cumulative modifications to these conditions. As change occurs, the community intends to monitor changes in the Specific Plan Area and amend the Specific Plan document the degree to which it remains sufficiently current.

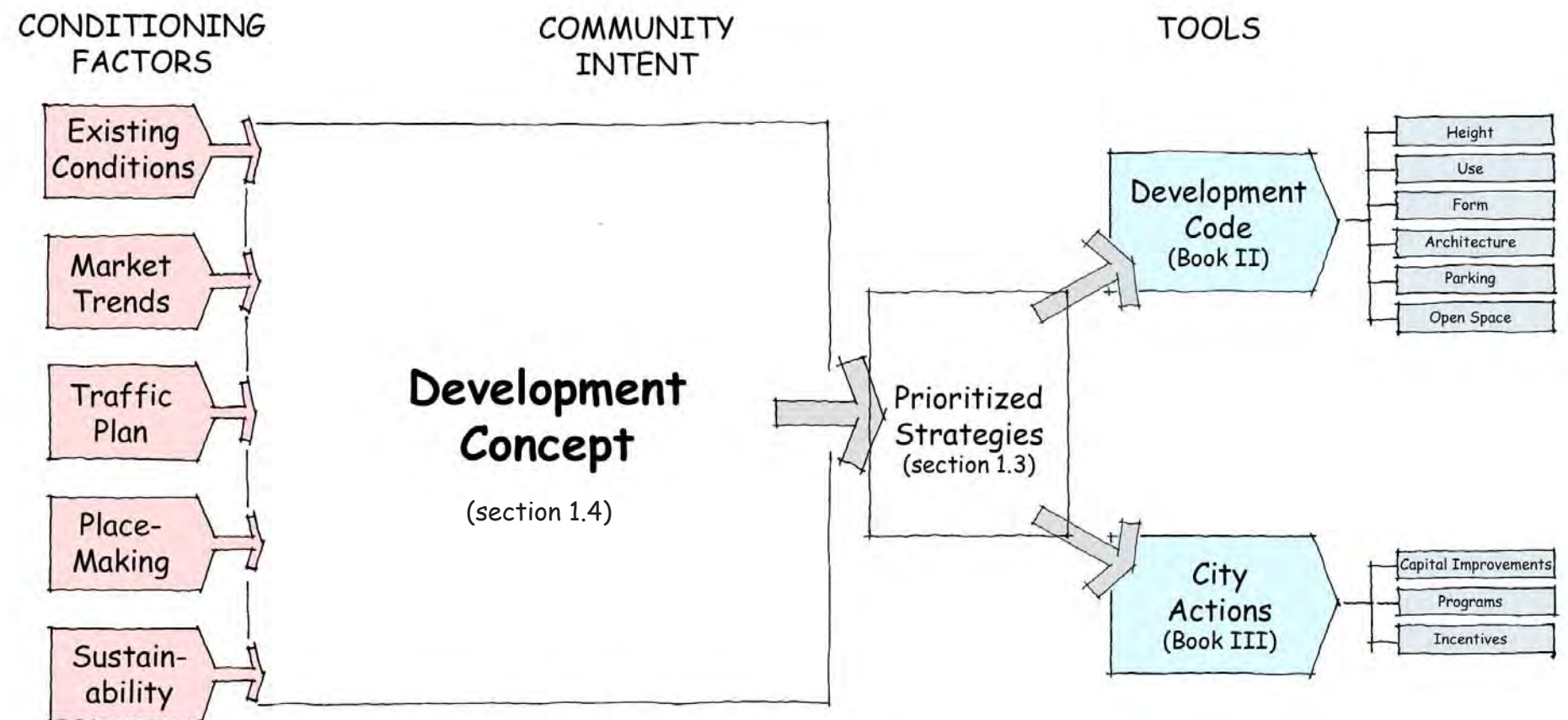


FIG. 1.1. CONDITIONING FACTORS OF THE SPECIFIC PLAN



# 1.3 REVITALIZATION STRATEGY

To orchestrate growth and change in the Plan Area in keeping with the community’s vision for the two corridors, the City leadership intends to promote and guide new investment and change by employing municipal policies and resources strategically. Keeping in mind that strategy must always remain sufficiently nimble to respond to unexpected opportunities and to make best use of resources as they become available, the strategic priorities that the City leadership intends to pursue are the following:

## 1) Harness Market Demand:

Realign development policies and planned public investments to capitalize on the primary market trends most likely to condition the types and formats of new investment in the Plan Area, which are the following:

- i) The shopping industry’s accelerating abandonment of exclusively auto-oriented shopping centers in favor of more open air and amenity-driven formats clustered at primary crossroads;
- ii) Rapid population growth and the associated growth in demand particularly for housing, (and the associated escalation in the value of property) in the built-out portions of Orange County;
- iii) Rapidly growing demand for walkable urban environments for living, working and shopping, in close proximity to existing or planned transit service;
- iv) Increasing regional commitment to transit, driven by traffic congestion, concern about global warming, and escalating gasoline prices.

## 2) Kick off Revitalization by Enabling Most Promising Opportunities First.

Place highest priority on leveraging the large-scale assembled vacant properties located between Bella Terra and Golden West College to: 1) kick off the revitalization of the corridor and its environs; 2) to bolster and enhance the success of Bella Terra; 3) to stimulate pioneering residential development in a sufficiently large increment to establish the beginnings of a unique new urban neighborhood; and 4) to exemplify the desired character and scale of the “Town Center District” area.

## 3) Make the Most of Value Already In Place: Attune Policies and Strategic Actions to Existing Conditions:

Implement a variable approach to corridor revitalization (see Figure 1.2 - A Variable Approach to Corridor Revitalization)

- i) **Restructuring** - In areas with severe disinvestment, vacancy and/or dominant redevelopment opportunities, establish policies that support fairly sweeping corridor restructuring.
- ii) **Transition** - In areas with some disinvestment and a moderate level of opportunity, establish policies to encourage a transition to the most viable future restructured condition, but that support the retention of existing value already in place.
- iii) **Preserve** - In areas with little current disinvestment, establish policies that primarily preserve existing value

## 4) Promote A Range of New Housing Choices.

Promote the addition of new housing in a wide variety of formats and densities to support corridor retail and services. Along Edinger Avenue in particular, promote the development of dense, high quality housing formats that are responsive to college-related housing demand, that position customers close in to Bella Terra and nearby corridor retail and services, and that provide the key ingredient to the creation of a more round-the-clock activity center in this evolving portion of the City.

## 5) Support the Continued Presence and Expansion of Auto-Dealerships along Beach Boulevard.

- i) Permit existing auto dealerships to expand anywhere on the corridor.
- ii) Encourage new auto sales dealership development along segments closer to 405, particularly north of Five Points.
- iii) Work with existing auto dealerships to investigate the potential for off-site overflow fleet storage, and to support the retention, expansion and eventual clustering of the auto dealership offerings along Beach Boulevard.

## 6) Integrate Interdependent Specific Plan Areas to Achieve of a Unified, Mutually Beneficial Community Vision.

Coordinate land use policies contained in the *Beach-Edinger Corridors Specific Plan* and the *Bella Terra II Specific Plan* to implement a singular community vision and market-focused strategy for new investment and city benefit. Subsume the *Pacifica Community Plan* into this Specific Plan as well.

## 7) Leverage Golden West College and Transportation Center.

Leverage the proximity of Golden West College as well as the Golden West Transit Center and the railroad tracks and right-of-way to stimulate investment in high density residential development in a pattern that builds on the greater propensity of student populations to walk and take transit when provided the opportunity to do so.

## 8) Implement “Center and Segment” Policies and Strategies.

Assist the transition from linear strip to a pattern of centers and segments, each with enhanced visual character and market focus, and with supportive (and differentiated) land use and development policies.

## 9) Relieve the Monotony; Enhance Corridor Identity

Promote a varied visual structure along Beach Boulevard between I-405 and the beach. Focus capital improvements in coordination with Caltrans and new construction on fostering the emergence of greater structural differentiation that is immediately obvious to motorists and pedestrians, and that enhances ease of orientation as well as the visual character and appeal of the long corridor.

## 10) Provide an investment-friendly planning environment

- i) Streamline the development process by 1) providing clarified development standards that spell out municipal requirements in sufficient detail to take the guesswork out of proposal preparation; and by 2) simplifying the development review process for proposals that adhere to Specific Plan standards and guidelines.
- ii) Employ municipal development policy tools to provide a reliable environment for investors to invest in a mixed use environment that provides enhanced clarity and reliability as to the character of future adjacent development.

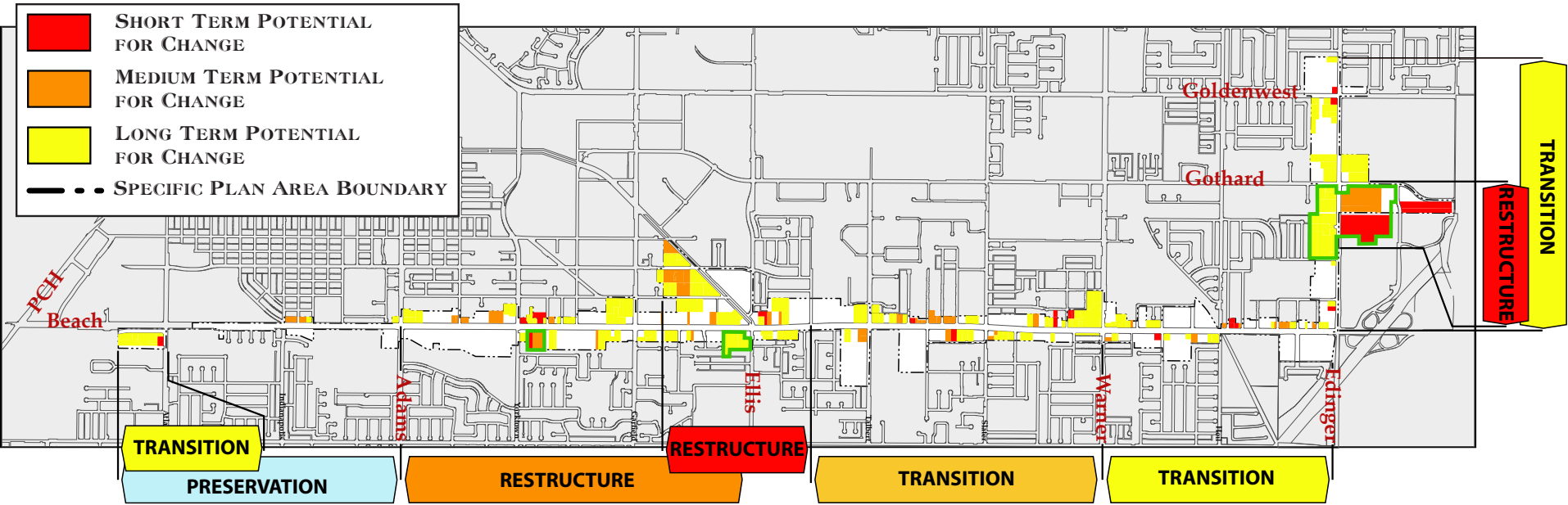


FIG. 1.2. A VARIABLE APPROACH TO CORRIDOR REVITALIZATION

## 1.4 THE DEVELOPMENT CONCEPT

The Beach and Edinger Corridors are composed of 550 individual privately held properties, and six and a half miles of public rights-of-way owned by the City of Huntington Beach (in the case of Edinger Avenue) and the State of California (in the case of Beach Boulevard). The overarching purpose of the Specific Plan is to orchestrate individual public and private investments to produce greater value than any separate project could practically achieve by providing a common purpose that all investors can rely upon, contribute to, and derive value from. This section describes the common purpose to which all investments shall be directed: the realization of a vision of the future that is sufficiently specific to provide a common purpose, yet loose enough to respond to opportunities and changes in the marketplace that will inevitably arise.

During the period of time in which development is guided by this Specific Plan, the Corridors will begin their transformation from commercial strip to a pattern of Centers and Segments (see Figure 1.3 developed during the community workshop visioning process). Whereas the commercial strip is undifferentiated – a linear pattern of exclusively commercial buildings, typically low-rise (with some very visible exceptions), surface parking lots, and monument signs, the future corridor will be increasingly characterized by an emerging structural differentiation: there will be clusters of shops, activity, mix, and intensity - *Centers*; and there will be longer linear portions distinguished by cohesive building types, frontage landscaping, and emerging market focus - *Segments*. Whereas the commercial strip caters to a narrow segment of market demand (the demand for motorist-oriented commercial goods and services), the emerging Centers and Segments will have a much wider appeal. *Town Centers* will offer community services as well as commercial goods and services that cater to the entire City in a setting of comfortable walking streets and transit service, amidst a mix of homes, offices and hotel rooms above the lively ground level shopping and entertainment venues; *Neighborhood Centers* will specialize in serving the needs of neighborhoods within a short drive; mixed-use *Neighborhood Boulevard* and *Town Center Boulevard* segments will focus on a synergistic mix of workplace, commercial and high density residential uses. Infill development on underutilized properties responding to the broad framework of the Specific Plan will contribute to an emerging pattern of coherent arrangements of buildings, streets, and blocks that were formerly lacking.

New public and private investment will complement many of the offerings that were already in place prior to the adoption of the plan. New shops and entertainment venues will add to the overall draw of the corridors, new and expanded auto dealerships will widen the already wide range of auto sales venues on offer, new residents will bring a larger “captive audience” to augment the drive-by traffic, and new street improvements will enhance the walking and driving environment as well as the visual identity of the corridors.

The implementation of this Specific Plan is intended to begin this transformation from the linear commercial strip that has fallen out of favor with market trends to a pattern of centers, boulevard and parkway segments more in keeping with contemporary consumer and investor preferences and with value already in place along the corridors. The particular characteristics envisioned for each of the Centers and Segments that will define the revitalized corridor are as outlined in the following sections.

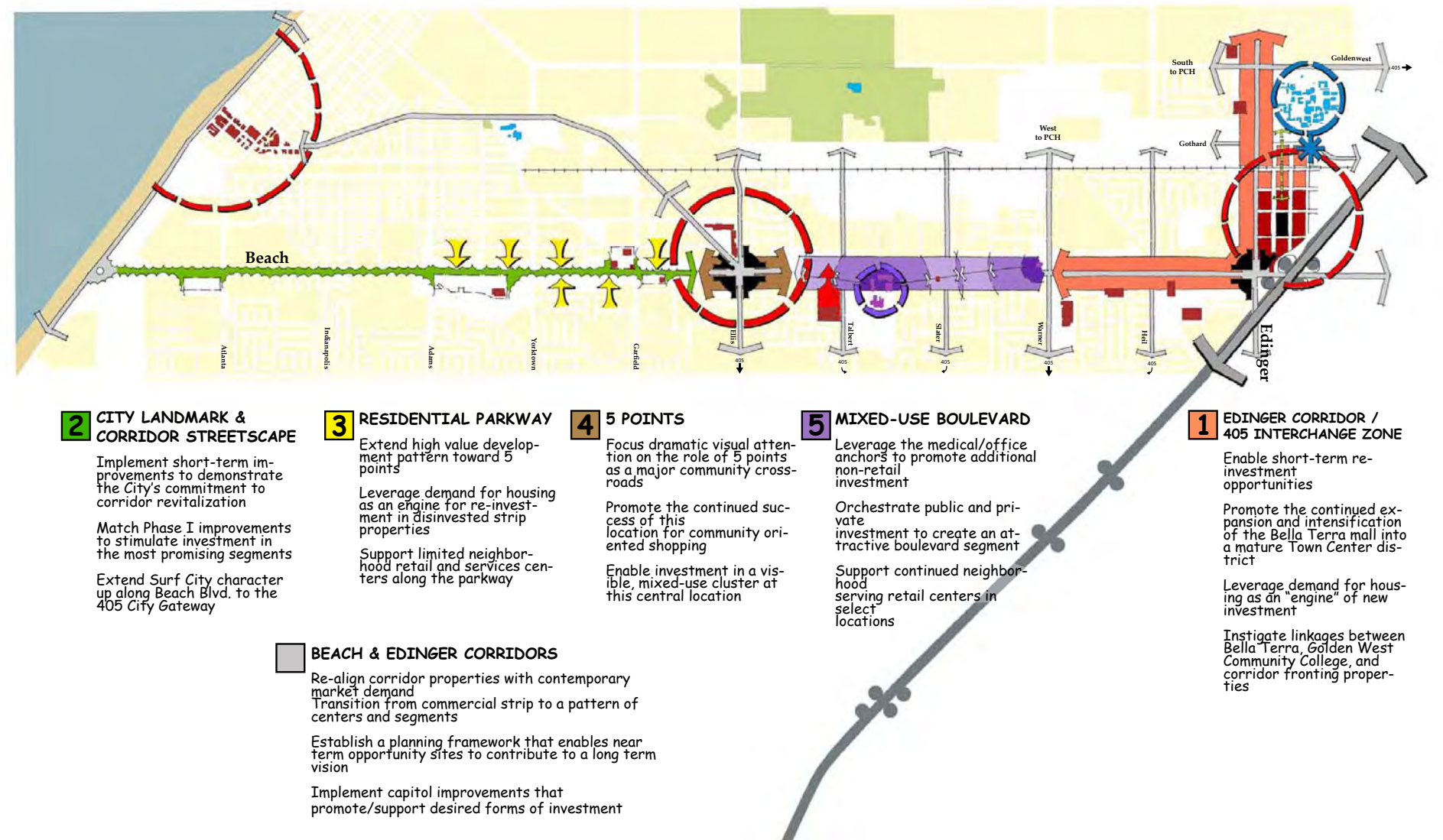


FIG. 1.3. CONCEPTUAL REVITALIZATION & URBAN DESIGN FRAMEWORK



### 1.4.1 BEACH BOULEVARD CORRIDOR

Along Beach Boulevard, the proposed Specific Plan would be divided into five general geographic transition areas (also referred to as segments): Residential Parkway, Neighborhood Parkway, Five Points District, Neighborhood Boulevard, and the Town Center Boulevard. These segments are shown in Figure 2.1 Corridor Centers and Segments Map, and described below. In addition, a separate land use designation (neighborhood centers) is identified on specific parcels throughout three of the segments.

#### 1) Neighborhood Centers:

Neighborhood Centers are existing shopping centers that would retain their overall function and encourage eventual transition of the properties to achieve greater land use efficiency and a mixture of complementary uses. To encourage eventual maximum efficiency of land use and a less exclusively auto-oriented type of development, the Specific Plan allows for intensification with upper level housing, office, and/or lodging. New development on these sites may continue to provide exclusively shopping center uses with surface parking, or may also feature a wider mixture of uses and structured parking. In either case, ground level uses would continue to provide convenient neighborhood-serving retail uses such as supermarkets and specialty grocery stores, pharmacies and banks, as well as small-scale restaurants and cafes, personal and business services. Upper level uses may include apartments, condominiums, offices, or hotel rooms.

Neighborhood Centers are identified within three segments, including (1) Residential Parkway, (2) Neighborhood Parkway, and (3) Neighborhood Boulevard. The Neighborhood Centers would be distinguished from other uses in the various segments by their enhanced visibility from the roadway. A portion of the new buildings would be built up to the back of the sidewalk, and the sidewalk would extend to the curb. Landscaping improvements would extend from the right-of-way line to the existing back-of-curb.

#### 2) Residential Parkway Segment:

In the southern-most portion of the Specific Plan, this area is located along Beach Boulevard between Adams Avenue to the southern Specific Plan boundary. The general planning approach to this particular area is preservation, as the majority of development along this segment is composed of existing residential uses. Infill and replacement development would be directed by plan regulations to primarily replicate and very subtly improve upon the best features of the existing pattern, including the buffering of residential uses from the street and increased landscaping. One Neighborhood Center is located in this segment, which is on the southeast corner of Beach Boulevard and Atlanta Avenue.

#### 3) Neighborhood Parkway Segment:

Transitioning north along Beach Boulevard, this segment is located between Adams Avenue and the Five Points Center (south of Ellis Avenue). Due to the aging commercial strip development that characterizes this segment, the overall planning approach for this area includes restructuring and revitalization. A wider range of land uses would be permitted for this segment compared to the primarily residential segment to the south (e.g., the Residential Parkway segment). It is envisioned that strip development would gradually be replaced by primarily residential development oriented away from Beach Boulevard and toward perpendicular side streets. In addition to residential development, office, lodging, and neighborhood-serving retail would also be permitted. In order to mimic the imagery of the Residential

Parkway to the south, all development would be required to provide front setbacks and landscaping, as well as streetscape improvements.

This segment also includes two designated Neighborhood Centers. One occupies the majority of the eastern frontage of Beach Boulevard between Adams and Yorktown Avenues, and the other occupies the southwest quadrant of the intersection of Beach Boulevard and Garfield Avenue.

#### 4) Five Points District Segment:

The Five Points area occupies the halfway point between the beachfront and I 405, and is organized around the confluence of Beach Boulevard and Main Street/Ellis Avenue. It contains the 160,000 sf Five Points Shopping Center, which includes a mix of national and regional retailers. The area south of Main Street is characterized by a more diverse mix of uses (e.g., office, medical services, multi-family, and senior housing) and building types. The planning approach to this area is two-fold: (1) retain the Five Points community retail center and support its eventual intensification and mix and (2) encourage the restructuring and revitalization of surrounding areas to enhance market focus and district appeal.

This area of the Specific Plan would be designated as a potential City center characterized by convenience and urban vitality. This area is envisioned to have greater development intensity than surrounding segments. The greatest development intensities would be provided in the core retail area. Infill development on underutilized properties would be composed of the types of coherent arrangements of building, streets, and blocks that are presently lacking in this centrally located district. New apartments, condominiums, and professional and medical office buildings would face public sidewalks with lobby entrances, shop fronts, and attractively detailed facades. Parking would be screened from view along primary street frontages. Buildings would be taller and development more compact in this segment compared to other City neighborhoods, providing the intensity and activity expected in a vibrant urban district.

#### 5) Neighborhood Boulevard Segment:

This segment along Beach Boulevard is generally located between the Five Points Center and Warner Avenue. The area is characterized by a significant amount of ageing commercial strip development, including auto-oriented uses, such as car dealerships. Development types are dominated by one-story, single-loaded commercial buildings surrounded by surface parking lots with little landscaping or pedestrian amenities. The planning approach to this area is to facilitate long-term transition from strip retail to uses more focused on nearby populations, featuring development types that retain visibility to motorists on Beach Boulevard, while providing a more visually attractive and comfortable pedestrian environment. Neighborhood-serving and hospital-serving retail and services, corner/crossroads located retail, and office and office-medical would be encouraged to take advantage of the proximity to the Huntington Beach Hospital and its related cluster of medical services. Infill residential uses would also be permitted throughout this segment. Landscape improvements would be required by new developments that extend from the right-of-way line to the existing back-of-curb.

As new investment replaces ageing strip development, the exclusively auto-oriented and zero-amenity pattern (i.e., land uses that people exclusively drive to, such as grocery stores) would be replaced by development oriented both to motorists and pedestrians. A portion of new buildings would be built close to the sidewalk with doors and windows opening out toward Beach Boulevard. Planting strips between the curb and sidewalk would help buffer pedestrians from traffic moving along the thoroughfare.

This segment also includes six neighborhood centers. The first three are located at the intersection of Beach Boulevard and Talbert Avenue (the northeast corner is not included in the project boundaries). Two more are located at the southern corners at the intersection of Beach Boulevard and Slater Avenue. The last is located at the southwest corner of Beach Boulevard and Warner Avenue.

#### 6) Town Center Boulevard Segment:

The Town Center Boulevard segment encompasses uses along Beach Boulevard from Warner Avenue to Edinger Avenue. Within this segment, Beach Boulevard is primarily characterized by small and shallow properties that currently limit redevelopment potential. The planning approach for this area is to encourage gradual transition to a more pedestrian-oriented development using distinctive building types and site treatments.

A wide range of City-oriented retail and service uses would be supported. New investment would be directed away from purely auto-oriented formats towards formats more supportive of pedestrian activity. While auto dealerships are encouraged to remain and expand, future development adjacent to dealerships would promote the addition of housing and supporting retail and services to create a more walkable environment. Entitlements would be provided for mixed-use development to encourage a gradual increase in land use efficiency. New development would be required to contribute to the emergence of a more attractive street environment by extending new and required improvements to the existing back-of-curb.

Physically, a greater proportion of buildings would be positioned closer to the street. The transformation of the Beach Boulevard portion of this segment would be supported by frontage improvements that include planting strips between the curb and sidewalk to help buffer pedestrians from traffic. Decorative boulevard-scale streetlights would replace utilitarian poles, which would contribute to an increasingly vivid corridor identity without restricting visibility of shop fronts. Modest setbacks between the buildings and sidewalk would contribute to the planting areas designed to soften and add color to the existing unbroken expanse of concrete and asphalt.

### 1.4.2 EDINGER AVENUE CORRIDOR

Edinger Avenue Corridor generally represents a continuation of the Town Center Boulevard Segment. The consistency and stability of commercial uses along this segment is similar to the Town Center Boulevard segment of Beach Boulevard, which is equally proximate to the I 405 interchange. But whereas the Beach Boulevard segment is characterized by small and shallow property configurations that limit redevelopment potential, the Edinger Avenue Corridor segment features a wide range of large and medium scale properties. The larger scale of the properties combined with their proximity to two important destinations—Bella Terra Mall and Golden West College—is likely to result in ongoing investment and intensification of this portion of the City.

New infill development on properties lining Edinger Avenue would be directed toward the augmentation of the existing pattern of isolated, low-intensity, single use, surface-parked development, with development that provides added connectivity, synergy, and mix. New uses would generally feature a mixture of ground-level shops and services, with upper-level homes, offices, or hotel rooms. An increasing number of buildings would feature multiple levels, incrementally adding spatial definition and reducing the apparent width of the thoroughfare. The future development pattern would make walking and bicycling viable choices and would ultimately make transit riding an appealing alternative to automobile use.



The area is intended to become a central City district characterized by emerging structural differentiation, vitality, and activity.

Bella Terra, while not included within the Specific Plan boundaries, will continue to grow in terms of retail offerings, mixture of uses, and intensity of development, ultimately evolving from a stand-alone shopping mall to become the core of a vital City district. It is envisioned that infill development on properties between Golden West College and the existing Bella Terra Mall, which is considered the Town Center Core, would form a new Town Center Neighborhood. The recently approved Village at Bella Terra (also not part of the proposed Specific Plan) would contribute to the infill envisioned for the Town Center Neighborhood. The Town Center Neighborhood, which is part of the Town Center District along with the Town Center Boulevard segment, would provide housing, office and lodging, while simultaneously providing a market to help support the Town Center Core shops and services. The Town Center Boulevard segment would include new development lining Edinger Avenue, which would ultimately transform the character of the corridor from a commercial strip to a Town Center Boulevard, with development increasingly reflecting the vitality and pedestrian friendly qualities of this central district. Together, the Town Center Core, Town Center Neighborhood, and Town Center Boulevard are the primary structural elements envisioned for Edinger Avenue.

### 1) Town Center District:

Edinger Avenue is generally referred to within the Specific Plan as the Town Center District. More specifically, the Town Center District collectively refers to Golden West College, which is not included within the Specific Plan boundaries, the new neighborhoods that are envisioned along Edinger Avenue, the Goldenwest Transit Center, and the shopping and entertaining core (i.e., Bella Terra, which is also not part of the Specific Plan). Overall, the district would be compact and clustered with a variety of land uses including retail, entertainment, civic, residential, office, and lodging. Walking and bicycling would be made more convenient through the district’s mixture of uses and concentrated development pattern. Along the street network, attractive mixed-use structures would be built right up to the sidewalk, forming a consistent “street wall.”

Although surrounding land uses are an integral part of the concept for overall description of the Town Center District, the following discussion provides information on the particular land uses changes that would be governed by implementation of the Specific Plan. Therefore, the Town Center Core (Bella Terra) as well as the recently approved Village at Bella Terra (both of which are contained in The Bella Terra Specific Plan [SP13]) are not included within this discussion.

### 2) Town Center Boulevard:

The Town Center Boulevard segment generally covers the properties located along Edinger Avenue itself (excepting the Town Center Neighborhood and Core Edge parcels described below). New development would be configured in a pattern that would make walking a viable option and would also accommodate a wider range of uses.

Physically, a greater proportion of buildings would be positioned between the street and parking facilities (or on top of some of the parking facilities), re-uniting activity-generating uses with public sidewalks. New development on properties lining Edinger Avenue would typically feature a mixture of ground-level shops and services, with upper-level homes, offices, or hotel rooms. As one travels out and away from the Town Center Core, development would become less compact: the spaces between buildings would expand, and the proportion of the buildings built

up to the sidewalk edge would decrease. The buildings may even become more low-rise, providing a transition from the district’s most urban core to its less urban edges. Over time, the amount of ground-level retail may likewise increase as one travels toward the Town Center Core and decrease in the other direction.

The transformation of the Edinger Corridor would be supported by frontage improvements, converting the existing large expanses of pavement into deep, tree-lined landscaping or protected parking access lanes between Goldenwest Avenue and Parkside Lane. Each block of new development within this area of the Specific Plan would feature a unique protected parking access lane with slow moving traffic and amenities that buffer the sidewalk from the central fast-moving center of Edinger Avenue. This Main Street-like environment along Edinger Avenue would also create a comfortable “buffer zone” for upper stories of housing, lodging, and/or offices.

With the wide thoroughfare in the center accommodating relatively high traffic volumes, the sidewalks buffered by rows of street trees and curbside parking and the cohesive building types oriented toward the street, the intended vision of the proposed Specific Plan would gradually create an identifiable civic boulevard. The Town Center Boulevard would provide the central spine and iconic identity of the emergent Town Center District.

### 3) Town Center Neighborhood:

Overall, the Town Center Neighborhood is envisioned to encompass the areas surrounding the Town Center Core to the west and north; those areas that are included within the Specific Plan boundaries specifically include the parcels north of the Town Center Core Edge along Edinger Avenue, between Gothard Street and the Union Pacific Railroad right-of-way (UPRR ROW). Existing uses within this area include the Goldenwest Transit Center and the vacant Levitz site.

The Amstar/Red Oak Project (formerly known as The Ripcurl Project) has been approved for a 385-unit mixed-use project. The site is currently occupied by an approximately 60,000 sq. ft. shopping/office center. Near-term development activities would take advantage of the large areas of vacant and underutilized land in this area to provide the investment opportunities that would begin the formation of the urban neighborhood surrounding and supporting the Town Center Core. This neighborhood would feature the City’s widest range of contemporary housing types and possibly a wide mixture of uses, all concentrated within walking distance of the Town Center Core’s theater, shops, restaurants, cafes, nightlife, and amenities. As infill proceeds and the region continues to invest in transit infrastructure, the neighborhood would benefit from the presence of the Goldenwest Transit Center. The vitality and identity of the neighborhood would primarily stem from the new development pattern. Buildings would be built close to the sidewalks with entrances facing the public thoroughfares. Streets and pedestrian ways would provide connectivity between the college, the shopping core, and the Goldenwest Transit Center. The pattern of pedestrian-scaled blocks that would be created by these streets and ways would be distinguished by the public spaces distributed among them. Building massing and façade composition would emphasize variety and street-side interest.

For comparison, the Town Center Neighborhood designation on these particular parcels would have the same development standards as those envisioned for the neighborhood areas that surround the core retail area in the Five Points District, as described in Section 1.4.1 Beach Boulevard Corridor.

### 4) Town Center Core Edge:

The Town Center Core Edge includes the linear portion at the edge of the Town Center Neighborhood along Edinger Avenue (between Gothard Street and the recently approved Village at Bella Terra immediately east of the UPRR ROW). New development would feature ground-level retail, restaurant, and entertainment uses contiguous with those planned for The Village at Bella Terra.

### 5) Residential Transition Areas:

New investment on large properties along Edinger Avenue would be configured to provide a significantly improved transition to the residential development that it abuts to the south. Where residential development to the south is characterized by rear lot conditions, new development would “complete the block,” matching rear lot to rear lot. New residential streets would be constructed to knit new development back into the fabric of existing neighborhoods and to provide an appropriate transition to denser housing or commercial development lining Edinger Avenue.

New streets required for new development along large properties would help establish an expanded medium-sized block and street network parallel to Edinger Avenue. New streets would provide the proper means by which portions of deep parcels currently occupied by low-intensity development could infill with a mix of office and medium density housing. This infill development would assist with the improvement of the transition from Edinger Avenue’s mixed-use environment to the existing housing beyond.

### 6) Connections to Golden West College & Goldenwest Transit Center

As the Town Center District grows, it would provide connections to and from Golden West College and the Goldenwest Transit Center. Improved pedestrian connections created by the Town Center Neighborhood development would allow the student and faculty population to benefit from the variety of retail and entertainment in the nearby Town Center Core. Visitors throughout the City and region could utilize the Transit Center’s central location to arrive by bus and/or train to patronize the Town Center Core. Likewise, transit would provide Town Center residents and students with the opportunity to travel to other workplace or activity centers without relying on their car.

### 7) Connection between Town Center Neighborhood and Village at Bella Terra

As a supplement to an on-site pedestrian walkway system, potential future pedestrian and bicycle access such as an at-grade crossing or an above-ground crossing shall be pursued across the existing rail line between the former Levitz and Montgomery Ward (Village at Bella Terra) sites. Funding mechanisms, including a fair share analysis for the Edinger Corridors, shall be pursued by the City of Huntington Beach in conjunction with affected property owners.

2.0 ORIENTATION

This section contains the Development Code that will govern all future private development actions in the Beach Boulevard/Edinger Avenue Corridors. The Development Code is used to evaluate development projects or improvement plans proposed for properties within the Plan Area (see Fig. 2.1. Corridor Centers and Segments Map).

2.0.1 Applicability

The policies contained within this chapter shall apply as follows:

1) New Construction, Additions and Exterior Renovations

- i) The policies contained within this section shall apply to new construction, as well as to significant additions (addition greater than 20% of the buildings floor area) or exterior renovations to existing structures, as further specified below.
- ii) Where significant additions are made to existing buildings, requirements for renovation or enlargements shall apply only to new floor area.
- iii) Where exterior improvements or renovations (not additions or replacement) are made to existing buildings, architectural and sign regulations shall apply to that portion of the building being renovated. No other Specific Plan requirements shall be required.
- iv) Improvements and additions to existing buildings that increase nonconformities are not permitted.

2) Use Changes

- i) The policies contained within this section shall apply to new land uses proposed for existing facilities.

3) Right of Continued Use

- i) Nothing contained in this section shall require any change in any existing building or structure for which an entitlement application has been previously issued, or in any proposed building or structure for which a building permit application was deemed complete prior to the effective date of this ordinance. Changes in the property’s ownership or tenants of existing uses shall require no change in any existing building or structure.

4) New Signs

- i) The regulations contained in section 2.9 shall apply to all new signs.
- ii) Any permanent signs made non-conforming as a result of the adoption of this Development Code may be repaired, but not structurally altered or made more non-conforming in any way.

2.0.2 How to Use the Development Code

Requirements for new development are contained in section 2.1 - “Development Standards.” The specific municipal Regulations governing the Development Standards are defined in detail in the subsequent eight sections that make up Book II.

To review requirements for new development for any given property:

- i) Locate that property on the Fig. 2.1 - Corridor Centers and Segments Map, and note which Corridor Center or Segment the Property is in;
- ii) Turn to section 2.1 – Development Standards to find the portion of that section containing the Development Standards that apply to that Corridor Center or Segment area, and hence to the property in question. Review all the Development Standards for the applicable Corridor Center or Segment.
- iii) To understand the specific Regulations that correspond to any of the Development Standards in the Development Standards charts, turn to the corresponding Regulation number and name (the Regulation numbers and names are the same in the Development Standards charts as they are in the Development Regulations sections - sections 2.2 thru 2.9 - of Book II.
- iv) Review Guidelines. Guidelines common to all properties in the Plan Area can be found in most sections of the Development Regulations.
- v) All development projects shall also consult and comply with the city specification including but not limited to Huntington Beach Fire Code and Huntington Beach Fire Department city specifications, Municipal Code and Public Works Standards. Where there is a conflict between these specifications and the Beach and Edinger Corridors Specific Plan, the Specific Plan shall apply as determined by the Planning and Building Director.

2.0.3 Development Code Categories

Three primary types of Development Code are contained within the policy text of Book II, as follows:

1) Development Standards.

Development standards are specifications for new development that the community considers essential to the creation and preservation of a high quality, sustainable and coherent city. Conformance with Development Standards is mandatory. Such provisions are indicated by the use of the words “shall,” “must,” “is required,” “is/is not permitted.”

2) Development Regulations

Development Regulations are the detailed municipal policies that establish the specific rules and performance measures upon which community Development Standards are based. Development Regulations do not vary from one plan area to another.

3) Guidelines

Guidelines provide additional information to assist the designers of new development to conform with the intent of the Specific Plan. Guidelines pertain to issues of visual character and aesthetics. Conformance with Guidelines is recommended, especially to insure the swiftest possible approval. Although conformance with Guidelines is recommended, developers are permitted to propose alternative design solutions to these aspects of the development if they are able to show that such design solutions meet the overall objectives of the Specific Plan. Guidelines are indicated by the use of the words “should,” “may,” “is/are encouraged.”



2.0 ORIENTATION	<h2>2.0.4 Development Code Organization</h2> <p>The Development Code contained in Book II is organized into the sections as displayed in the diagram on the following page.</p>	
	<h2>2.0.5 How to get your Project Approved</h2>	
	<h3>1) Site Plan Review</h3> <p>Development projects within the Beach and Edinger Corridors Specific Plan area shall be subject to Site Plan Review, unless a conditional use permit is required pursuant to sections 2.1.4, 2.1.8 and 2.2.1. A Site Plan Review shall be required for all new development, with the exception of interior improvements, general maintenance and repair or other minor construction activities that do not result in an intensification of the use. These exceptions may be subject to other City permits and approvals prior to commencement.</p> <p>A Site Plan Review request shall be submitted to the City on a standard City application form and include standard submittal requirements as set forth on the application form. The application shall include any request for a subdivision pursuant to the Subdivision Map Act. The application shall also include a completed environmental assessment form and mitigation monitoring matrix, showing the project’s consistency with the Specific Plan Program EIR. This information will be used to determine if any further environmental analysis will be required for the project.</p> <p>A Site Plan Review application, tentative map and environmental assessment may also require analysis and comments from various department of the City. In order to approve a Site Plan Review application, the Director of Planning and Building shall make the following findings:</p> <ul style="list-style-type: none"><li>i) The project is consistent with the City’s General Plan and all applicable requirements of the Municipal Code; and</li><li>ii) The project will not be detrimental to the general welfare of persons working or residing in the vicinity nor detrimental to the value of the property and improvements in the neighborhood; and</li><li>iii) The project will not adversely affect the Circulation Plan of this Specific Plan; and</li><li>iv) The project complies with the applicable provisions of the Beach and Edinger Corridors Specific Plan and other applicable regulations.</li></ul> <p>The Director of Planning and Building has the authority to approve, conditionally approve, or deny a Site Plan Review and Subdivision Map, if any. The Zoning Administrator has the authority to approve or deny a Negative Declaration, Mitigated Negative Declaration, or Variance. The Planning Commission has the authority to approve or deny an Environmental Impact Report and a Conditional Use Permit unless otherwise specified for a particular use.</p> <p>The action of the Director of Planning shall be final unless appealed to the Planning Commission by the applicant or a member of City Council within 10 calendar days of. An appeal of a Site Plan Review or tentative map shall be subject to the procedures outlined in the City’s Zoning and Subdivision Ordinance.</p> <p>A Site Plan Review approval shall be valid for a period of one year. A maximum of two one-year extensions may be requested. A time extension request must be made in writing by the applicant, property owner(s), and /or authorized designee, a minimum of thirty days prior to the expiration of the current approval. If construction activity</p>	<p>does not commence within the approval or extension period, the entitlement shall be terminated.</p>
	<h3>2) Environmental Determination</h3> <p>The extent and intensity of all anticipated development activity for the Beach and Edinger Corridors Specific Plan area has been identified in the Specific Plan and analyzed at a programmatic level in Program Environmental Impact Report No. 08-008.</p> <p>Development project requests consistent with the Specific Plan may be subject to additional environmental review as required by CEQA. Environmental mitigation measures, as specified in the EIR may apply to individual Site Plan Reviews. As noted above, a mitigation measure matrix identifying how a project will comply with/incorporate the Program EIR mitigation measures is required as part of the submittal application. Additional mitigation measures may also apply to the project, should further environmental review indicate as such.</p>	
	<h3>3) Request for Deviation</h3> <p>Deviations from the Development Standards of the Specific Plan may be granted at the time of Site Plan Review for special circumstances and/or unique architectural features. Requests for Deviation may include but are not limited to building height, setbacks, open space, parking and landscaping. Deviation requests, up to 10 percent of any single standard, may be considered by the Director of Planning and Building. Requests for deviations greater than 10 percent shall be subject to approval of a Variance application by the Zoning Administrator, pursuant to the procedures outlined in the City’s Zoning and Subdivision Ordinance. Development and construction phasing of selected project components may be approved by the Director and shall not require a Request for Deviation or Variance to the Specific Plan.</p> <p>Deviations may be allowed when, in the opinion of the Director of Planning and Building, significantly greater benefits from the project can be provided than would occur if all the minimum requirements were met. Some additional benefits that may make a project eligible for consideration include: greater open space, greater setbacks, unique or innovative designs, public open space, and the use of energy conservation or green building technology. The Director of Planning and Building may approve the Request for Deviation in whole or in part upon making the following findings:</p> <ul style="list-style-type: none"><li>i) Promote better design, environmental and land planning techniques and contribute to the economic viability of the community, through aesthetically pleasing architecture, landscaping and site layout; and</li><li>ii) Not be detrimental to the general health, welfare, safety and convenience of the neighborhood or City in general, nor detrimental or injurious to the value of property or improvements of the neighborhood or of the City in general; and</li><li>iii) Be consistent with the objectives of the Specific Plan in achieving a project adapted to the area and compatible with the surrounding environment; and</li><li>iv) Be consistent with the goals and policies of the City’s General Plan, and</li></ul>	<p>comply with State and Federal Law.</p>
<h2>4) Transfers Between Corridors</h2> <p>The Specific Plan sets forth a Maximum Amount of Net New Development (MAND) for each development type that can be built within each corridor and for each development type (See section 2.1.1). Dwelling Units, commercial square footage and hotel rooms may be transferred between the two corridors. Entitlement applications involving a transfer request will require the following:</p> <ul style="list-style-type: none"><li>i) An Infrastructure Analysis documenting that the transfer does not exceed proposed infrastructure capacity. If capacity will be exceeded based on the required analysis, recommendations for additional infrastructure improvements must also be submitted. Required infrastructure modifications shall be the responsibility of the party requesting the transfer, and shall be placed as conditions of approval on the appropriate development entitlement.</li><li>ii) An Environmental Analysis in the form of the City’s Initial Study documentation that the proposed transfer will not affect the conclusions of the environmental analysis contained in Certified EIR No. 08-008. If the conclusions are affected, additional environmental analysis may be required subject to processing requirements of section 2.0.5</li><li>iii) A policy analysis documenting that the transfers are consistent with the goals, policies, and programs of the City of Huntington Beach General Plan and this Specific Plan.</li></ul> <p>Transfer requests, up to 20 percent of an individual development type, e.g., hotel rooms, may be considered by the Director of Planning and Building. Transfer requests greater than 20 percent shall be considered by the Planning Commission. A transfer request shall not constitute a Zoning Text Amendment and will be processed in conjunction with a Site Plan Review.</p>		
<h2>5) Appeals</h2> <p>The action of the Director of Planning and Building shall be final unless appealed to the Planning Commission by the applicant or a member of City Council within 10 calendar days of action of written notification by the Planning and Building Director. An appeal of a Site Plan Review or tentative map shall be subject to</p>		



Book II: Development Code Organization

2.0. ORIENTATION	2.1. DEVELOPMENT STANDARDS	Development Regulations							
		2.2. BUILDING USE REGULATIONS	2.3. BUILDING SCALE REGULATIONS	2.4. FRONTAGE & BUILDING PLACEMENT REGULATIONS	2.5. STREET REGULATIONS	2.6. OPEN SPACE REGULATIONS	2.7. PARKING REGULATIONS	2.8. ARCHITECTURE REGULATIONS	2.9. SIGNAGE REGULATIONS
2.0.1. Applicability	2.1.1. Maximum Amount of New Development (MAND)	2.2.1. Use Types	2.3.1. Building Height	2.4.1. Building Orientation to Streets and Public Open Spaces	2.5.1. Improvements to Existing Streets	2.6.1. Provision of Public Open Space	2.7.1. Provision of Parking	2.8.1. Facade Height Articulation Regulations	2.9.1. Freestanding Sign Setbacks
2.0.2. How to Use the Development Code	2.1.2. Establishment of Corridor Centers and Segments	2.2.2. Special Retail Configuration	2.3.2. Special Building Height Limits	2.4.2. Private Frontage Types	2.5.2. Provision of New Streets	2.6.2. Special Public Open Space Requirement	2.7.2. Parking Types	2.8.2. Architectural Elements Regulations	2.9.2. Total Sign Area
2.0.3. Development Code Categories	2.1.3. Town Center - Core	2.2.3. Affordable Housing Requirement	2.3.3. Building Length	2.4.3. Front Yard Setback	2.5.3. Block Size	2.6.3. Provision of Private Open Space	2.7.3. General Parking Requirements	2.8.3. Architectural Character	2.9.3. Sign Type Regulations
2.0.4. Development Code Organization	2.1.4. Town Center - Neighborhood		2.3.4. Special Building Length Limits	2.4.4. Side Yard Setback	2.5.4. Street Connectivity	2.6.4. Public Open Space Types	2.7.4. Parking Guidelines		2.9.4. Sign Guidelines - All Sign Types
2.0.5. How to Get your Project Approved	2.1.5. Neighborhood Center		2.3.5. Building Massing	2.4.5. Rear Yard Setback	2.5.5. Required East-West Street Connection	2.6.5. Private Open Space Types			
	2.1.6. Town Center Boulevard Segment			2.4.6. Alley Setback	2.5.6. Residential Transition Boundary Street	2.6.6. Stormwater Best Management Practices			
	2.1.7. Neighborhood Boulevard Segment			2.4.7. Frontage Coverage	2.5.7. Street Types (New Street Design)	2.6.7. Stormwater BMP Types			
	2.1.8. Neighborhood Parkway Segment			2.4.8. Space Between Buildings		2.6.8. Open Space Landscaping			
	2.1.9. Residential Parkway Segment			2.4.9. Build-to-Corner		2.6.9. Setback Area Landscaping			
	2.1.10. Residential Transition Zone								

the procedures outlined in the City’s Zoning and Subdivision Ordinance.

2.1 DEVELOPMENT STANDARDS

2.1.1 Maximum Amount of Net New Development (MAND)

This section establishes the maximum amount of net new construction of residential and commercial development permitted in the *Beach and Edinger Corridors Specific Plan*.

The City will monitor and publish the amount of new development that occurs after the adoption of the specific plan in a form to be determined by the Planning and Building Director. Updates to this summary of development will occur each time new development takes place. Except as specified in Section 2.2.4, when the MAND is reached, no further development may be permitted without an amendment to the MAND provisions and environmental review.

Upon issuance of a Building Permit, a project shall be deemed to be entitled to its allocation of the MAND specified in the Building Permit, but such entitlement shall expire unless construction commences for such units within one year of the date of issuance of the Building Permit and is pursued reasonably to completion as determined by the Chief Building and Safety Official. No Building Permit may be issued to allow a net increase in development in excess of the MAND, except as specified in Section 2.2.4.

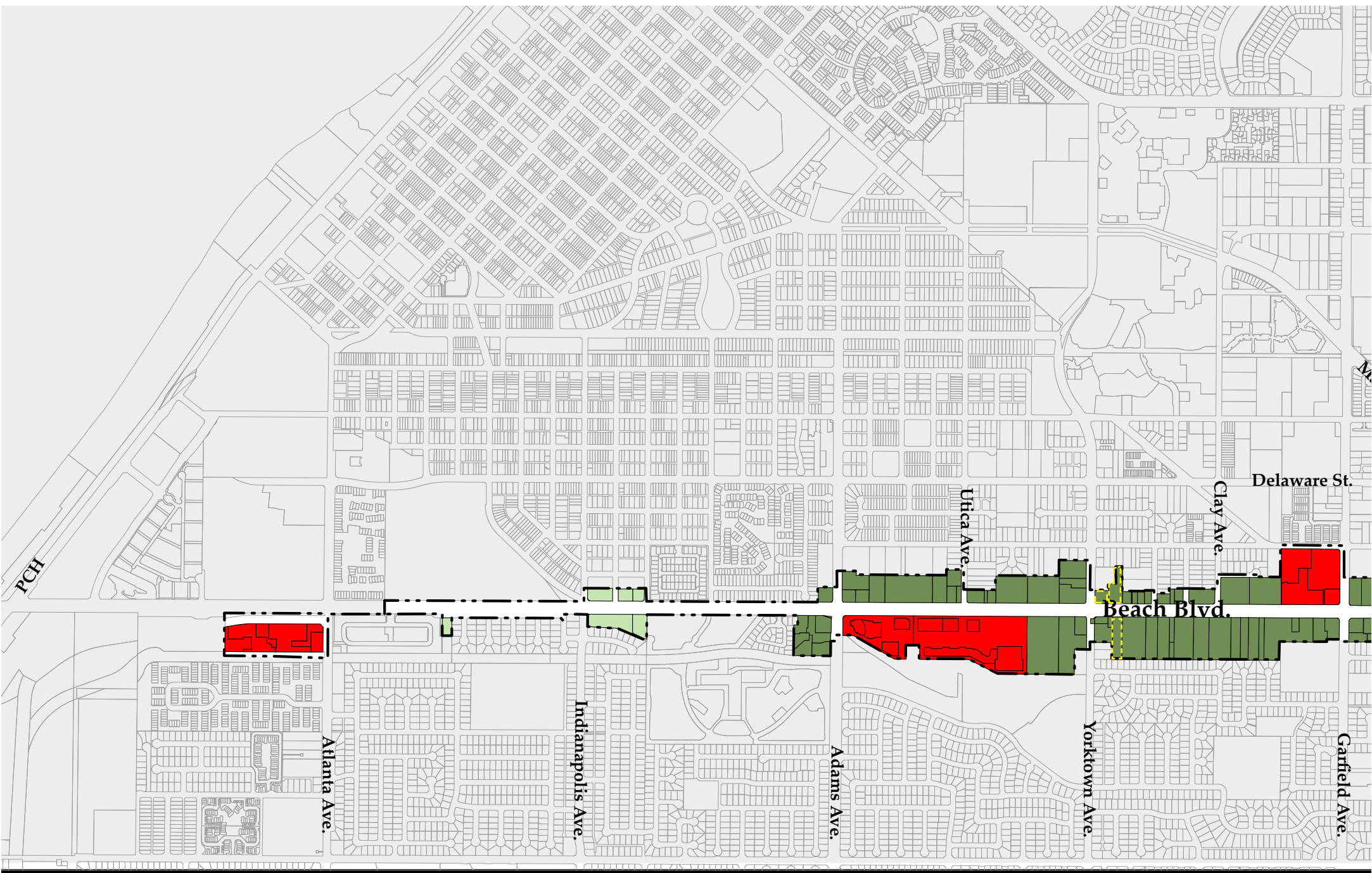
The MAND established herein corresponds to the installation of intersection capacity improvements necessary to maintain the community’s level of service standard for primary arterial corridor intersections as well as installation of utility infrastructure (i.e. sewer, water, drainage and dry utilities). Detailed descriptions of those infrastructure improvements are included in Book III – Public Improvements.

The MAND for the Beach and Edinger Corridors planning area is analyzed in Environmental Impact Report (EIR) No. 08-008 corresponding to the *Beach and Edinger Corridors Specific Plan* and is set forth as follows:

Corridor	Permitted Dwelling Units	Remaining Dwelling Units*	Retail SF	Office SF	Hotel Rooms
Edinger	1,375	0	206,000	0	150
Beach	525	200	532,000	112,000	200
Total	1,900	2,100	739,4000	112,000	350

*\*As of the effective date of this amendment to the BECSP (06.04.15)*

See section 2.0.5 for process requirements to transfer building units between Corridors. A request to change a MAND total constitutes a Specific Plan Amendment subject to section i.2.



2.1.2 Establishment of Corridor Centers and Segments

1) Corridor Centers and Segments Map

- i) Eight (8) Corridor *Centers* and *Segments* are established as the basic organizing principle for the Development Standards applied to all properties in the Specific Plan Area.
- ii) The Centers and Segments are established in the specific locations and with the specific names indicated in the Fig.2.1 Corridor Centers and Segments Map.

2) Transect Designations

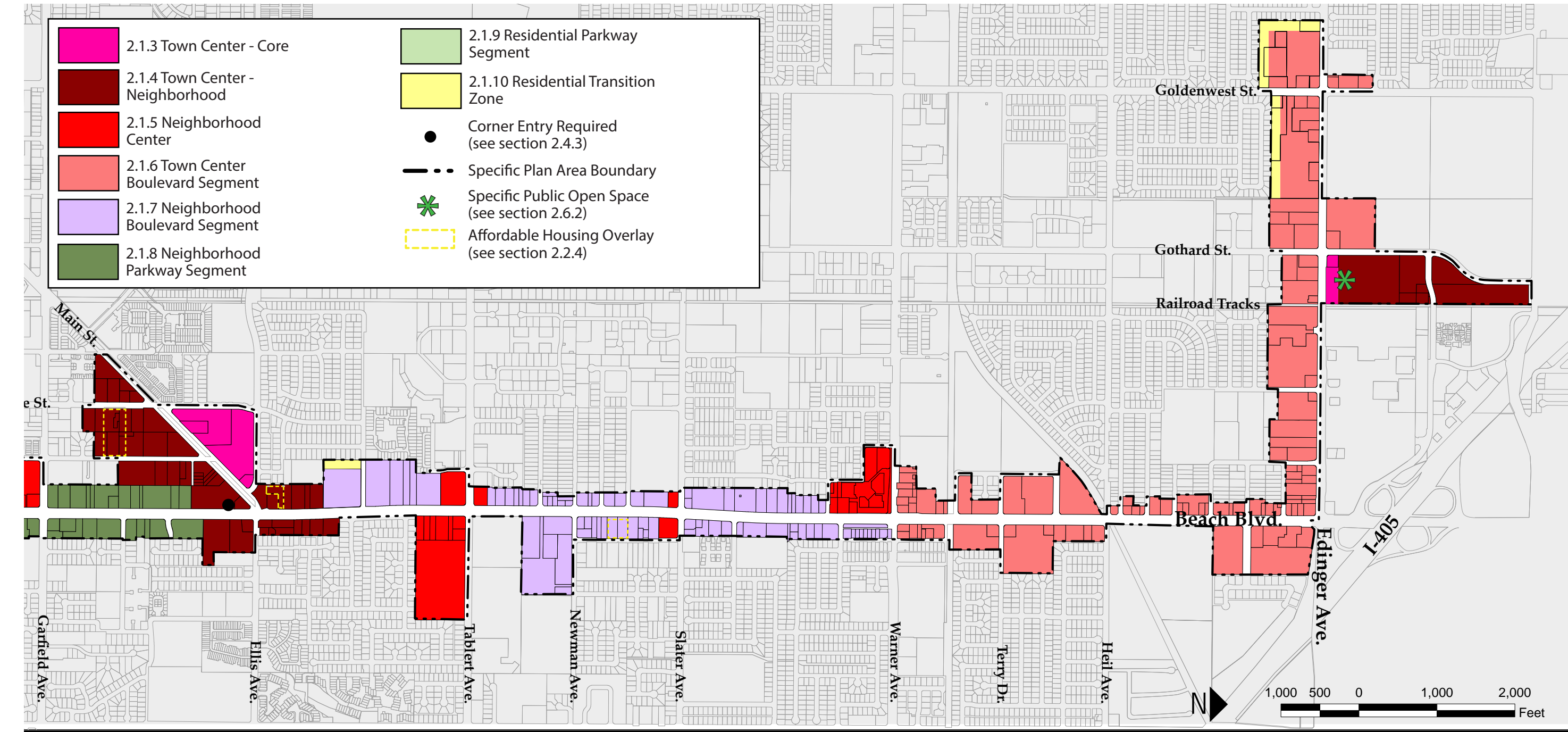
- i) The Transect is a system of classification for built environments established in the SmartCode. It uses the concept that place types range from rural (T1) to urban (T6). The character and function of a district, center, or segment is an extension of its place in the continuum of the Transect. Every element of the built environment has a place in the Transect. This Development Code uses the Transect as the underlying principle of organization for the Plan Area’s Centers and Segments and the corresponding Development Standards. For reference purposes, Transect Zones have been identified for the Centers and Segments established in this plan as indicated in the Fig.2.1 Corridor Centers and Segments Map legend.

3) How Corridor Centers and Segments Apply To Parcels

Every parcel in the Plan Area shall be regulated by its location in one or more designated Corridor Centers and Segment as shown in the Fig.2.1. Corridors Centers and Segments Map. Where further clarification is necessary, boundaries shall be determined by consulting with the Planning Director/Designee as described below for split parcels.



Fig. 2.1 Corridor Centers and Segments Map



**a) *Parcels with a single Designation***

All development on parcels, assembled parcels, or portions of allocated in a single Corridor Centers or Segment must conform to the development standards that apply to that corridor or segment.

**b) *Split Parcels:***

- i) Where Corridor Centers and Segments or Plan Area Boundaries appear to connect between parcel lines of adjacent properties as shown on the Fig.2.1. Corridors Centers and Segments Map, the boundary shall be determined by the Planning and Building Director/Designee as measured on a scaled version of the Corridors Centers and Segments Map.
- ii) Each portion of the Split Parcel shall be regulated by the applicable Corridor Centers or Segment.

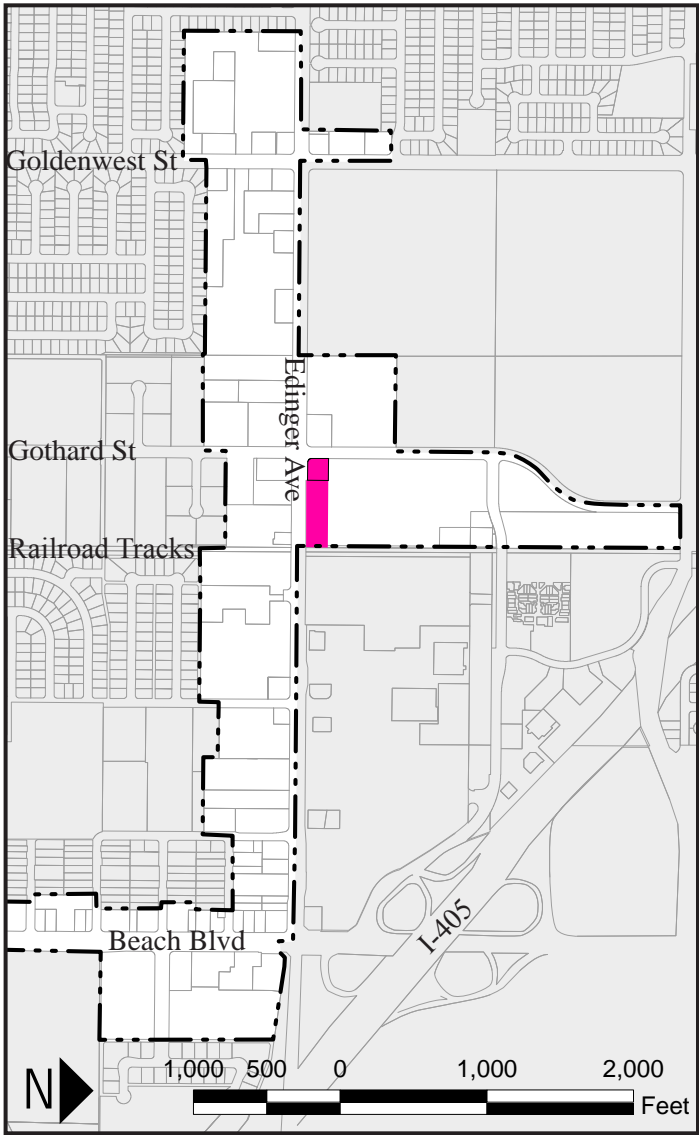
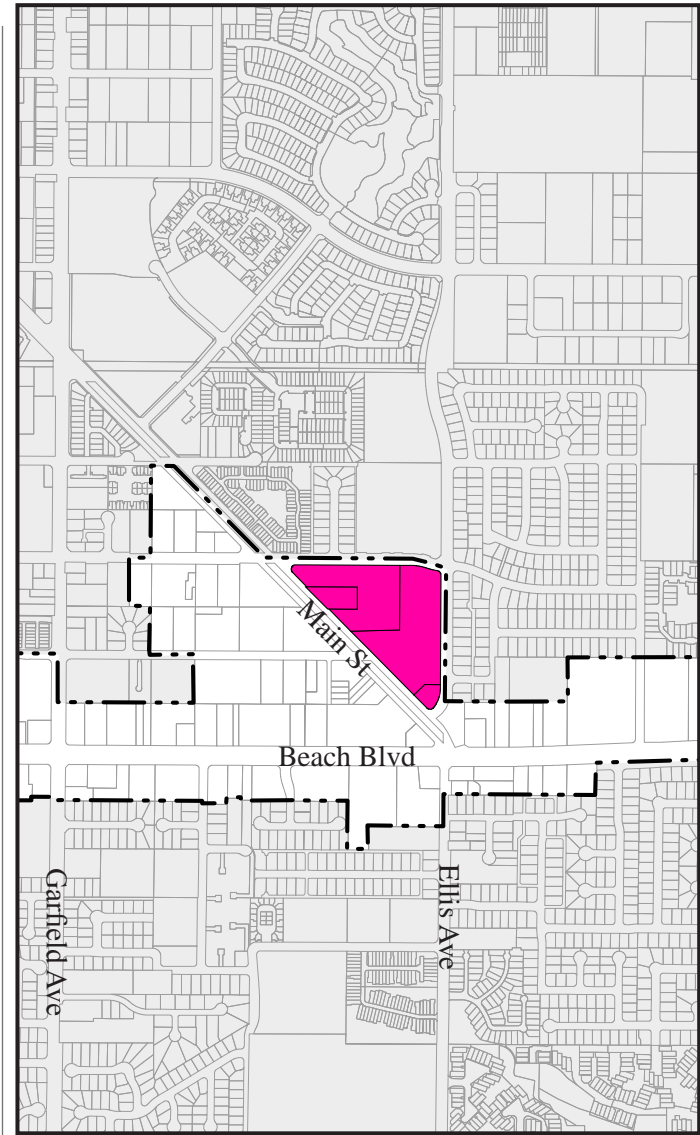
**4) *Development Standards Charts***

- i) The development standards applied to each Corridor Center and Segment are contained in the Development Standards Charts contained in 2.1.3 – 2.1.10.
- ii) Each chart lists Regulations in the order that they appear in section 2.2 – 2.9 in the left column of the chart.
- iii) Each chart lists the Development Standards that apply to each Regulation in the right column of the chart.

**5) *Regulations and Guidelines***

- i) Remember to review Regulatory Definitions, General Requirements, and Guidelines common to all properties within the Plan Area contained in sections 2.2 – 2.9 as necessary.





Development Standards Charts Legend:

Symbol:		(C7)	Limited to the expansion of existing uses	(L3)	On Main St.	(L8)	On Gothard St.
permitted	These elements are allowed, by right, unless otherwise specified in Section 2.2.1 Use Types	(C8)	For development over 20,000 sf of building area	(L4)	On Ellis Ave. and Delaware St.	(L9)	On Ash St. and Cypress Ave.
---	Not permitted	(C9)	For development over 20 dwelling units or lodging rooms	(L5)	South of Atlanta Ave.	(L10)	On Warner Ave.
n/a	Not applicable	(C10)	Courtyard buildings may exceed the indicated length as defined in 2.3.4 Building Length				
required	These are required elements of all new development as indicated	(C11)	The number of spaces provided must include at least one space for each student	Use:			
conditional	Requires a conditional use permit	(C12)	Commercial projects proposing additions greater than 20% of existing sq. footage but not exceeding 50% have the option to provide the Classic Blvd. design or a Neighborhood St. configuration with a 6' sidewalk and 6' parkway; however Classic Blvd. street lights and furnishings shall apply	(U1)	For residential development with 3 or more units		
limited	Limitations apply as specified in section 2.4.3. Private Frontage Types	(C13)	Min. 10' setback along front and sides of building for a depth of min. 100' for structures above 3 stories	(U2)	Live Entertainment and Dancing not permitted		
plus	Development shall not exceed the height of housing across the street plus the number of stories indicated (see section 2.3.2 Special Building Height Limits)	(C14)	All residential projects shall include retail/commercial at street level; deviations may be permitted subject to a CUP from PC	(U3)	Large Scale Specialty Goods and Foods Only (see section 2.2.1)		
CS	Permitted in a Corner Store configuration (see section 2.2.2. Special Retail Configurations)	(C15)	CUP from PC shall be required for all residential and mixed use: residential/commercial projects	(U4)	Movie Theaters not permitted		
NC	Permitted in a Neighborhood Center configuration (see section 2.2.2. Special Retail Configurations)	(C16)	May be reduced to min. of 15' pursuant to a CUP from the PC	(U5)	Ground floor Health & Exercise Clubs are conditional		
PP / N-PP	Required for Priority Projects / Encouraged for Non-Priority Projects (see section 2.6.7. Stormwater BMP Types)	(C17)	Single story commercial buildings may be permitted to have a 15' front yard setback from primary St. and 10' front setback along secondary St. (may be reduced pursuant to a CUP from the PC)	(U7)	Only Health & Exercise clubs		
(A)	Exceptions apply to anchor retail buildings	(C18)	May be provided in tandem configuration; All projects with a residential component shall submit a parking management plan subject to review and approval by the Planning and Building Dept.	(U8)	Only Gas Stations		
		(C19)	Requires a CUP from the ZA	(U9)	Residential Required as Shown on Map		
		Location:			(U10)	Auto dealers only	
		(L1)	On Beach Blvd.	(L6)	Between Ellis		
		(L2)	On Edinger Ave.	(L7)	North of Ellis		
					(U11)	Not applicable to new or existing auto dealers	
Special Conditions:		Signs:					
(C1)	Ground floor allowed	(S1) Exceptions apply for changeable copy & other sign variations, see section 2.9.Signage Regulations					
(C2)	Upper floors only	(S3) 50% shall be allowable during the month of December					
(C3)	Parkway Landscaping required along Beach Blvd.	(S4) Must occur as tower format					
(C4)	Side, and rear yards shall consist of heavy screening (see section 2.6.7)	(S5) For churches, schools, and commercial recreation uses within public parks, maximum area of 32 sqft and maximum height of 6 ft					
(C5)	With shopfront	(S6) Signs are restricted to corner stores only					
(C6)	Or 20% of parcel depth whichever is smaller	(S7) Signs are permitted at entrances to multi-family buildings					

2.2 Building Use Regulations	Standards
2.2.1 Use Types	
1) Retail	
a) Specialty Goods Anchors	permitted
b) Community Oriented Anchors	permitted
c) Entertainment Anchors	permitted
d) Eating & Drinking Establishments	permitted
e) Specialty Goods & Foods	permitted
f) Entertainment & Recreation	conditional (U5)(C19)
g) Convenience Uses	permitted
h) Business Services	permitted
i) Personal Services	permitted
j) Personal Enrichment Services	permitted
k) Service Commercial & Repair	---
l) Large Scale Commercial Goods	---
m) Vehicle Sales	---
2) Civic & Cultural	conditional (C19)
3) Office	
a) Professional Services	permitted (C2 & L4)
b) Medical Services	permitted
4) Lodging	permitted (C2)
5) Live Work	---
6) Residential	
a) Multi-Family w/ Common Entry	conditional (C2, C14, C15, L4)
b) Multi Family w/ Individual Entries	conditional (C14, C15, L4)
c) Attached Single Family	---
d) Detached Single Family	---
2.2.2 Special Retail Configurations	
a) Neighborhood Center	n/a
b) Corner Store	n/a
c) Drive-through	---
2.2.3 Affordable Housing Requirement	
required or not required	required (U1)

2.3 Building Scale Regulations	Standards
2.3.1 Building Height	
minimum height	3 stories; (A): 1 story
maximum height	4 stories
2.3.2 Special Building Height Limits	
Edinger/Beach/Main	4 story max
Across the Street From Housing	plus 3 stories max
Adjacent to Housing	required
2.3.3 Building Length	
maximum	300 ft
2.3.4 Special Building Length Limits	
Limited Corner Building - maximum	n/a
Limited Mid-Block Building - maximum	n/a
2.3.5 Building Massing - Primary Volume Proportions	
Length : Height - Edinger/Beach/Main	3L:2H to 5L:2H
Length : Height - All other streets	1L:3H to 3L:1H

2.4 Frontage & Building Placement Regulations	Standards
2.4.1 Building Orientation to Streets & Public Open Space	
required or not required	required
2.4.2 Private Frontage Types	
3) Private Frontage Specification	
a) Shop-Front	permitted
Shopfront Length - maximum	50 ft
Tenant Length - maximum	50 ft
Articulation Length - maximum	50 ft
b) Corner Entry	permitted (C5)
c) Arcade	---
d) Grand Portico	---
e) Forecourt	permitted (C5)
f) Common Lobby Entry	limited
g) Stoop	permitted (L4) only
h) Porch	---
i) Front Door	---
j) Edge Treatment: Fenced	---
k) Edge Treatment: Terraced	---
l) Edge Treatment: Flush	permitted
2.4.3 Front Yard Setback	
minimum - Edinger	30 ft (C16)(C17)
minimum - Main	30 ft (C16)(C17)
minimum - all other public streets	30 ft (C16)(C17)
Upper story setback	Required (C13)
2.4.4 Side Yard Setback	
min w/ living space windows	10 ft
min w/out living space windows	0 ft
2.4.5 Rear Yard Setback	
minimum	5 ft
2.4.6 Alley Setback	
minimum	5 ft
2.4.7 Frontage Coverage	
minimum - Edinger/Beach/Main	90%
minimum - all other streets	75%
2.4.8 Space Between Buildings	
minimum	20 ft
2.4.9 Build-to-Corner	
required or not required	required

2.5 Street Regulations	Standards
2.5.1 Improvements to Existing Streets	
3) Classic Boulevard	required (L2)(C12)
6) Standard Avenue	required (L3)
7) Neighborhood Streets	required except (L2, L3)
2.5.2 Provision of New Streets - (see section 2.5.2)	
2.5.3 Block Size	
Maximum Block Size	1900 ft
Maximum Edinger Block Face	n/a
2.5.4 Street Connectivity	
required or not required	required
2.5.5 Required East-West Street Connection	
required or not required	n/a
2.5.6 Residential Transition-Boundary Street	
required or not required	n/a
2.5.7 Street Types (New Street Design)	
1) City Street	permitted
2) Public Open Space w/ City Street	permitted
3) Neighborhood Street	---
4) Public Open Space w/ Neighborhood St.	---
5) Alley	permitted

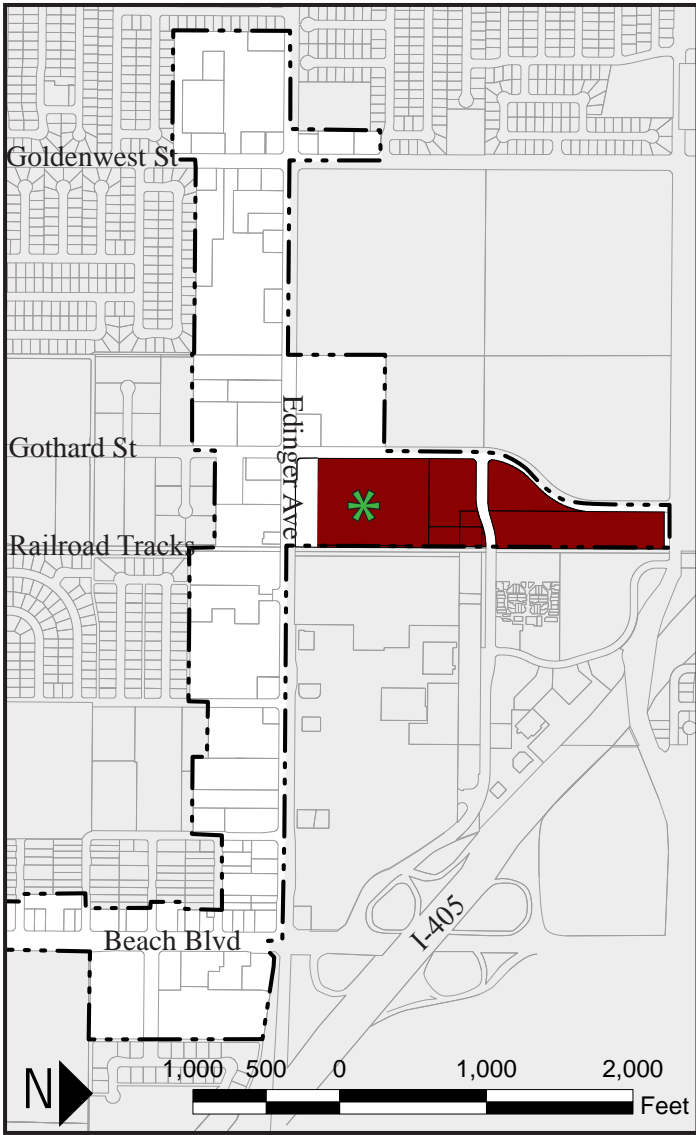
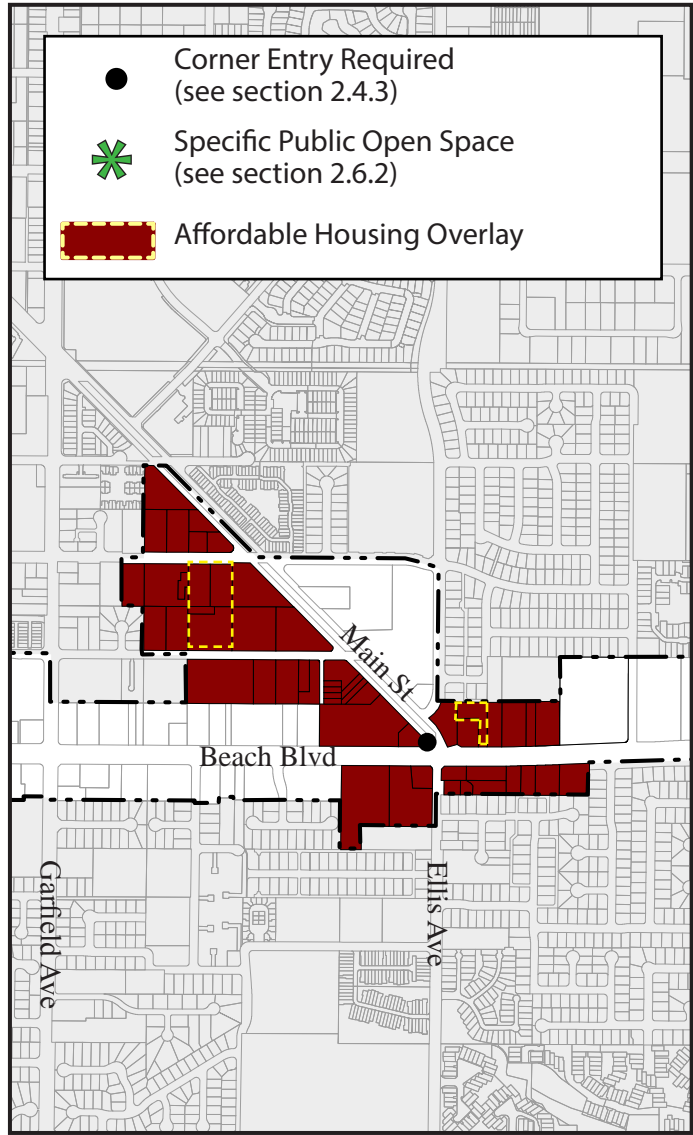
2.6 Open Space Regulations	Standards
2.6.1 Provision of Public Open Space	
1) Retail	50 s.f. / 1000 s.f. (C8)
2) Civic & Cultural	n/a
3) Office	n/a
4) Lodging	n/a
5) Live Work	n/a
6) Residential	n/a
2.6.2 Special Public Open Space	
	n/a
2.6.3 Provision of Private Open Space	
1) Retail	n/a
2) Civic & Cultural	n/a
3) Office	n/a
4) Lodging	n/a
5) Live Work	n/a
6) Residential	
a) Attached & Multi-Family	60 s.f. / unit
b) Detached Single-Family Homes	n/a
2.6.4 Public Open Space Types	
1) Park	---
2) Linear Green	permitted
3) Square	permitted
4) Plaza	permitted
5) Mid-Block Green	permitted
6) Courtyard Plaza	permitted
7) Passage/Paseo	permitted
8) Pocket Park/Playground	permitted
2.6.5 Private Open Space Types	
1) Courtyard	permitted
2) Private Yard	permitted
3) Porch	---
4) Rooftop Deck or Garden	permitted
5) Balcony	permitted
2.6.6 Stormwater Management Best Management Practices	
required or not required	required
2.6.7 Stormwater BMP Types	
Source Control BMPs	required
Site Design BMPs	required
Treatment Control BMPs	PP / N-PP
2.6.8 Open Space Landscaping	
required or not required	required
2.6.5 Setback Area Landscaping Types	
1) Perimeter Block Setback Areas	
a) Sidewalk Extension	permitted
b) Parkway Landscaping	---
c) Boulevard Landscaping	---
d) Neighborhood Street Landscaping	permitted (L4) only
2) Interior Block Setback Areas	
a) Groundcover	required
b & c) Moderate or Heavy Screening	---

2.7 Parking Regulations	Standards
2.7.1 Provision of Parking	
1) Retail Anchors	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
2) Eating & Drinking Establishments	
spaces per 1000 s.f.	4 min / 10 max
location	on site
3) Specialty Foods / Goods	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
4) Entertainment & Recreation	
spaces per 1000 s.f.	6 min / 10 max
location	within 500 feet
5) Convenience Uses	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
6) Personal & Business Services	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
7) Presonl Enrichment Services	
spaces per 1000 s.f.	5 max (C11)
location	within 500 feet
7) Commercial Goods & Vehicle Sales	
spaces per 1000 s.f.	n/a
location	n/a
8) Civic & Cultural	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
9) Office - Professional	
spaces per 1000 s.f.	2.5 min / 3.5 max
location	within 200 feet
10) Office - Medical	
spaces per 1000 s.f.	4.5 min / 5.5 max
location	within 200 feet
11) Lodging	
spaces per guest room	1 min / 1.2 max
location	within 200 feet
12) Live-Work	
spaces per unit	1 min / 1 max
spaces per employee	0 min / 1 max
location	within 200 feet
13) Residential Uses	(C18)
spaces per studio & 1br unit	2 min
spaces per 2br unit	2 min
spaces per 3br+unit	2.5 min
guest spaces per 10 units	5 min (0.5 per unit)
location	on site
14) Neighborhood Center Exceptions	
spaces per 1000 s.f.	0 min / 3 max
15) Corner Store Exceptions	
spaces per 1000 s.f.	0 min
location	on street only
2.7.2 Parking Types	
1) Surface Lot - Front	---
2) Surface Lot - Side	---
3) Surface Lot - Rear	permitted
4) Surface Lot - Exposed	---
5) Structure - Exposed	---
6) Structure - Wrapped: Ground Level	---
7) Structure - Wrapped: All Levels	permitted (L4) only
8) Partially Submerged Podium	permitted
9) Structure - Underground	permitted

2.8 Architecture Regulations	Standards
2.8.1 Facade Height Articulation Regulations	
Top	required
Base	required
2.9 Signage Regulations	Standards
2.9.1 Freestanding Sign Setbacks	(see section 2.9.1)
2.9.2 Total Sign Area	(see section 2.9.2)
2.9.3 Sign Type Regulations	
Grand Projecting Sign	permitted
maximum height	30 ft
Marquee Sign	permitted
maximum number of faces	3
maximum area	500 s.f.
Wall Sign	permitted
maximum size	200 s.f. (S1 & S5)
Monument Sign & Ground Sign	---
maximum number of faces	n/a
maximum height	n/a
maximum area	n/a
secondary sign	n/a
bonus sign	n/a
Pole Mounted Sign & Tower Sign	---
maximum number of faces	n/a
maximum height	n/a
secondary sign	n/a
bonus sign	n/a
maximum area	n/a
secondary sign	n/a
bonus sign	n/a
Projecting Sign	permitted
maximum area	8 s.f.
Awning Face Sign	permitted
maximum area	20 % of awning face
Awning Valance Sign	permitted
lines of lettering	1
letter height (whichever is less)	2/3 valance height : 8 in
Awning Side Sign	permitted
lines of lettering	1
letter height	8 in
Canopy Fascia Sign	permitted
maximum height (whichever is less)	2/3 fascia height : 12 in
maximum width	2/3 canopy width
lines of lettering	1
Above Canopy Sign	permitted
maximum height (whichever is less)	1 1/2 fascia height : 24 in
maximum width	2/3 canopy width
lines of lettering	1
Café Umbrella Sign	permitted
maximum area	n/a
Recessed Entry Sign	permitted
maximum area	20 s.f.
Window Sign	permitted
maximum area	20% of window (S3)

2.7 Parking Regulations Note: The maximum number of parking spaces permitted only applies to parking spaces that are provided in surface parking lots.





Development Standards Charts Legend:

Symbol:		(C7)	Limited to the expansion of existing uses		(L3)	On Main St.	(L8)	On Gothard St.
permitted	These elements are allowed, by right, unless otherwise specified in Section 2.2.1 Use Types	(C8)	For development over 20,000 sf of building area		(L4)	On Ellis Ave. and Delaware St.	(L9)	On Ash St. and Cypress Ave.
---	Not permitted	(C9)	For development over 20 dwelling units or lodging rooms		(L5)	South of Atlanta Ave.	(L10)	On Warner Ave.
n/a	Not applicable	(C10)	Courtyard buildings may exceed the indicated length as defined in 2.3.4 Building Length					
required	These are required elements of all new development as indicated	(C11)	The number of spaces provided must include at least one space for each student		Use:			
conditional	Requires a conditional use permit		Commercial projects proposing additions greater than 20% of existing sq. footage but not exceeding 50% have the option to provide the Classic Blvd. design or a Neighborhood St. configuration with a 6' sidewalk and 6' parkway; however Classic Blvd. street lights and furnishings shall apply		(U1)	For residential development with 3 or more units		
limited	Limitations apply as specified in section 2.4.3. Private Frontage Types	(C12)			(U2)	Live Entertainment and Dancing not permitted		
plus	Development shall not exceed the height of housing across the street plus the number of stories indicated (see section 2.3.2 Special Building Height Limits)	(C13)	Min. 10' setback along front and sides of building for a depth of min. 100' for structures above 3 stories		(U3)	Large Scale Specialty Goods and Foods Only (see section 2.2.1)		
CS	Permitted in a Corner Store configuration (see section 2.2.2. Special Retail Configurations)	(C14)	All residential projects shall include retail/commercial at street level; deviations may be permitted subject to a CUP from PC		(U4)	Movie Theaters not permitted		
NC	Permitted in a Neighborhood Center configuration (see section 2.2.2. Special Retail Configurations)	(C15)	CUP from PC shall be required for all residential and mixed use: residential/commercial projects		(U5)	Ground floor Health & Exercise Clubs are conditional		
PP / N-PP	Required for Priority Projects / Encouraged for Non-Priority Projects (see section 2.6.7. Stormwater BMP Types)	(C16)	May be reduced to min. of 15' pursuant to a CUP from the PC		(U7)	Only Health & Exercise clubs		
(A)	Exceptions apply to anchor retail buildings	(C17)	Single story commercial buildings may be permitted to have a 15' front yard setback from primary St. and 10' front setback along secondary St. (may be reduced pursuant to a CUP from the PC)		(U8)	Only Gas Stations		
		(C18)	May be provided in tandem configuration; All projects with a residential component shall submit a parking management plan subject to review and approval by the Planning and Building Dept.		(U9)	Residential Required as Shown on Map		
		(C19)	Requires a CUP from the ZA		(U10)	Auto dealers only		
					(U11)	Not applicable to new or existing auto dealers		



2.6 Open Space Regulations	Standards
2.6.1 Provision of Public Open Space	
1) Retail	50 s.f. / 1000 s.f. (C8)
2) Civic & Cultural	n/a
3) Office	100 s.f. / 1000 s.f. (C8)
4) Lodging	30 s.f. / room (C9)
5) Live Work	50 s.f. / unit (C9)
6) Residential	50 s.f. / unit (C9)
2.6.2 Special Public Open Space	
	required (M)
2.6.3 Provision of Private Open Space	
1) Retail	n/a
2) Civic & Cultural	n/a
3) Office	n/a
4) Lodging	n/a
5) Live Work	n/a
6) Residential	
a) Attached & Multi-Family	60 s.f. / unit
b) Detached Single-Family Homes	n/a
2.6.4 Public Open Space Types	
1) Park	permitted
2) Linear Green	permitted
3) Square	permitted
4) Plaza	permitted
5) Mid-Block Green	permitted
6) Courtyard Plaza	permitted
7) Passage/Paseo	permitted
8) Pocket Park/Playground	permitted
2.6.5 Private Open Space Types	
1) Courtyard	permitted
2) Private Yard	permitted
3) Porch	---
4) Rooftop Deck or Garden	permitted
5) Balcony	permitted
2.6.6 Stormwater Management Best Management Practices	
required or not required	required
2.6.7 Stormwater BMP Types	
Source Control BMPs	required
Site Design BMPs	required
Treatment Control BMPs	PP / N-PP
2.6.8 Open Space Landscaping	
required or not required	required
2.6.5 Setback Area Landscaping Types	
1) Perimeter Block Setback Areas	
a) Sidewalk Extension	permitted; required (C5)
b) Parkway Landscaping	---
c) Boulevard Landscaping	required (L1 except C5)
d) Neighborhood Street Landscaping	permitted
2) Interior Block Setback Areas	
a) Groundcover	required
b & c) Moderate or Heavy Screening	permitted

2.7 Parking Regulations	Standards
2.7.1 Provision of Parking	
1) Retail Anchors	
spaces per 1000 s.f.	n/a
location	n/a
2) Eating & Drinking Establishments	
spaces per 1000 s.f.	4 min / 10 max
location	within 500 feet
3) Specialty Foods / Goods	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
4) Entertainment & Recreation	
spaces per 1000 s.f.	n/a
location	n/a
5) Convenience Uses	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
6) Personal & Business Services	
spaces per 1000 s.f.	3 min / 5 max
location	within 500 feet
7) Presonl Enrichment Services	
spaces per 1000 s.f.	5 max (C11)
location	within 500 feet
7) Commercial Goods & Vehicle Sales	
spaces per 1000 s.f.	n/a
location	n/a
8) Civic & Cultural	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
9) Office - Professional	
spaces per 1000 s.f.	2.5 min / 3.5 max
location	within 200 feet
10) Office - Medical	
spaces per 1000 s.f.	4.5 min / 5.5 max
location	within 200 feet
11) Lodging	
spaces per guest room	1 min / 1.2 max
location	within 200 feet
12) Live-Work	
spaces per unit	1 min / 1 max
spaces per employee	0 min / 1 max
location	within 200 feet
13) Residential Uses	(C18)
spaces per studio & 1br unit	2 min
spaces per 2br unit	2 min
spaces per 3br+unit	2.5 min
guest spaces per 10 units	5 min (0.5 per unit)
location	on site
14) Neighborhood Center Exceptions	
spaces per 1000 s.f.	0 min / 3 max
15) Corner Store Exceptions	
spaces per 1000 s.f.	0 min
location	on street only
2.7.2 Parking Types	
1) Surface Lot - Front	---
2) Surface Lot - Side	---
3) Surface Lot - Rear	permitted
4) Surface Lot - Exposed	---
5) Structure - Exposed	---
6) Structure - Wrapped: Ground Level	permitted
7) Structure - Wrapped: All Levels	permitted
8) Partially Submerged Podium	permitted
9) Structure - Underground	permitted

2.8 Architecture Regulations	Standards
2.8.1 Facade Height Articulation Regulations	
Top	required
Base	required
2.9 Signage Regulations	Standards
2.9.1 Freestanding Sign Setbacks	(see section 2.9.1)
2.9.2 Total Sign Area	(see section 2.9.2)
2.9.3 Sign Type Regulations	
Grand Projecting Sign	---
maximum height	n/a
Marquee Sign	---
maximum number of faces	n/a
maximum area	n/a
Wall Sign	permitted
maximum size	200 s.f. (S1 & S5)
Monument Sign & Ground Sign	permitted
maximum number of faces	2
maximum height	6 ft
maximum area	48 s.f. (S1)
secondary sign	---
bonus sign	24 s.f.
Pole Mounted Sign & Tower Sign	---
maximum number of faces	n/a
maximum height	n/a
secondary sign	n/a
bonus sign	n/a
maximum area	n/a
secondary sign	n/a
bonus sign	n/a
Projecting Sign	permitted
maximum area	8 s.f.
Awning Face Sign	permitted
maximum area	20 % of awning face
Awning Valance Sign	permitted
lines of lettering	1
letter height (whichever is less)	2/3 valance height : 8 in
Awning Side Sign	permitted
lines of lettering	1
letter height	8 in
Canopy Fascia Sign	permitted
maximum height (whichever is less)	2/3 fascia height : 12 in
maximum width	2/3 canopy width
lines of lettering	1
Above Canopy Sign	permitted
maximum height (whichever is less)	1 1/2 fascia height : 24 in
maximum width	2/3 canopy width
lines of lettering	1
Café Umbrella Sign	permitted
maximum area	n/a
Recessed Entry Sign	permitted
maximum area	20 s.f.
Window Sign	permitted
maximum area	20% of window (S3)

2.7 Parking Regulations Note: The maximum number of parking spaces permitted only applies to parking spaces that are provided in surface parking lots.



Development Standards Charts Legend:

Symbol:		(C7)	Limited to the expansion of existing uses	(L3)	On Main St.	(L8)	On Gothard St.
permitted	These elements are allowed, by right, unless otherwise specified in Section 2.2.1 Use Types	(C8)	For development over 20,000 sf of building area	(L4)	On Ellis Ave. and Delaware St.	(L9)	On Ash St. and Cypress Ave.
---	Not permitted	(C9)	For development over 20 dwelling units or lodging rooms	(L5)	South of Atlanta Ave.	(L10)	On Warner Ave.
n/a	Not applicable	(C10)	Courtyard buildings may exceed the indicated length as defined in 2.3.4 Building Length				
required	These are required elements of all new development as indicated	(C11)	The number of spaces provided must include at least one space for each student	Use:			
conditional	Requires a conditional use permit		Commercial projects proposing additions greater than 20% of existing sq. footage but not exceeding 50% have the option to provide the Classic Blvd. design or a Neighborhood St. configuration with a 6' sidewalk and 6' parkway; however Classic Blvd. street lights and furnishings shall apply	(U1)	For residential development with 3 or more units		
limited	Limitations apply as specified in section 2.4.3. Private Frontage Types	(C12)	Min. 10' setback along front and sides of building for a depth of min. 100' for structures above 3 stories	(U2)	Live Entertainment and Dancing not permitted		
plus	Development shall not exceed the height of housing across the street plus the number of stories indicated (see section 2.3.2 Special Building Height Limits)	(C13)	All residential projects shall include retail/commercial at street level; deviations may be permitted subject to a CUP from PC	(U3)	Large Scale Specialty Goods and Foods Only (see section 2.2.1)		
CS	Permitted in a Corner Store configuration (see section 2.2.2. Special Retail Configurations)	(C14)	CUP from PC shall be required for all residential and mixed use: residential/commercial projects	(U4)	Movie Theaters not permitted		
NC	Permitted in a Neighborhood Center configuration (see section 2.2.2. Special Retail Configurations)	(C15)	May be reduced to min. of 15' pursuant to a CUP from the PC	(U5)	Ground floor Health & Exercise Clubs are conditional		
PP / N-PP	Required for Priority Projects / Encouraged for Non-Priority Projects (see section 2.6.7. Stormwater BMP Types)		Single story commercial buildings may be permitted to have a 15' front yard setback from primary St. and 10' front setback along secondary St. (may be reduced pursuant to a CUP from the PC)	(U7)	Only Health & Exercise clubs		
(A)	Exceptions apply to anchor retail buildings	(C18)	May be provided in tandem configuration; All projects with a residential component shall submit a parking management plan subject to review and approval by the Planning and Building Dept.	(U8)	Only Gas Stations		
		(C19)	Requires a CUP from the ZA	(U9)	Residential Required as Shown on Map		
Special Conditions:				(U10)	Auto dealers only		
(C1)	Ground floor allowed			(U11)	Not applicable to new or existing auto dealers		
(C2)	Upper floors only			Signs:			
(C3)	Parkway Landscaping required along Beach Blvd.			(S1)	Exceptions apply for changeable copy & other sign variations, see section 2.9.Signage Regulations		
(C4)	Side, and rear yards shall consist of heavy screening (see section 2.6.7)			(S3)	50% shall be allowable during the month of December		
(C5)	With shopfront			(S4)	Must occur as tower format		
(C6)	Or 20% of parcel depth whichever is smaller			(S5)	For churches, schools, and commercial recreation uses within public parks, maximum area of 32 sqft and maximum height of 6 ft		
				(S6)	Signs are restricted to corner stores only		
				(S7)	Signs are permitted at entrances to multi-family buildings		
		Location:					
		(L1)	On Beach Blvd.	(L6)	Between Ellis		
		(L2)	On Edinger Ave.	(L7)	North of Ellis		

2.2 Building Use Regulations	Standards
2.2.1 Use Types	
1) Retail	
a) Specialty Goods Anchors	---
b) Community Oriented Anchors	NC
c) Entertainment Anchors	---
d) Eating & Drinking Establishments	NC
e) Specialty Goods & Foods	NC (U3)
f) Entertainment & Recreation	conditional (C19)
g) Convenience Uses	NC
h) Business Services	NC
i) Personal Services	NC
j) Personal Enrichment Services	NC
k) Service Commercial & Repair	permitted (U8)
l) Large Scale Commercial Goods	---
m) Vehicle Sales	---
2) Civic & Cultural	conditional (C19)
3) Office	
a) Professional Services	permitted (C2)
b) Medical Services	permitted (C2)
4) Lodging	permitted (C2)
5) Live Work	permitted (C2 or C1/L10)
6) Residential	
a) Multi-Family w/ Common Entry	conditional (C2 or C1/ L9,C14,C15)
b) Multi Family w/ Individual Entries	conditional (C2 or C1/ L9,C14,C15)
c) Attached Single Family	---
d) Detached Single Family	---
2.2.2 Special Retail Configurations	
a) Neighborhood Center	required (L1 & L10)
b) Corner Store	n/a
c) Drive-through	permitted
2.2.3 Affordable Housing Requirement	
required or not required	required (U1)

2.3 Building Scale Regulations	Standards
2.3.1 Building Height	
minimum height	1 story
maximum height	4 stories
2.3.2 Special Building Height Limits	
Edinger/Beach/Main	4 story max (L1)
Across the Street From Housing	plus 2 story max
Adjacent to Housing	required
2.3.3 Building Length	
maximum	300 ft
2.3.4 Special Building Length Limits	
Limited Corner Building - maximum	120 ft (C10)
Limited Mid-Block Building - maximum	80 ft (C10)
2.3.5 Building Massing - Primary Volume Proportions	
Length : Height - Edinger/Beach/Main	3L:2H to 3L:1H
Length : Height - All other streets	2L:3H to 5L:2H

2.4 Frontage & Building Placement Regulations	Standards
2.4.1 Building Orientation to Streets & Public Open Space	
required or not required	required (A)
2.4.2 Private Frontage Types	
3) Private Frontage Specification	
a) Shop-Front	permitted
Shopfront Length - maximum	N/A
Tenant Length - maximum	N/A
Articulation Length - maximum	50 ft
b) Corner Entry	permitted
c) Arcade	permitted
d) Grand Portico	permitted
e) Forecourt	permitted
f) Common Lobby Entry	permitted
g) Stoop	permitted except (L1)
h) Porch	---
i) Front Door	---
j) Edge Treatment: Fenced	permitted except (L1)
k) Edge Treatment: Terraced	permitted except (L1)
l) Edge Treatment: Flush	permitted
2.4.3 Front Yard Setback	
minimum - Beach	30 ft (C16)(C17)
minimum - all other public streets	30 ft (C16)(C17)
upper story setback	Required (C13)
2.4.4 Side Yard Setback	
min w/ living space windows	10 ft
min w/out living space windows	5 ft
2.4.5 Rear Yard Setback	
minimum	10 ft
2.4.6 Alley Setback	
minimum	5 ft
2.4.7 Frontage Coverage	
minimum - Edinger/Beach/Main	50%
minimum - all other streets	50% (A)
2.4.8 Space Between Buildings	
minimum	20 ft
2.4.9 Build-to-Corner	
required or not required	required

2.5 Street Regulations	Standards
2.5.1 Improvements to Existing Streets	
4) Palm Tree Boulevard	
b) Neighborhood Center Streetfront	required (L1, L7)
5) Parkway	
c) Neighborhood Center w/ Palm	required (L1, L5, L6)
7) Neighborhood Streets	required except (L1)
2.5.2 Provision of New Streets - (see section 2.5.2)	
2.5.3 Block Size	
Maximum Block Size	3000 ft
Maximum Edinger Block Face	n/a
2.5.4 Street Connectivity	
required or not required	required
2.5.5 Required East-West Street Connection	
required or not required	n/a
2.5.6 Residential Transition-Boundary Street	
required or not required	n/a
2.5.7 Street Types (New Street Design)	
1) City Street	permitted
2) Public Open Space w/ City Street	permitted
3) Neighborhood Street	permitted
4) Public Open Space w/ Neighborhood St.	permitted
5) Alley	permitted



Development Standards Charts

2.6 Open Space Regulations	Standards
2.6.1 Provision of Public Open Space	
1) Retail	50 s.f. / 1000 s.f. (C8)
2) Civic & Cultural	n/a
3) Office	100 s.f. / 1000 s.f. (C8)
4) Lodging	30 s.f. / room (C9)
5) Live Work	50 s.f. / unit (C9)
6) Residential	50 s.f. / unit (C9)
2.6.2 Special Public Open Space	
	n/a
2.6.3 Provision of Private Open Space	
1) Retail	n/a
2) Civic & Cultural	n/a
3) Office	n/a
4) Lodging	n/a
5) Live Work	n/a
6) Residential	
a) Attached & Multi-Family	60 s.f. / unit
b) Detached Single-Family Homes	n/a
2.6.4 Public Open Space Types	
1) Park	---
2) Linear Green	---
3) Square	permitted
4) Plaza	permitted
5) Mid-Block Green	permitted
6) Courtyard Plaza	permitted
7) Passage/Paseo	permitted
8) Pocket Park/Playground	permitted
2.6.5 Private Open Space Types	
1) Courtyard	permitted
2) Private Yard	permitted
3) Porch	---
4) Rooftop Deck or Garden	permitted
5) Balcony	permitted
2.6.6 Stormwater Management Best Management Practices	
required or not required	required
2.6.7 Stormwater BMP Types	
Source Control BMPs	required
Site Design BMPs	required
Treatment Control BMPs	PP / N-PP
2.6.8 Open Space Landscaping	
required or not required	required
2.6.5 Setback Area Landscaping Types	
1) Perimeter Block Setback Areas	
a) Sidewalk Extension	permitted; required (L1)
b) Parkway Landscaping	---
c) Boulevard Landscaping	---
d) Neighborhood Street Landscaping	permitted
2) Interior Block Setback Areas	
a) Groundcover	required
b & c) Moderate or Heavy Screening	required

2.7 Parking Regulations	Standards
2.7.1 Provision of Parking	
1) Retail Anchors	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
2) Eating & Drinking Establishments	
spaces per 1000 s.f.	6 min / 10 max
location	within 200 feet
3) Specialty Foods / Goods	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
4) Entertainment & Recreation	
spaces per 1000 s.f.	6 min / 10 max
location	within 500 feet
5) Convenience Uses	
spaces per 1000 s.f.	3 min / 4 max
location	within 200 feet
6) Personal & Business Services	
spaces per 1000 s.f.	3 min / 4 max
location	within 200 feet
7) Presonl Enrichment Services	
spaces per 1000 s.f.	5 max (C11)
location	within 200 feet
7) Commercial Goods & Vehicle Sales	
spaces per 1000 s.f.	n/a
location	n/a
8) Civic & Cultural	
spaces per 1000 s.f.	3 min / 4 max
location	within 500 feet
9) Office - Professional	
spaces per 1000 s.f.	3 min / 4 max
location	within 200 feet
10) Office - Medical	
spaces per 1000 s.f.	4.5 min / 5.5 max
location	within 200 feet
11) Lodging	
spaces per guest room	1 min / 1.2 max
location	within 200 feet
12) Live-Work	
spaces per unit	1 min / 1 max
spaces per employee	0 min / 1 max
location	within 200 feet
13) Residential Uses	
spaces per studio & 1brunit	2 min
spaces per 2br unit	2 min
spaces per 3br+unit	2.5 min
guest spaces per 10 units	5 min (0.5 per unit)
location	on site
14) Neighborhood Center Exceptions	
spaces per 1000 s.f.	0 min / 3 max
15) Corner Store Exceptions	
spaces per 1000 s.f.	0 min
location	on street only
2.7.2 Parking Types	
1) Surface Lot - Front	---
2) Surface Lot - Side	permitted
3) Surface Lot - Rear	permitted
4) Surface Lot - Exposed	--- (A)
5) Structure - Exposed	---
6) Structure - Wrapped: Ground Level	---
7) Structure - Wrapped: All Levels	permitted
8) Partially Submerged Podium	permitted
9) Structure - Underground	permitted

2.8 Architecture Regulations	Standards
2.8.1 Facade Height Articulation Regulations	
Top	required
Base	required
2.9 Signage Regulations	Standards
2.9.1 Freestanding Sign Setbacks	(see section 2.9.1)
2.9.2 Total Sign Area	(see section 2.9.2)
2.9.3 Sign Type Regulations	
Grand Projecting Sign	---
maximum height	n/a
Marquee Sign	---
maximum number of faces	n/a
maximum area	n/a
Wall Sign	permitted
maximum size	200 s.f. (S1 & S5)
Monument Sign & Ground Sign	permitted
maximum number of faces	2
maximum height	6 ft
maximum area	48 s.f. (S1)
secondary sign	---
bonus sign	24 s.f.
Pole Mounted Sign & Tower Sign	permitted (L1, L2)
maximum number of faces	2
maximum height	15 ft
secondary sign	---
bonus sign	5 ft
maximum area	70 s.f. (S4)
secondary sign	---
bonus sign	30 s.f.
Projecting Sign	permitted
maximum area	8 s.f.
Awning Face Sign	permitted
maximum area	20 % of awning face
Awning Valance Sign	permitted
lines of lettering	1
letter height (whichever is less)	2/3 valance height : 8 in
Awning Side Sign	permitted
lines of lettering	1
letter height	8 in
Canopy Fascia Sign	permitted
maximum height (whichever is less)	2/3 fascia height : 12 in
maximum width	2/3 canopy width
lines of lettering	1
Above Canopy Sign	permitted
maximum height (whichever is less)	1 1/2 fascia height : 24 in
maximum width	2/3 canopy width
lines of lettering	1
Café Umbrella Sign	permitted
maximum area	10% of umbrella surface
Recessed Entry Sign	permitted
maximum area	20 s.f.
Window Sign	permitted
maximum area	20% of window (S3)

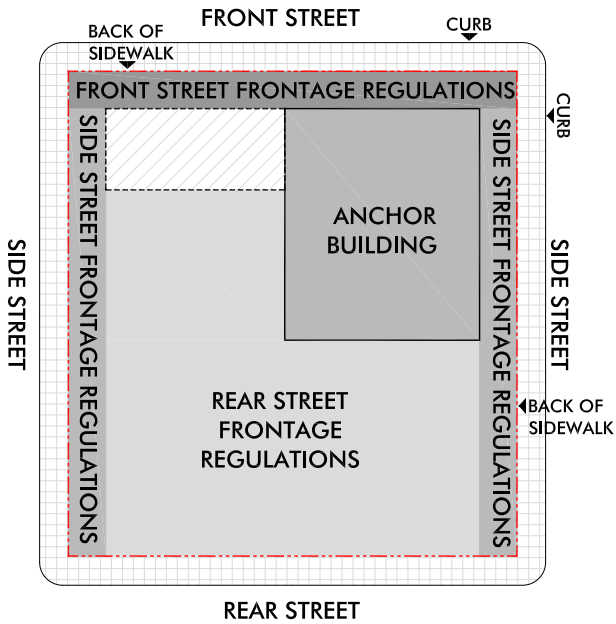
2.7 Parking Regulations Note: The maximum number of parking spaces permitted only applies to parking spaces that are provided in surface parking lots.

Special Requirements

a) Anchor Exceptions

Regulations listed in the Anchor Exceptions chart below may be applied to retail anchors exceeding 30,000 square feet as exceptions to the regulations indicated with an (A) in the Development Standards Charts. Anchor buildings shall have at least one “Front Street” that shall establish how these exceptions apply.

Beach Blvd. shall always be a Front Street. All streets that are not Front Streets are either Side Streets or Rear Streets as indicated in accompanying Anchor Buildings diagram.



Regulations	Front Street	Side Street	Rear Street
2.4.1 Building Orientation			
required or not required	required	not required	not required
2.4.4 Front Yard Setback			
maximum	no deviation	no maximum	no maximum
2.4.8 Frontage Coverage			
minimum	no deviation	20%	0%
2.7.2 Parking Types			
Surface Lot - Exposed	no deviation	permitted	permitted
Structure - Wrapped on Ground Level	permitted	permitted	permitted
Structure - Exposed	no deviation	permitted	permitted
2.8.2 Architectural Element Regulations			
Façade Composition	no deviation	No minimum glazing requirement along two selected street frontages	