

CITY OF SAN CARLOS

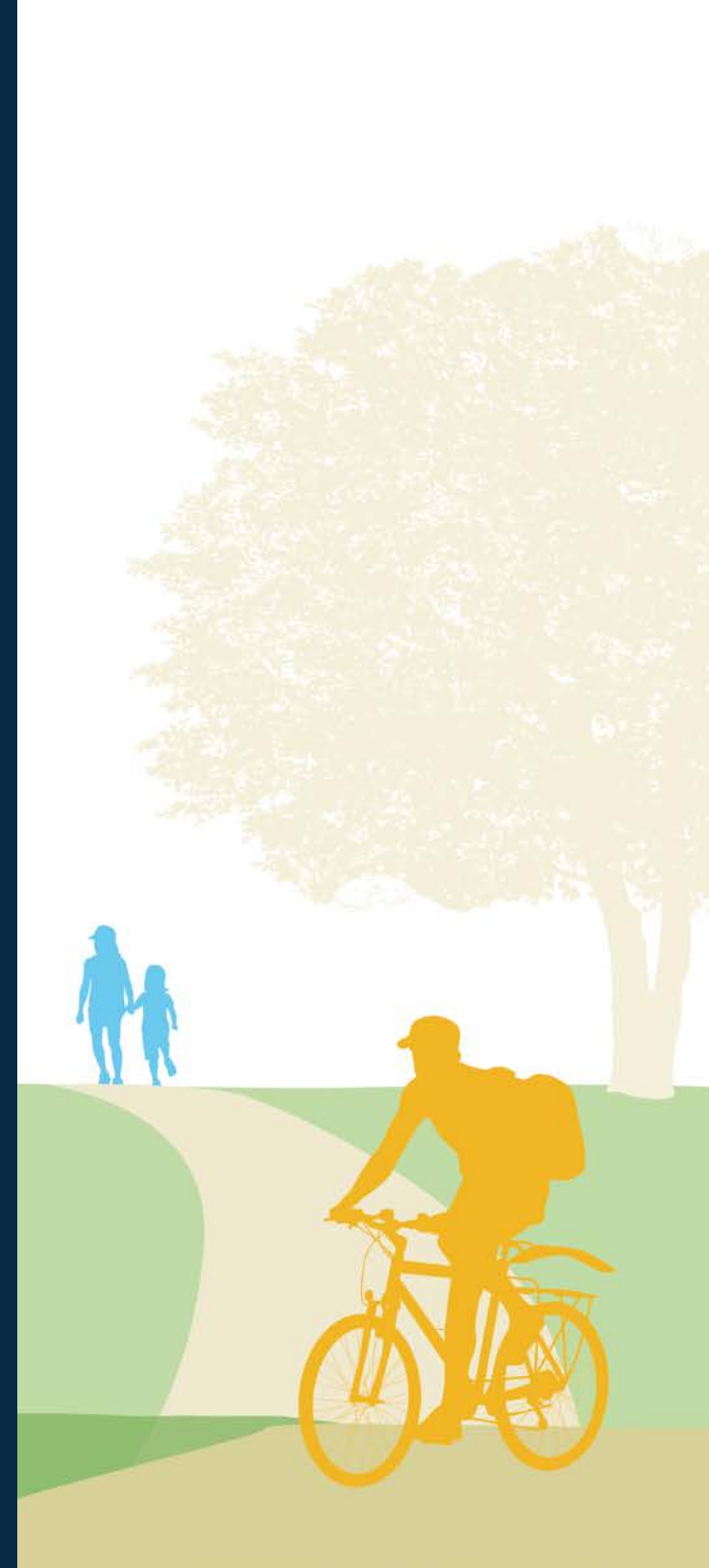
Bicycle and Pedestrian Master Plan

FINAL | ADOPTED JUNE 9, 2020

Prepared by
Alta Planning
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Prepared for

CITY OF
GOOD
LIVING
SAN CARLOS



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01.

Introduction

The City of San Carlos is committed to improving the quality of life for residents and visitors by providing walking and bicycling as convenient and comfortable modes of transportation and recreation. Located between San Francisco and Silicon Valley, San Carlos is a key connection within the broader network of bustling employment and recreation hubs that characterizes the Peninsula region.

The Bicycle and Pedestrian Master Plan is an essential tool for guiding city staff and the development community in building a balanced transportation system where active modes are supported and accessible. The ultimate goal of the plan is to promote walking and bicycling through the creation of safe, comfortable and connected networks, and to encourage alternatives to single-occupancy motor vehicle trips.

Purpose of the Plan

This Bicycle and Pedestrian Master Plan establishes a long-term vision for improving walking and bicycling in San Carlos and provides a strategy to develop a comprehensive bicycling and walking network that provides access to transit, schools, and downtown. This document also identifies a plan to implement these projects and programs through prioritization and phasing to ensure projects are manageable and fundable.

Benefits of Walking and Bicycling

Active modes of transportation such as walking and bicycling create lasting impacts on both individuals and the communities they live in. The development of a safe and comfortable physical environment that supports these modes has been shown to:

- Improve access to outdoor amenities and increase recreational opportunities
- Reduce the risk of bicycle and pedestrian involved collisions and injuries
- Provide affordable transportation options for low-income and disadvantaged residents
- Increase a city's livability and quality of life
- Decrease visual and noise pollution caused by automobiles
- Reduce greenhouse gas emissions into the atmosphere

Community Engagement Process

Engaging the San Carlos community has been a priority throughout the Plan process. A variety of outreach opportunities were used to seek input from residents and community members. The plan development process also included extensive coordination with partner agencies and other City departments to ensure this Plan meets community needs, advances initiatives of local and regional partners, and includes projects and programs that can feasibly be implemented. Refer to **Appendix C** for a summary of comments from community events.

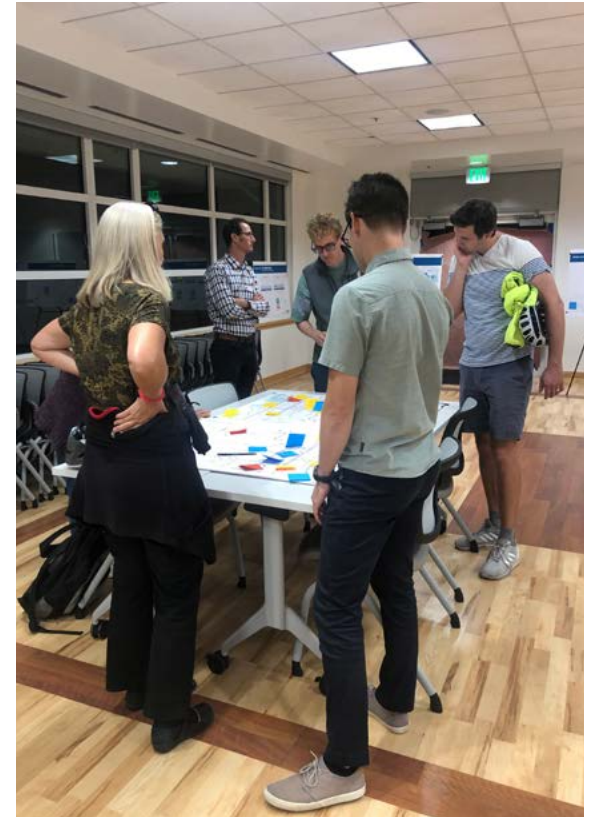
Community Workshops

Community Workshops 1 and 2

Two community workshops were held on February 21, 2019 and March 5, 2019 with over 70 community members in attendance. The workshops were setup as an open house with stations around the room for participants to rotate between at their own pace. Participants were able to review the existing walking and biking network, collision data, and provided comments on where they would like to see improvements to the walking and biking network in San Carlos.

Community Workshop 3

A third workshop was held on October 23, 2019. The workshop was attended by nearly 60 residents, who reviewed draft network maps for bicycling and walking improvements in San Carlos. Attendees provided feedback on the draft networks, which helped refine and prioritize the project list.



01. INTRODUCTION

Online Survey

An online survey was available on the City's website from November 28, 2018 to March 15, 2019. The survey received 380 responses, documenting responses to why people walk, bike, and take transit in San Carlos, and what barriers prevent people from doing so. While the U.S. census provides us with data on who walks and bikes to work, this survey provides a much fuller understanding of why people walk and bike in San Carlos.

The top reason people indicated they bike in San Carlos is for recreation and exercise, as reported by over half of the respondents. Over a quarter of respondents bike in San Carlos as part of a work trip, and also to shop, dine, and run errands. The top two barriers people identified to riding, or riding more, is dealing with aggressive drivers, and poor road conditions for bicycling.

Table 1: Identified concerns when biking around San Carlos

CONCERN	NUMBER	PERCENT
Dealing with aggressive drivers	58	54.21%
Poor road conditions or lack of bicycle facilities (i.e. bike lanes, shared lane markings, multi- use trails)	58	54.21%
Knowing a safe route to get where you are going	33	30.84%
Other	33	30.84%
Carrying the things you need with you	26	24.30%
The amount of time you have	26	24.30%
Transporting other people	19	17.76%
Having your bike stolen or vandalized	10	9.35%
Finding a bike that you can ride comfortably	10	9.35%
Putting your bike on a bus or a train	9	8.41%
Knowing the rules of the road for biking	2	1.87%

01. INTRODUCTION

Eighty percent of respondents walk in San Carlos for recreation, to shop, dine, and run errands. In contrast, a third of survey respondents indicated they walk as part of a trip to work or school. Around half of survey respondents highlighted the same top three barriers to walking: sidewalks in poor condition, challenging crossings, and too heavy and fast vehicle traffic.

Table 2: Identified concerns when walking around San Carlos

CONCERN	NUMBER	PERCENT
Sidewalks or paths are in poor condition	181	51.86%
Intersections and crossings are challenging	175	50.14%
Traffic is too heavy and fast	159	45.56%
Other	94	26.93%
Concern about personal safety or security	90	25.79%
The amount of time you have	77	22.06%
Carrying the things you need with you	57	16.33%
Transporting other people	35	10.03%

Community Input Map

An interactive mapping tool was posted on the City's website and used throughout development of this Plan to gather input and feedback from the community directly on a map of the city.

Early in the process, community members were encouraged to draw routes or place pins on the map and add comments to identify desired walking or bicycling improvements, challenging locations, and other information about the walking and bicycling environment. This input informed the recommended bicycling and walking network improvements.

Once the draft network recommendations were created, community members were invited to "like" or "dislike" projects to show their preferences, in addition to adding comments on specific projects and seeing the feedback left by others. This input helped to refine the network recommendations.

Pop-Up Events and Walking Tours

Three pop-up style events were held at the San Carlos Art and Wine Faire (October 4, 2018), Adult Community Center (October 12, 2018), and the Farmer's Market (November 4, 2018). Participants were asked to identify where they like to walk and bike and to identify locations where they would like to see bicycle and pedestrian improvements in San Carlos. In addition, two walking tours were conducted on March 5, 2019 and March 29, 2019 to discuss improvement to walking and biking facilities in San Carlos.

Commission and Council Meetings

Transportation and Circulation Commission

The project team met with the Transportation and Circulation Commission throughout the project. The first meeting was held on January 15, 2019 to provide an overview of the Plan and garner feedback on walking and biking in San Carlos. The second meeting was held on December 17, 2019 to share the draft network recommendations.

Economic Development Advisory Commission

The project team presented to the Economic Development Advisory Commission on September 24, 2019 to share the draft network recommendations.

City Council

A meeting with City Council was held on June 9, 2020 to adopt the Plan.

Plan Organization

The Plan is organized as follows:

Chapter 1: Introduction outlines the Plan’s purpose, community engagement process, and provides an overview of the Plan’s structure.

Chapter 2: Vision, Goals, Objectives, and Policies captures the vision and policy framework for the Bicycle and Pedestrian Plan.

Chapter 3: San Carlos Today provides an inventory of existing walking and bicycling facilities in San Carlos, as well as documents user experience and comfort of those facilities.

Chapter 4: Recommendations describes the specific projects, programs, and policy changes recommended to meet the active transportation needs of San Carlos.

Chapter 5: Implementation provides a strategy to evaluate and prioritize projects and provide details on funding opportunities to implement the Plan.

Appendix A. Design Guidelines identifies design recommendations for pedestrian and bicycle treatments throughout the city.

Appendix B. Funding Sources provides additional details on the potential funding sources described in Chapter 5.

Appendix C. Community Engagement includes written comments received from the community.

01. INTRODUCTION

02.

Vision, Goals, Objectives, and Policies

02. VISION, GOALS, OBJECTIVES, AND POLICIES

The San Carlos Bicycle and Pedestrian Master Plan is organized around a Vision Statement, three overarching goals, and a series of specific objectives and policies.



Vision

A statement that serves as an aspirational guide.



Goals

Broad, long-range targets for making the vision a reality.



Objectives

The specific outcomes we want to achieve.



Policies

Specific strategies for how to achieve the goals and objectives.

Vision

San Carlos is a community where people of all ages and abilities can comfortably and conveniently walk or ride a bicycle.

Goals, Objectives, and Policies

GOAL 1:

Maintain and Expand the Pedestrian and Bicycle Network

Maintain a complete and convenient network for walking and biking in San Carlos with connections to the region.

Objective 1.A:

Plan, design, and construct a complete pedestrian and bicycle network that accommodates the needs of all mobility types, users, and ability levels.

Policy 1.A.1: *Obtain funding for all high priority project recommendations by 2026.*

Policy 1.A.2: *Implement the recommendations for pedestrian and bicycle signage from the Citywide Wayfinding Study.*

Policy 1.A.3: *Coordinate implementation of the Peninsula Bikeway with neighboring jurisdictions.*

Objective 1.B:

Coordinate bicycle and pedestrian needs with other street and infrastructure investments.

Policy 1.B.1: *Incorporate Green Streets/Green Infrastructure best practices, as appropriate to the context, for new*

streets and street retrofits, to enhance the pedestrian and bicyclist experience and reduce the impacts of development on storm water resources and enhance the natural environment.

Policy 1.B.2 *Evaluate all streets during pavement resurfacing projects to determine if bicycle and pedestrian facilities can be provided (e.g. bike lanes, wider curb lanes or shoulders, crosswalk upgrades) when the striping is reapplied.*

Policy 1.B.3 *Work with transit providers to improve bicycle and pedestrian access (first/last mile connections) to transit stations and improve the comfort of transit stops by providing secure bike parking, benches, and covered waiting areas at stations and stops.*

Policy 1.B.4 *Work with adjacent governmental entities, the County of San Mateo, public service companies, and transit agencies to ensure that Plan recommendations are incorporated into their planning and areas of responsibility, and vice versa.*

GOAL 2:

Increase Support for Walking and Bicycling

Increase awareness and support of walking and bicycling through programs and citywide initiatives.

Objective 2.A:

Encourage more people to walk and bicycle in San Carlos as part of their daily routine.

Policy 2.A.1: *Continue to support the County of San Mateo Safe Routes to School program.*

Policy 2.A.2: *Explore opportunities for implementing a bicycle and/or scooter share program within San Carlos. Using lessons learned from other jurisdictions, develop policies to ensure safe use and accountability.*

Policy 2.A.2: *Encourage local community input in the planning and implementation of significant bike-ways and other pedestrian or bicycle related improvements by holding public meetings and workshops within the neighborhood where the project will be implemented.*

Policy 2.A.3: *Develop and distribute a citywide bicycle route map.*

Policy 2.A.4: *Renew Bicycle Friendly Community (BFC) Application through the League of American Bicyclists for Fall 2020. Strive to improve the city's BFC status by 2026.*

GOAL 3:

Improve Access and Safety for Pedestrians and Bicyclists

Design roadways that are accessible and comfortable for people of all ages and abilities to walk and bicycle.

Objective 3.A:

Design a Low Stress Bikeway Network suitable for all ages and ability levels riding bicycles.

Policy 3.A.1: *Design a network of continuous Low Stress Bike-ways as identified in this Master Plan. Projects that improve comfort at intersections and along corridors with high stress should be prioritized.*

Policy 3.A.2: *Prioritize the installation of bicycle parking in the public right-of-way including parks, downtown, and public facilities.*

Policy 3.A.3: *Work with private property owners to install bike parking in key commercial destinations.*

Objective 3.B:

Consider implementing a Vision Zero policy before 2024.

Policy 3.B.1: *Annually review the number, locations, and contributing factors of pedestrian-related and bicycling-related collisions to identify and implement ongoing improvements at key locations throughout the transportation network.*

Policy 3.B.2: *Identify opportunities to reduce exposure for people bicycling by reducing crossing distances or providing dedicated facilities.*

Policy 3.B.3: *Study the need to adopt school zone speed limits as low as 15 MPH.*

02. VISION, GOALS, OBJECTIVES, AND POLICIES

03.

San Carlos Today

Introduction

San Carlos contains 5.5 square miles of land situated between Belmont and Redwood City on the San Francisco Bay Peninsula. The relatively small area allows the City to examine the fine grain bicycle and walking network connecting residents to the city's parks and trails, retail and commercial districts, and regional transit centers. San Carlos' proximity to regional transportation corridors, such as the San Francisco Bay Trail, Caltrain, and El Camino Real, serve as active and shared transportation assets for residents and visitors to San Carlos. The population of San Carlos is nearly 30,000 people and will grow to 33,700 by 2030¹.

This chapter include the following sections:

- Walking and Biking Today describes the existing pedestrian and bicycle network in San Carlos, transit access, and demographics.
- User Experience and Perceived Comfort analyzes the level of traffic stress on roadways for pedestrians and bicyclists.
- Safety analyzes the data on bicycle- and pedestrian-related collisions in San Carlos.



¹ Association of Bay Area Governments (ABAG)

Walking and Biking Today

Existing Pedestrian Network

San Carlos has 63 miles of roadway with sidewalks on both side of the street and 22 miles of roadway with sidewalks on one side of the street – covering respectively 65% and 22% of roadways in San Carlos (see inventory in **Table 3**). The city’s steeper street grades, specifically the roadways in the north western section by Arguello Park and Arundel Elementary, generally are lacking sidewalks, as shown in **Figure 1**. Public feedback indicates that people walking often have trouble navigating areas where sidewalks end, exist on only one side of the street, or switch sides of the street. For example, San Carlos Avenue and Brittan Avenue are both collector streets that provide pedestrian connections to downtown and other nearby destinations, but have areas of partial sidewalk that can be challenging to navigate.

A Pedestrian Safety Assessment conducted for the City of San Carlos in 2010 identified key strengths, enhancement areas, and opportunity areas. Some of the key strengths included the City’s comprehensive enforcement policies, extensive public involvement processes, and an existing inventory of sidewalks and pedestrian opportunity areas. Areas that the Assessment identified the City could improve or add included:

- Adoption of Routine Accommodations for New Development
- Preparation of a Pedestrian Master Plan
- Safe-Routes-to-School Program and Grant Funding
- Traffic Calming Programs
- Collision History and Collision Reporting Practices
- Pedestrian-Oriented Speed Limits and Speed Surveys

- Pedestrian-Oriented Traffic Signal and Stop Sign Warrants
- Updated ADA Transition Plan for Streets and Sidewalks
- Pedestrian Safety Program
- Collection of Pedestrian Volumes
- Pedestrian/Bicycle Coordinator
- Use of Leading Pedestrian Intervals

Table 3: Roadways with Sidewalks

FACILITY TYPE	MILEAGE
Full Sidewalks	63.0
Partial Sidewalks	21.6
No Sidewalk	12.5
Total	97.1

03. SAN CARLOS TODAY

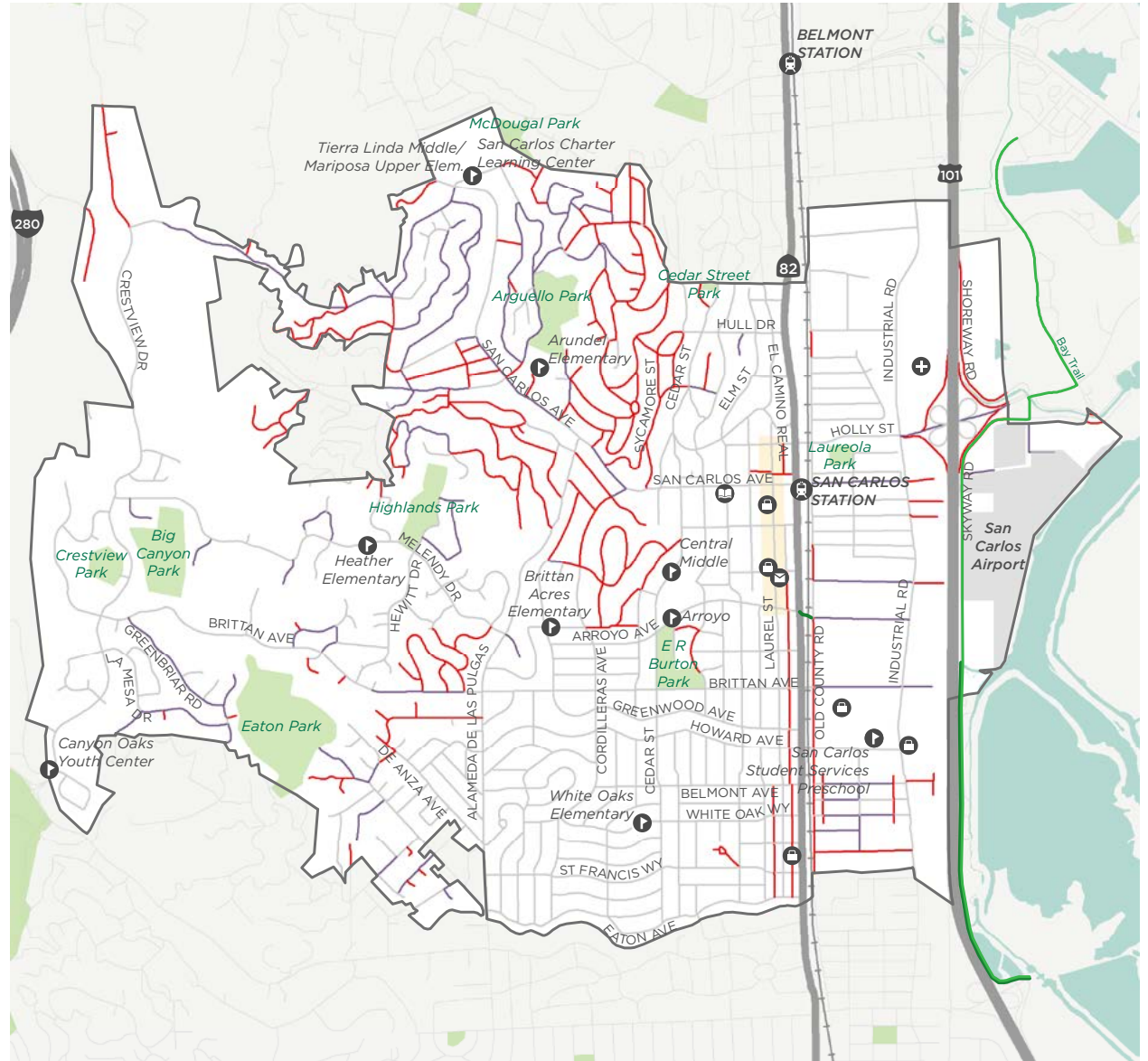
Figure 1:
Sidewalk Network

- Full Sidewalk
- Partial Sidewalk
- No Sidewalk
- Class I Shared-Use Path

Destinations + Boundaries

- Post Office
- Shopping
- Hospital
- Library
- School
- Caltrain Station
- Bay Trail
- Downtown
- Park
- San Carlos City Limit

0 0.25 0.5 MILES



Existing Bicycle Network

The California Department of Transportation (Caltrans) designates four classes of bicycle facilities: Class I shared use paths, Class II bicycle lanes, Class III bicycle routes, and Class IV separated bikeways. San Carlos has over 20 miles of existing bikeway facilities, including 4.1 miles of shared use path, 10.8 miles of Class II bicycle lanes, and 7.1 miles of Class III Bicycle Routes (see inventory in **Table 4 and Figure 2**). North-south bikeways include bicycle lanes on Alameda de las Pulgas, Old County Road, and Industrial Road and a bike route on Cedar Street. Intermittent bikeways on San Carlos Avenue, Brittan Avenue, and Arroyo Street provide East-West connections. San Carlos currently has no separated bikeways (Class IV).

Bike Parking

Bicycle travel requires a network of supportive amenities to provide bike storage and maintenance options. The 2012 Bicycle Plan identified the need for a city-wide bicycle parking siting plan. In 2015, the City added six bicycle racks in Downtown. In addition, the San Carlos Caltrain Station has 36 bike racks and 48 bicycle keyed lockers managed by Caltrain.

New development projects within San Carlos are required to provide bicycle parking. The number and type of support facility required can vary by land use type and size. The specific requirements are defined in Chapter 18.20 of San Carlos' Municipal Code.



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Table 4: Existing Bikeways and Mileage by Type







FACILITY TYPE		MILES
<p>Class I Shared Use Paths</p> <p>Shared use paths are completely separated from the street. They allow two-way travel by people bicycling and walking, and are among the most comfortable facilities for children and inexperienced riders as there are few potential conflicts between people bicycling and people driving.</p>		4.1
<p>Class II Bicycle Lanes</p> <p>Bicycle lanes are striped preferential lanes on the roadway for one-way bicycle travel. Some bicycle lanes include a striped buffer on one or both sides to increase separation from the traffic lane or from parked cars.</p>		10.8
<p>Class III Bicycle Routes</p> <p>Bicycle routes are signed where people bicycling share a travel lane with people driving. Because they are shared facilities, bicycle routes are most appropriate for low-speed and low-volume streets. Some Class III bicycle routes include shared lane markings or “sharrows” that recommend proper bicycle positioning in the center of the travel lane and alert drivers that bicyclists may be present. Bike Boulevards (Class IIIB) are bike routes on streets that prioritize through trips for bicyclists</p>		7.1
<p>Class IV Separated Bikeways</p> <p>Separated bikeways are on-street bicycle facilities that are physically separated from motor vehicle traffic by a vertical element or barrier, such as a curb, bollards, or vehicle parking aisle. They can allow for one-or two-way travel on one or both sides of the roadway.</p>		0.0
<p>Total</p>		22.0

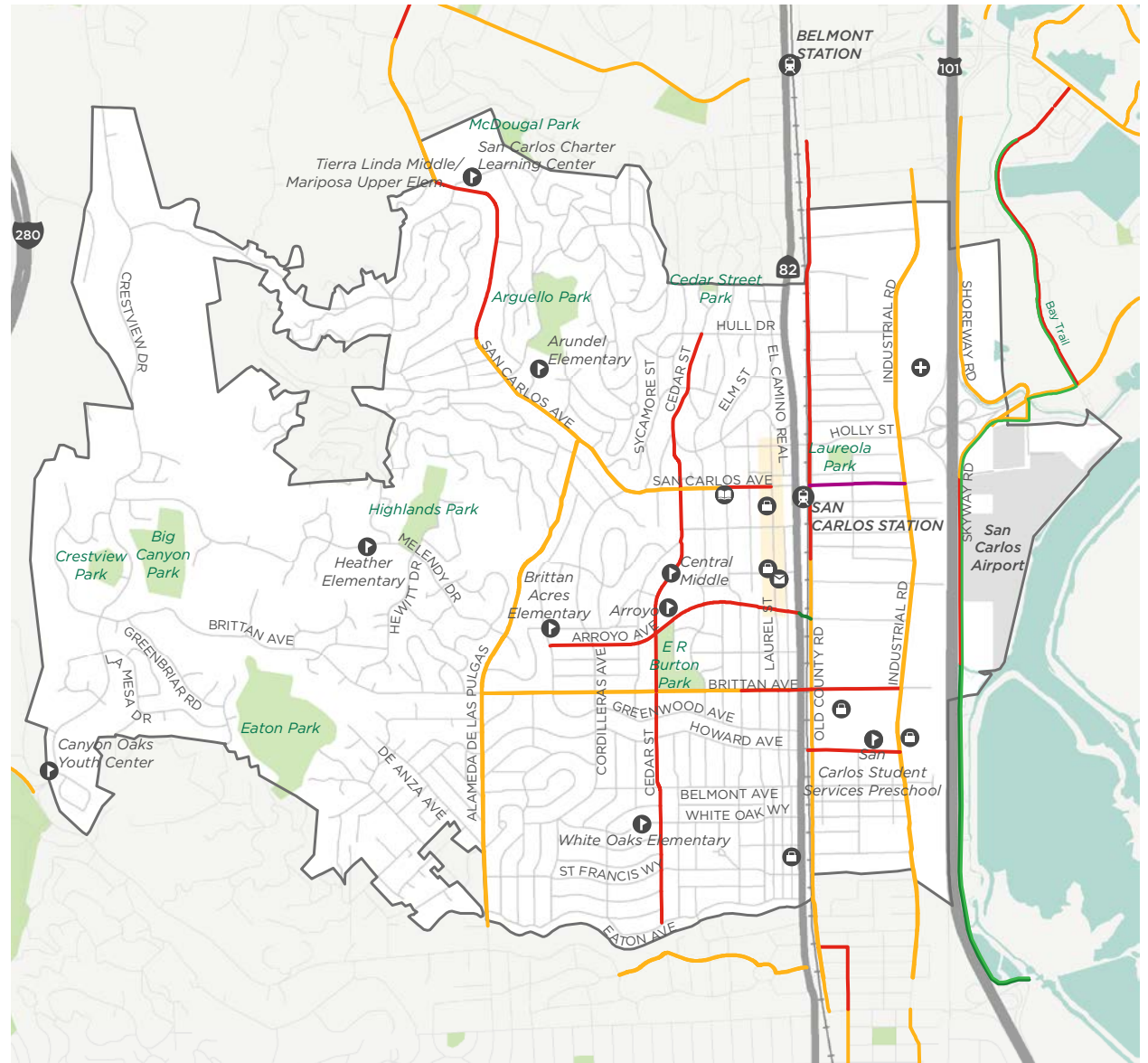
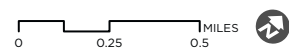
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Figure 2:
Existing Bikeways

- Class I Shared-Use Path
- Class II Bicycle Lane
- Class IIIB Bicycle Boulevard
- Class III Bicycle Route

**Destinations +
Boundaries**

-  Post Office
-  Shopping
-  Hospital
-  Library
-  School
-  Caltrain Station
- Bay Trail
- Downtown
- Park
- San Carlos City Limit



Transit and Other Facilities

Cities across California are exploring how improvements to transit services and surrounding roadway infrastructure can make it easier for people to walk and bike to their local transit stop or station. Often called “Safe Routes to Transit” these programs acknowledge that people often walk and bike to transit stops, and efforts to fund station and access improvements supports more vulnerable users without reliable access to automobiles. The San Carlos Caltrain Station and the recently developed San Carlos Transit Station provide important transit hubs for residents to reach schools, shopping, and employment areas across the Bay Area. The Transit Center project is being developed to improve connections between SamTrans fixed-route bus services, San Carlos Caltrain, local shuttles, and people walking and biking. As shown in **Figure 3**, SamTrans bus routes in San Carlos include:

- The 397, 398, ECR, and RAPID lines provide connections to stops along El Camino Real, connecting cities along the Peninsula from Daly City to Palo Alto.
- Route 61 is a local route that connects the San Carlos Caltrain Station with a stop at Carlmont High School and near other schools on San Carlos Avenue, Melendy Avenue, and Brittan Avenue.

- Route 295 connections San Mateo, Hillsdale, San Carlos, and Redwood Caltrain Stations through Alameda de las Pulgas.
- Route 260 connects the Belmont and San Carlos Caltrain Station and provides a connection across Highway 101 to Redwood Shores.

All SamTrans buses are equipped with front-loading bicycle racks holding two bicycles, and two additional bicycles are allowed inside the bus, depending on the passenger load. The SamTrans Short-Range Transit Plan (2017-2026) indicates that all SamTrans coach buses are being retrofitted with new racks that hold three bicycles.

Caltrain has reported that the average weekday passenger boarding for the San Carlos Caltrain Station is 1,331 for 2018. The average mid-week bicycle boarding is 140 bikes per day making San Carlos Station rank 14th out of the 29 stations in the Caltrain system. Weekend bicycle counts were even higher with 252 bike boarding on Saturday and 142 bike boarding on Sunday.

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Figure 3:
Transit Network

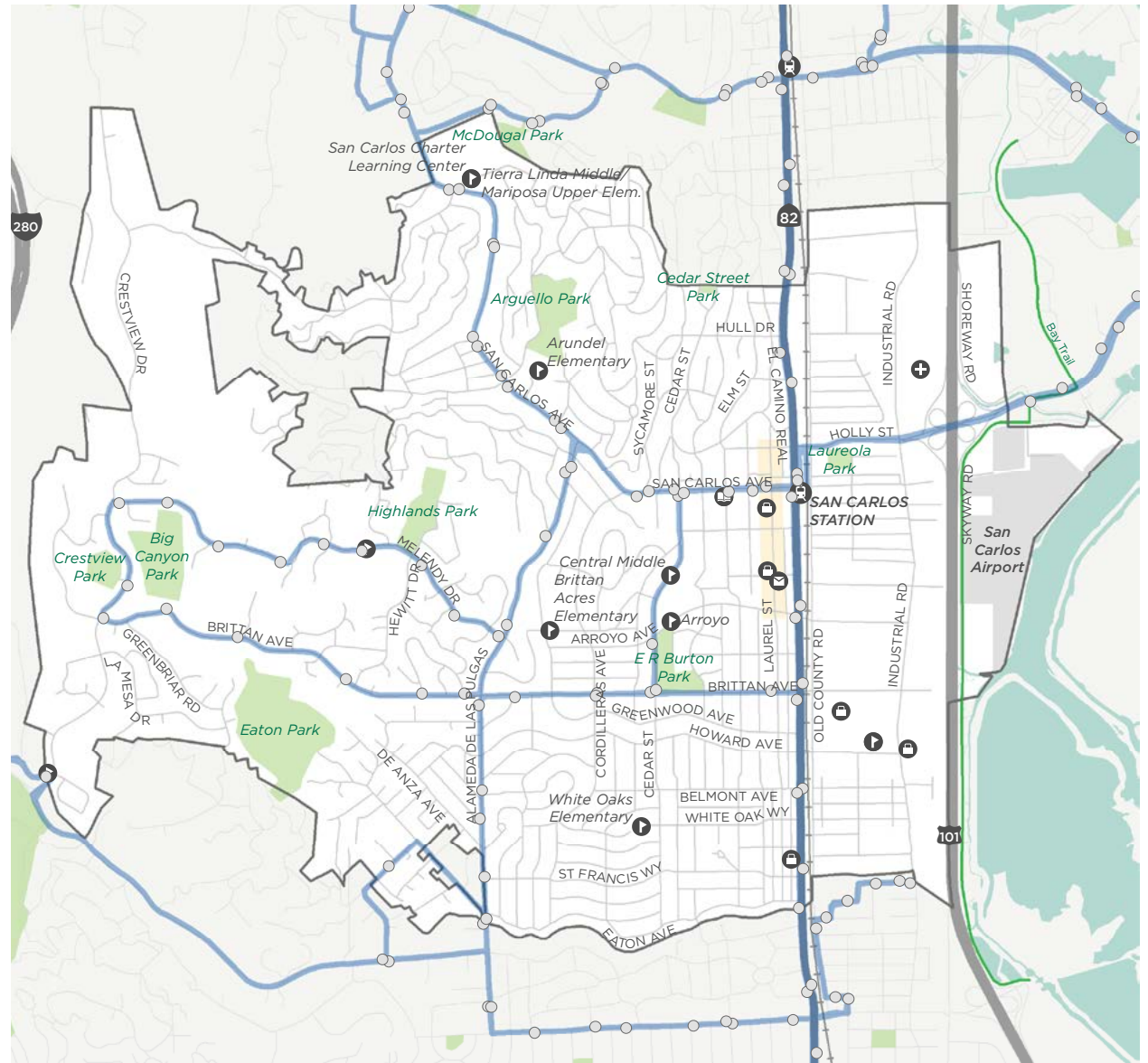
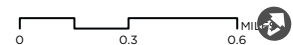
SamTrans Bus Network

- Stop
- Route

Destinations + Boundaries

- ✉ Post Office
- 🛒 Shopping
- 🏥 Hospital
- 📖 Library
- 🎓 School
- 🚆 Caltrain Station

- Bay Trail
- Downtown
- Park
- San Carlos City Limit



03. SAN CARLOS TODAY

Land Use & Major Destinations

San Carlos is primarily comprised of single-family homes (60% of land), and quaint neighborhood streets are a defining character of much of the City. Within the last two decades, some multifamily housing has been constructed in the Downtown area and along Laurel Street and the El Camino Real corridor north of Holly Street and south of Arroyo Street. Multifamily housing has also been concentrated along the southern edge of the Devonshire Area and along the western boundary of the city. The City's most recent Housing Element, adopted in 2015, identifies the capacity for 596 new residential units. Based on the identified underutilized parcels, much of this development will occur along El Camino Real, Laurel Street, Old County Road, San Carlos Avenue, and nearby roads. The ongoing proposal for multifamily housing as part of the Black Mountain proposal is anticipated to bring 69 townhomes off of Melendy Drive. Through the Bicycle and Pedestrian Master Plan effort, the City of San Carlos can strategize how this anticipated development can be paired with bicycle and pedestrian improvements in these areas.

Within San Carlos, there are a number of activity generators that have the potential to generate significant demand for walking and biking. These include:

Parks and Recreation Areas

- Arguello Park
- Bay Trail
- Big Canyon Park (including hiking trails)
- Burton Park
- Cedar Street Park
- Crestview Park (including hiking trail)
- Eaton Park (including hiking trails)
- Highlands Park
- Laureola Park
- McDougal Park

Commercial Areas and Corridors

- San Carlos Downtown Area
- San Carlos Market Place

Employment Areas

- San Carlos Downtown Area

Schools

- Tierra Linda Middle School
- Central Middle School
- White Oaks Elementary
- Heather Elementary
- Arroyo Elementary
- San Carlos Student Services Preschool
- Brittan Acres Elementary
- Mariposa Elementary
- Arundel Elementary
- San Carlos Charter Learning Center

Civic Buildings

- San Carlos City Hall and Library
- San Carlos Adult Community Center
- San Carlos Youth Center

Medical Facilities

- Palo Alto Medical Foundation

Transit

- San Carlos Caltrain Station

Demographics

San Carlos is home to just under 30,000 residents (29,954), according to the 2017 American Community Survey five-year estimates. Two thirds of households are families, and a third of households have children under the age of 18. As seen in **Table 5**, residents identify predominantly as white (77%), with 15% of residents identifying as Asian. Ten percent of residents identify as Hispanic or Latino. The average household income is \$152,521, significantly higher than the countywide average of \$105,667.

Table 5: Demographic Comparison to San Mateo County
Source: ACS 2017 5-year estimates

RACE	SAN CARLOS	SAN MATEO COUNTY
White	77.3%	52.1%
Black or African American	0.6%	2.4%
American Indian and Alaska Native	0.1%	0.3%
Asian	14.8%	27.6%
Native Hawaiian and Other Pacific Islander	0.3%	1.4%
Some other race	1.9%	10.8%
Two or more races	5.1%	5.3%
Hispanic or Latino	10.2%	24.9%

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Commuting Modes

Based on journey to work data from the 2017 ACS 5-year data, two percent of residents walk to work and one percent of residents bike to work (see **Table 6**). This represents approximately 300 daily work-based bicycle trips and 600 daily work-based walking trips. The census data does not account for commuters with multiple modes of travel to and from work, such as a commuter that may ride a bicycle to the Caltrain Station before transferring to transit; for this response, the trip would be counted as a transit trip.

As shown in **Figure 4**, almost an equal number of people come to and leave San Carlos for their primary jobs, about 13,000 in each direction. Just over a thousand people live and work in San Carlos.

As part of the Bicycle and Pedestrian Master Plan process an online survey was conducted from December 2018 to March 2019 to get a better understanding of where people bike and walk in San Carlos. As shown in **Table 7**, 81% of survey respondents indicate they walk to shop, dine, or run errands and 35% of survey respondents bike to these destinations. In addition, 81% and 52% of survey respondents are walking and biking for recreation or exercise, respectively.

Table 6: Employment Inflow/ Outflow for San Carlos

Source: 2017 ACS 5-year estimates

MODE (HOME-BASED WORK TRIPS)	2010	2017
Drive Alone	79.7%	76.0%
Carpool	6.9%	5.5%
Public Transit	4.8%	7.3%
Bicycle	--	1.1%
Walk	2.2%	1.9%
Work at Home	4.6%	7.4%
Other	1.8%	2.0%

Table 7: Reasons People Bike or Walk in San Carlos

Source: San Carlos Bicycle and Pedestrian Master Plan survey, 2019

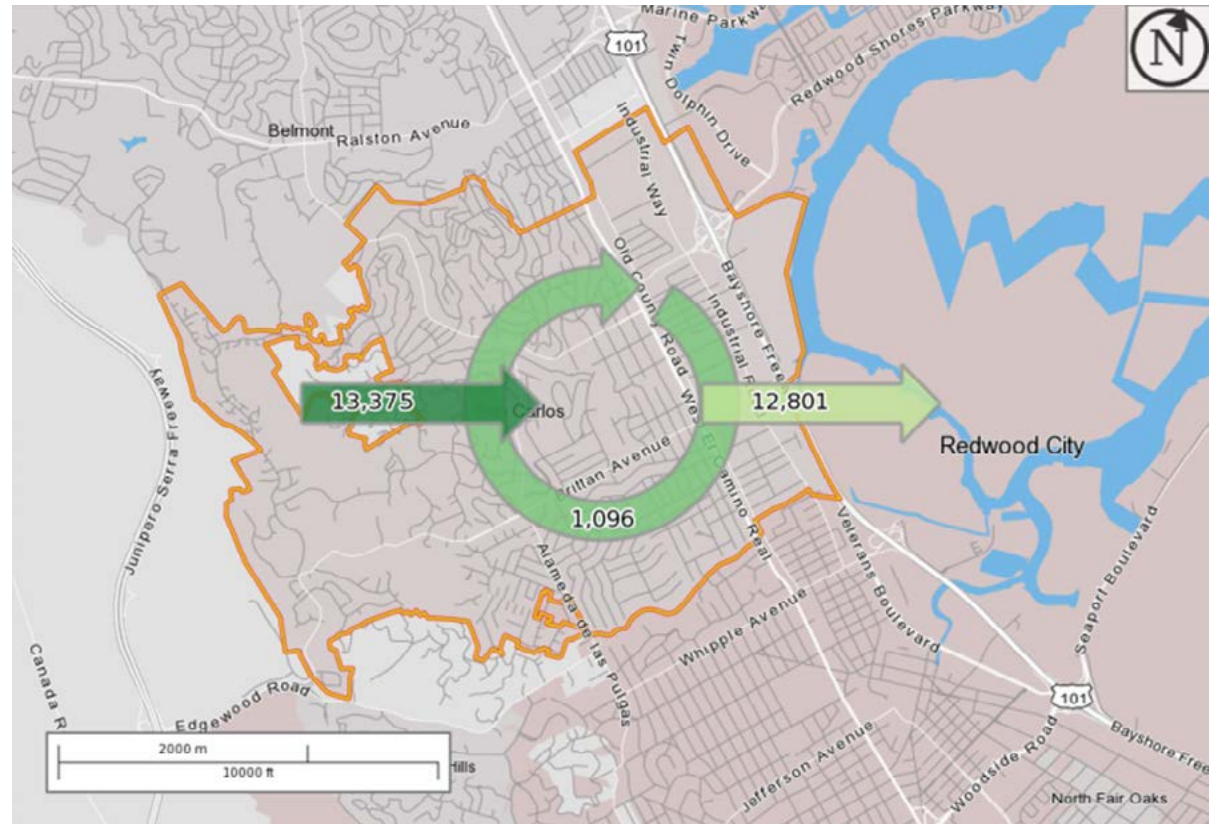
REASON	WALK		BIKE	
	#	%	#	%
As part of a trip to work	126	34%	95	28%
To shop, dine out, run errands, visit people, go to an event, or similar activity	301	81%	118	35%
For recreation or exercise	300	81%	173	52%
Other	29	8%	20	6%

*Does not include responses from people who indicated they do not currently walk or bike in San Carlos

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Figure 4:
Employment Inflow/
Outflow for San Carlos

Source: OntheMap, US Census Bureau



User Experience and Perceived Comfort

Bicycle Level of Traffic Stress

Traffic stress is the perceived sense of danger associated with riding in or adjacent to vehicle traffic. Studies have shown that traffic stress is one of the greatest deterrents to bicycling. The less stressful—and therefore more comfortable—a bicycle facility is, the wider its appeal to a broader segment of the population. A bicycle network will attract a large portion of the population if it is designed to reduce conflicts with motor vehicles and if it connects people with where they want to go. Streets are considered low stress if they have relatively low vehicle speeds and volumes (e.g., a shared, low-traffic neighborhood street) or if there is greater physical separation between the bikeway and traffic lane on roadways with higher traffic volumes and speeds (e.g., a separated bikeway on a major street).

Types of Bicyclists

Research indicates that the majority of people in the United States (56-73%) would bicycle if dedicated bicycle facilities were provided. However, only a small percentage of Americans (1-3%) are willing to ride if no facilities are provided. This research into how people perceive bicycling as a transportation choice has indicated that most people fall into one of four categories, illustrated below.



1%-3% STRONG & FEARLESS

Very comfortable and willing to ride on streets without designated facilities



5%-10% ENTHUSIASTIC & CONFIDENT

Very comfortable, but prefer streets with designated bike lanes



50%-60% INTERESTED, BUT CONCERNED

Comfortable on trails and streets with buffered or separated bike lanes and interested in biking more



30% NOT CURRENTLY INTERESTED

Physically unable or very uncomfortable even on streets with separated bike lanes

Level of Traffic Stress Classification

To better meet the needs of the “Interested, But Concerned” cyclist, planners developed the Bicycle Level of Traffic Stress (BLTS) analysis as an objective, data-driven evaluation model to help identify streets with high levels of traffic stress.

The BLTS analysis quantifies stress levels when riding along a given roadway based on several roadway characteristics including: number of lanes, posted speed limit, existing bike facilities, street width, elevation change, and others. The outputs can also be codified to correspond to different types of bicyclists:

- **All ages and abilities (BLTS 1)**
Level 1 includes off-street shared use paths and some very low-stress roadways suitable for all ages and abilities.
- **Average adult (BLTS 2)**
Level 2 includes roadways that are comfortable enough that the mainstream adult population would ride a bicycle on them.
- **Confident adult (BLTS 3)**
Level 3 includes arterial roadways with bicycle facilities that are probably only comfortable for an experienced, confident bicyclist.
- **Strong and fearless (BLTS 4)**
Level 4 includes arterial roadways with no or minimal bicycle facilities ridden only by strong or fearless bicyclists.

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The lower the number, the lower the stress and the higher the level of comfort for people on bicycles. BLTS 1 and 2 roads are typically the roadways that appeal to the “Interested, but Concerned” cyclists.

Neighborhood streets carry relatively little vehicular traffic, at typically slower speeds and are generally considered BLTS 1 or 2. Multi-use trails, like the Bay Trail, are also considered BLTS 1. Collector and arterial streets without bicycle facilities are generally considered BLTS 3. Terrain, high vehicle speeds, curves, and other difficult environmental conditions also raise BLTS, while the presence of bicycle lanes reduce the BLTS. **Figure 5** displays the BLTS classifications for San Carlos. El Camino Real has BLTS 4, indicative of the high speeds and multiple travel lanes that makes it uncomfortable to people biking. Brittan Avenue, San Carlos Avenue, Old County Road, and Industrial Road are classified as BLTS 3.

Pedestrian Level of Traffic Stress

Pedestrian connectivity and access to amenities is measured with the understanding that the quality of a pedestrian’s experience is influenced by a number of physical factors including elements like presence (or lack of) sidewalks, the type of roadway, the speed of traffic, elevation change, and the presence of pedestrian amenities. Similar to bicycles, roads that have lower traffic speeds and volumes, or greater separation from traffic, will provide a more comfortable experience for pedestrians.

These factors are aggregated into a single Pedestrian Level of Traffic Stress (PLTS) score. These scores range from PLTS 1 to PLTS 4, with PLTS 1 being the least stressful and PLTS 4 being the most stressful.

Figure 6 shows the PLTS classification for San Carlos. Crosstown connectors, such as San Carlos Avenue, Industrial Avenue, Old County Road, and Brittan Avenue are classified as PLTS 3, due to their higher traffic volumes and roadway class. With high speeds and multiple lanes of travel, El Camino Real has PLTS 4.

1. Roger Geller, City of Portland Bureau of Transportation. *Four Types of Cyclists*. <http://www.portlandonline.com/transportation/index.cfm?a=237507>. 2009; 2 Dill, J., McNeil, N. *Four Types of Cyclists? Testing a Typology to Better Understand Bicycling Behavior and Potential*. 2012.

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Figure 5:
Bicycle Level of
Traffic Stress (BLTS)

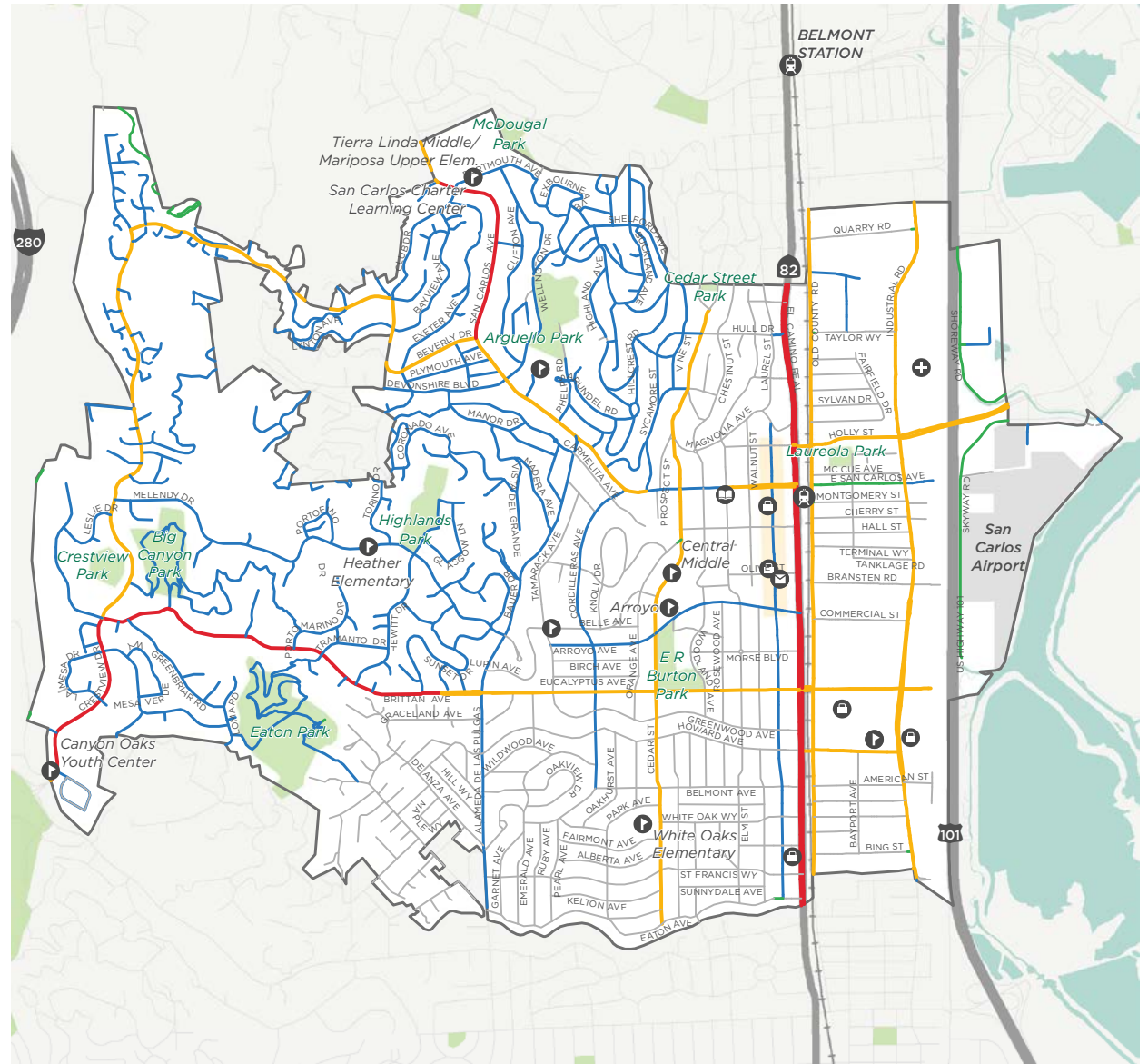
BLTS Score

- Level 1 All Ages and Abilities
- Level 1 All Ages and Abilities (Residential)
- Level 2 Average Adult
- Level 3 Confident Adult
- Level 4 Fearless Adult

Destinations + Boundaries

- Post Office
- Shopping
- Hospital
- Library
- School
- Caltrain Station
- Downtown
- Park
- San Carlos City Limit

0 0.25 0.5 MILES



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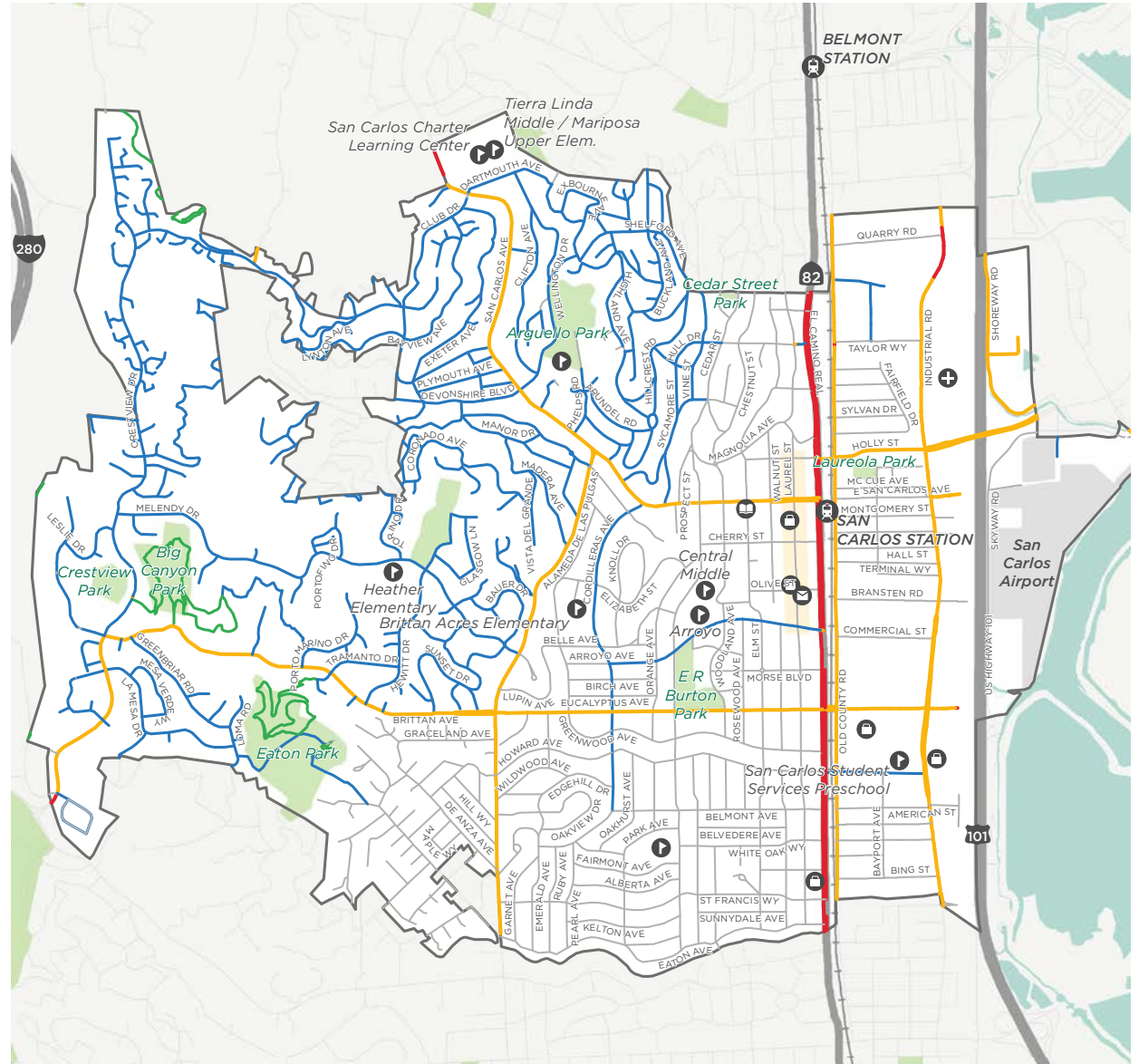
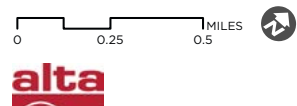
Figure 6:
Pedestrian Level of
Traffic Stress (PLTS)

PLTS Score

- Level 1 All Ages and Abilities
- Level 1 All Ages and Abilities (Residential)
- Level 2 Average Adult
- Level 3 Confidant Adult
- Level 4 Fearless Adult

Destinations + Boundaries

- Post Office
- Shopping
- Hospital
- Library
- School
- Caltrain Station
- Downtown
- Park
- San Carlos City Limit



Safety

Data on bicycle- and pedestrian-involved collisions can provide insight into locations or roadway features that tend to have higher collision rates, as well as behaviors and other factors that contribute to collisions. These insights inform the recommendations to address safety challenges facing people bicycling and walking.

Figure 7 show the location of bicycle-involved collisions and **Figure 8** shows pedestrian collisions within San Carlos between 2013 and 2017.




Between 2013 and 2017 there were 40 pedestrian-involved collisions and 35 bicycle-involved collisions, 75 in total (see **Table 8**). These collisions were concentrated along El Camino Real, Laurel Street, Industrial Road, Arroyo Avenue, and San Carlos Avenue. There were three pedestrian fatalities during the analysis period occurring at the intersections of El Camino Real and Belmont Avenue, San Carlos Avenue and Arundel Road, and on Highway 101 near the San Carlos Airport.

Table 8: Collision Summary (2013-2017)
2013-2017 Transportation Injury Mapping System, UC Berkeley










COLLISION TYPE	TOTAL COLLISIONS	FATALITIES	SEVERE INJURIES	MINOR INJURIES
Bicycle-involved collisions	35	0	1	34
Pedestrian-involved collisions	40	3	4	33


Figure 7:
Bicycle Collisions

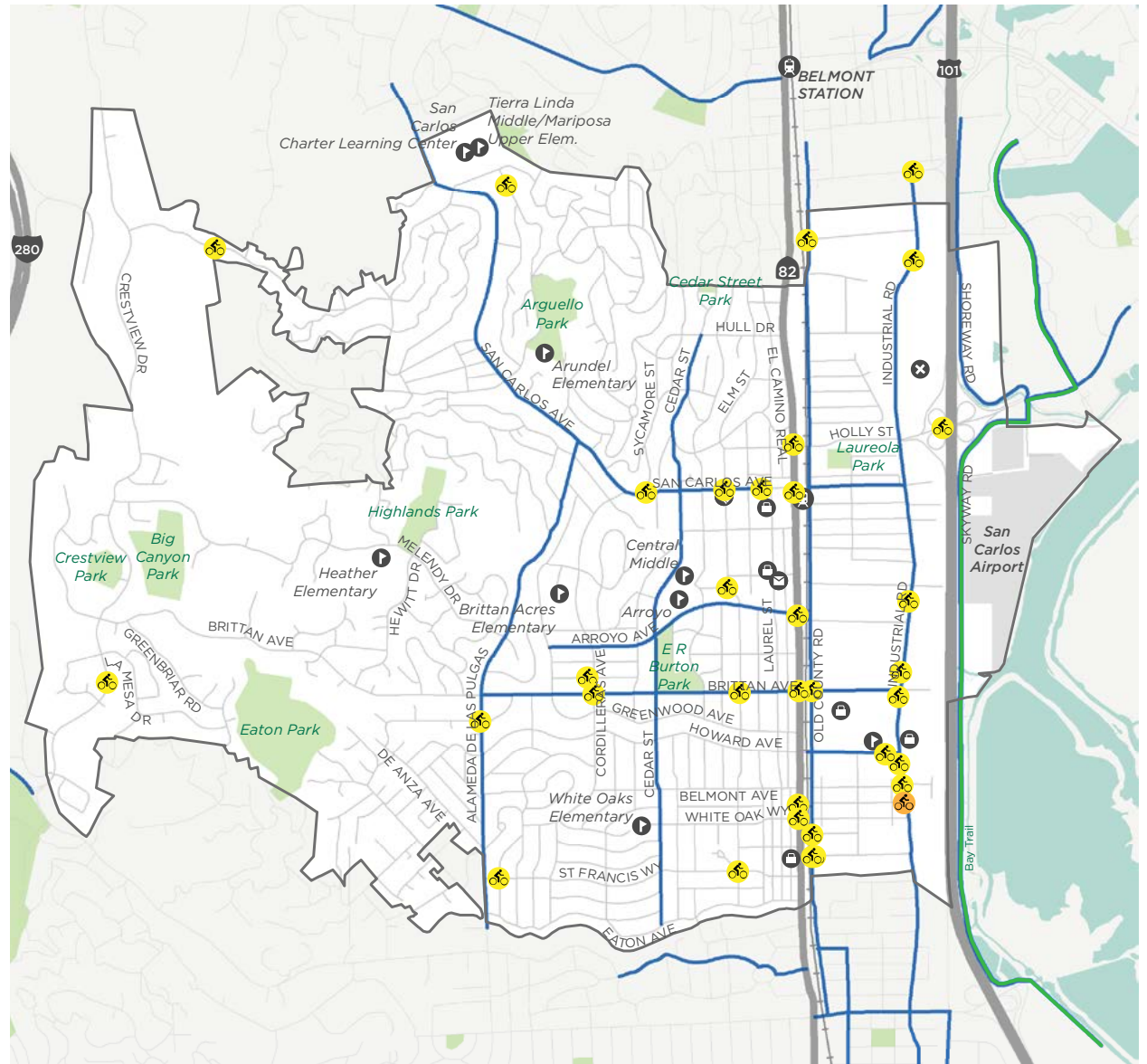
**Bicycle Collisions
(2013 - 2017)**

-  Severe Injury (1)
-  Minor Injury (34)
-  Existing Bikeway

**Destinations +
Boundaries**

-  Post Office
-  Shopping
-  Hospital
-  Library
-  School
-  Caltrain Station
-  Bay Trail
-  Park
-  San Carlos City Limit




0 0.25 0.5 MILES 




03. SAN CARLOS TODAY







Figure 8:
Pedestrian Collisions



**Pedestrian Collisions
(2013 - 2017)**


-  Fatality (3)
-  Severe Injury (4)
-  Minor Injury (33)

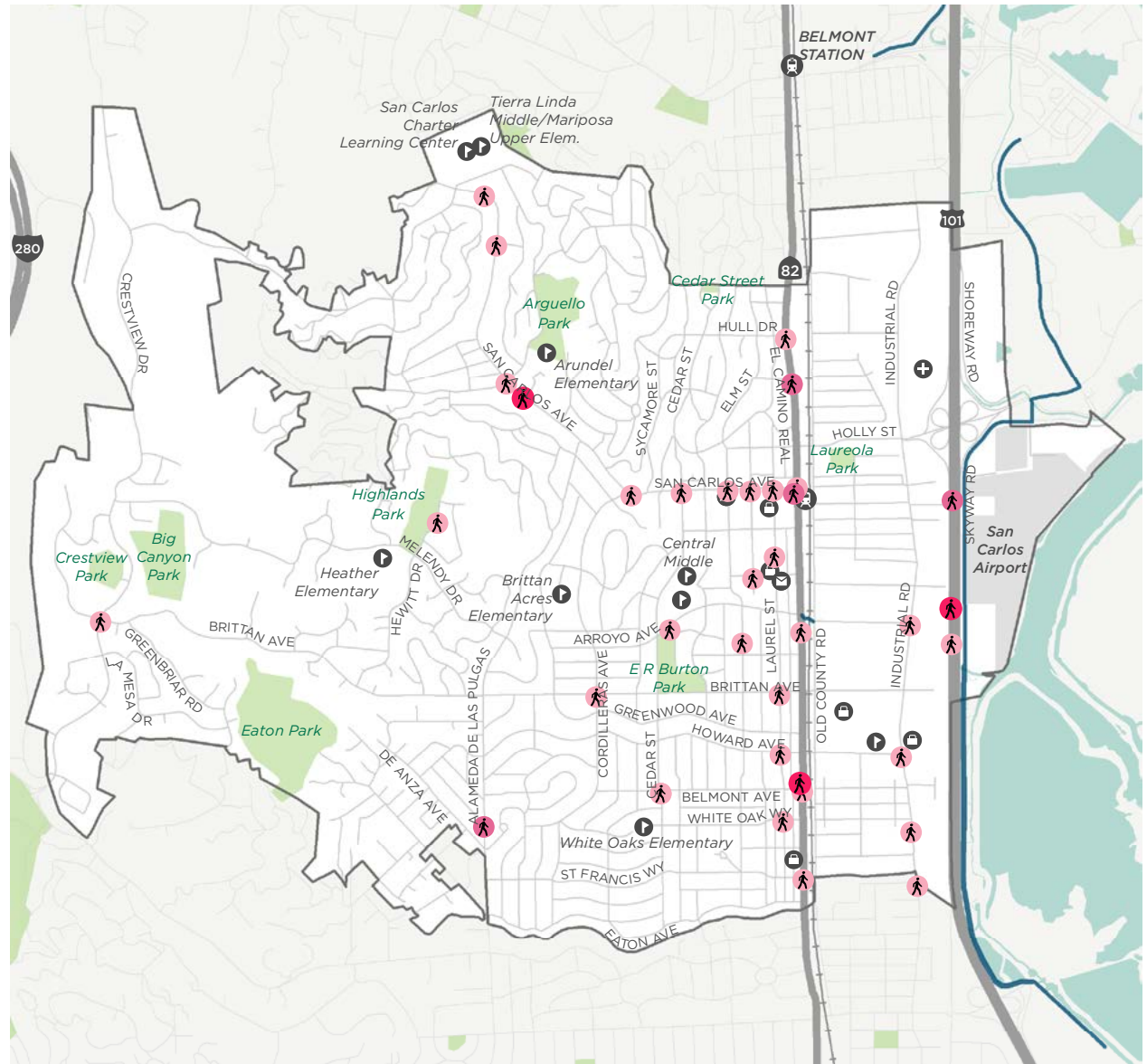
 Existing Shared-Use Path

**Destinations +
Boundaries**

-  Post Office
-  Shopping
-  Hospital
-  Library
-  School
-  Caltrain Station

-  Park
-  San Carlos City Limit

0 0.25 0.5 MILES 



04.

Recommendations

Introduction

The walking and biking network described in this Plan seeks to provide the San Carlos community with convenient and comfortable choices.

Built on the needs and opportunities identified through the evaluation of existing conditions, progress made since the development of the 2012 Bicycle Master Plan, extensive community input, and data driven analyses, this chapter presents the projects, programs, and policy changes for the City of San Carlos.

Recommendations are considered planning-level, meaning they should be used as a guide when implementing projects. In some cases, traffic impact analysis and more detailed design analysis will be required to evaluate specific site conditions and develop designs that reflect conditions and constraints.

This chapter includes the following sections:

- Projects describes the proposed bicycling and walking infrastructure improvements
- Citywide Projects describes improvements that should be pursued throughout San Carlos as opportunities arise, but specific locations for these improvements have not been identified
- Programs includes recommended education, encouragement, enforcement, and evaluation activities to be pursued or expanded by the City and its partners
- Policy Changes includes changes to municipal codes, operating procedures, or other policies that will support a more walkable and bikeable San Carlos

Projects

Bicycle Network Projects

Bicycle network projects are categorized based on the four classifications recognized by Caltrans, along with two sub-classifications, described in detail in the Design Guidelines in Appendix A. These include:

Class I Shared Use Paths: Dedicated paths for walking and bicycling completely separate from the roadway



Class IIB Buffered Bicycle Lanes: Bicycle lanes that include a striped “buffer” area either between the bicycle lane and travel lane or between the bicycle lane and parked cars



Class II Bicycle Lanes: Striped lanes for bicyclists



Class III Bicycle Routes: Signed routes for bicyclists on low-speed, low-volume streets where lanes are shared with motorists



04. RECOMMENDATIONS

Class IIIB Bicycle Boulevards: Bicycle routes that are further enhanced with traffic calming features or other treatments to prioritize bicyclist comfort



Class IV Separated Bikeways: Onstreet bicycle facilities with a physical barrier between the bicycle space and motor vehicle lanes, including bollards, curbs, or parking



A summary of existing and proposed bicycle network improvements is provided in **Table 9**. **Figure 9** displays the recommended bicycle network for San Carlos.

Table 9: Existing and Proposed Bikeway Milage

BIKEWAY TYPE	EXISTING MILES	PROPOSED MILES
Class I Shared Use Path	4.1	5.1
Class II Bicycle Lanes	10.8	5.1
Class IIB Buffered Bicycle Lanes	0.0	4.4
Class III Bicycle Routes	6.8	3.4
Class IIIB Bicycle Boulevards	0.3	8.8
Class IV Seperated Bikeways	0.0	6.2
Total	22.0	33.0

04. RECOMMENDATIONS

Figure 9:
Bikeway Projects

Proposed Bikeways

- Class I Shared-Use Path
- Class II Bicycle Lane
- Class IIB Buffered Bike Lane
- Class III Bicycle Route
- Class IIIB Bicycle Boulevard
- Class IV Separated Bikeway
- Parking Removal
- Lane Reduction
- Peninsula Bikeway

Improvement Areas

- Downtown

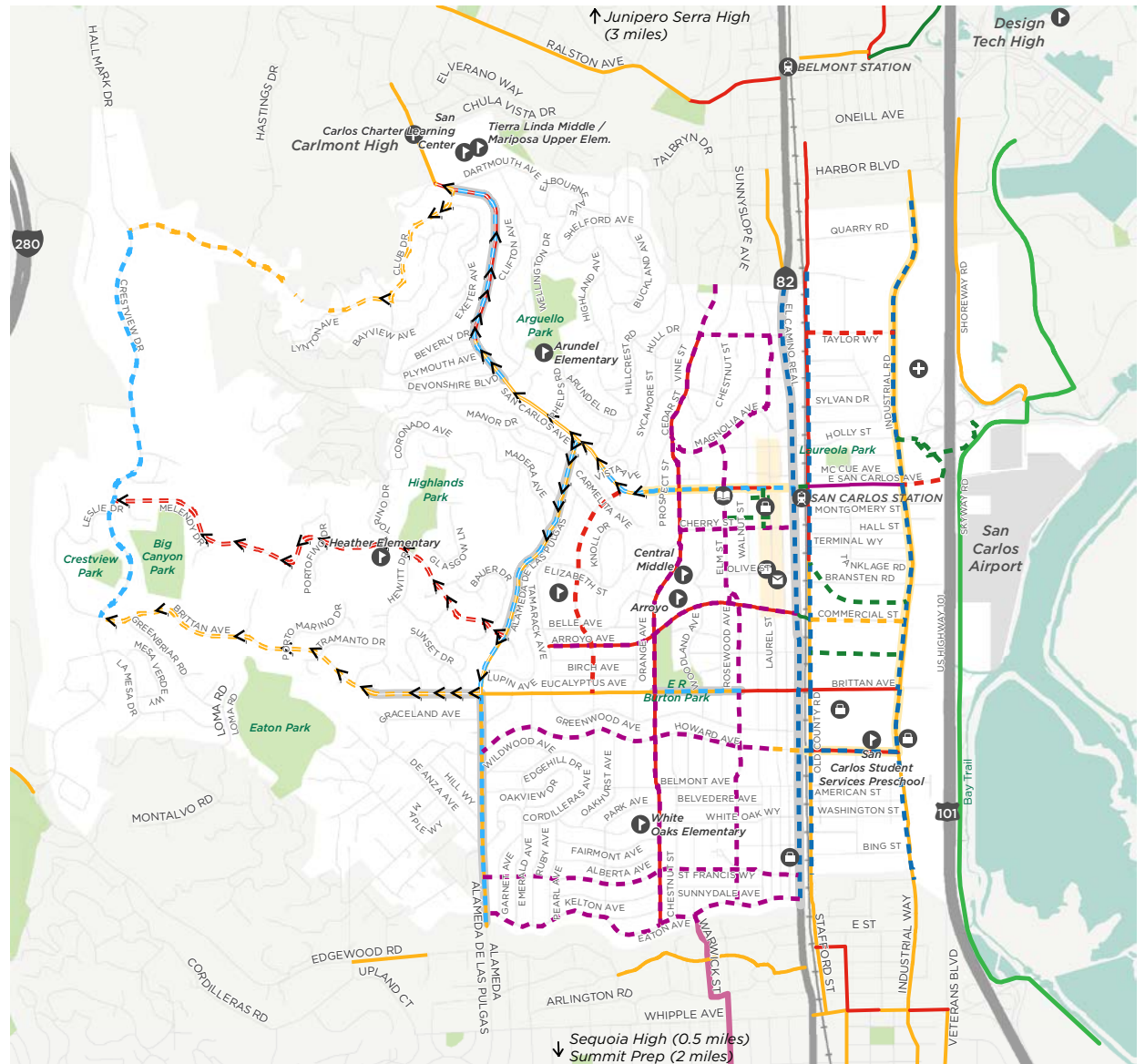
Existing

- Class I Shared-Use Path
- Class II Bicycle Lane
- Class IIIB Bicycle Boulevard
- Class III Bicycle Route

Destinations + Boundaries

- Post Office
- Shopping
- Hospital
- Library
- School
- Caltrain Station
- Bay Trail
- Park
- Uphill Bikeway

0 0.25 0.5 MILES



04. RECOMMENDATIONS

Cross Sections

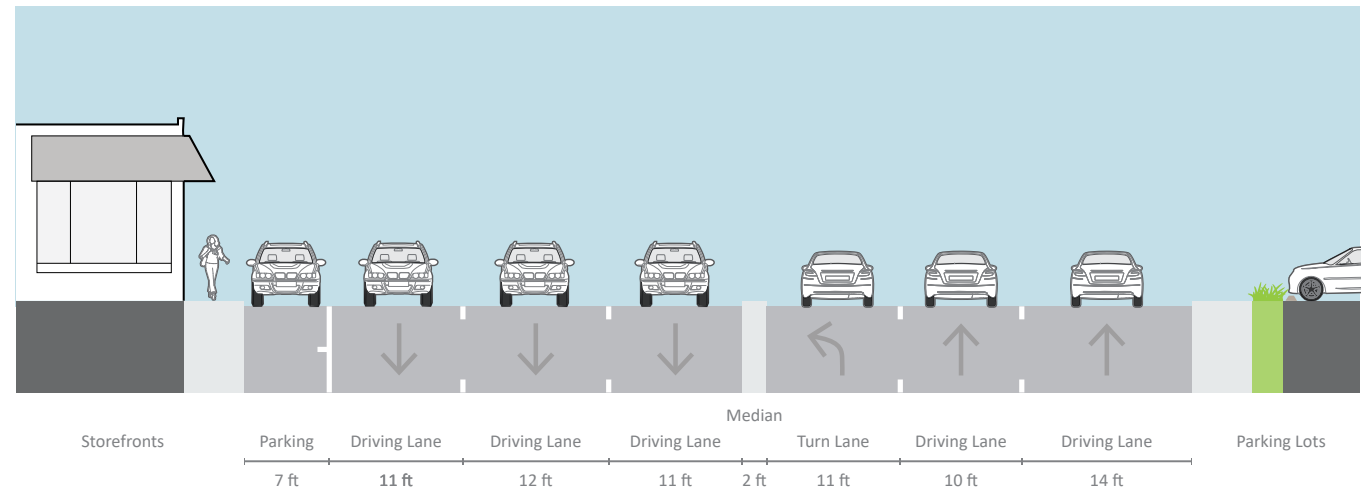
Figures 10 through 16 illustrate conceptual improvements along key corridors within the city. The following concepts explore the opportunities to install bicycle facilities and potential impacts to parking and travel lanes. Note that these are illustrative concepts only and specific design details will be considered during the design phase and associated public outreach for each recommended improvement.

Figure 10:

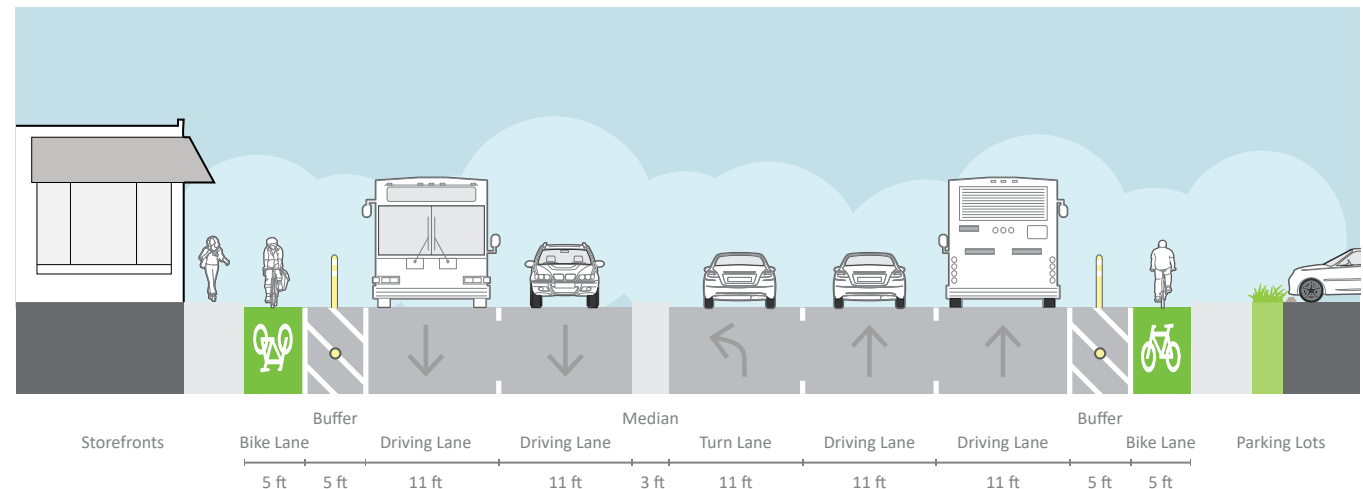
El Camino Real 78' Width

El Camino Real, South of
San Carlos Avenue

Existing Conditions



Proposed Separated Bikeway

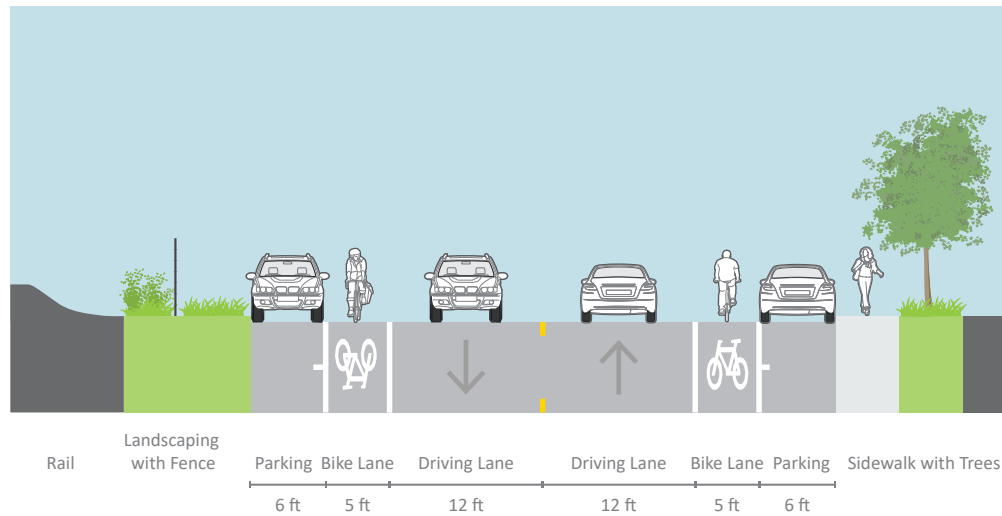


04. RECOMMENDATIONS

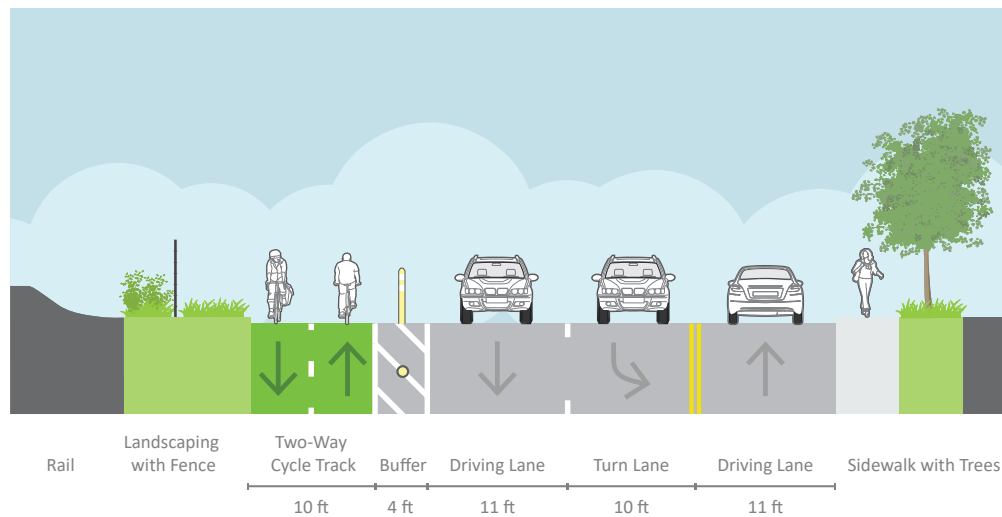
Figure 11:
Old County Road
45' Width

Old County Road, between
Howard Ave and Bing St

Existing Conditions

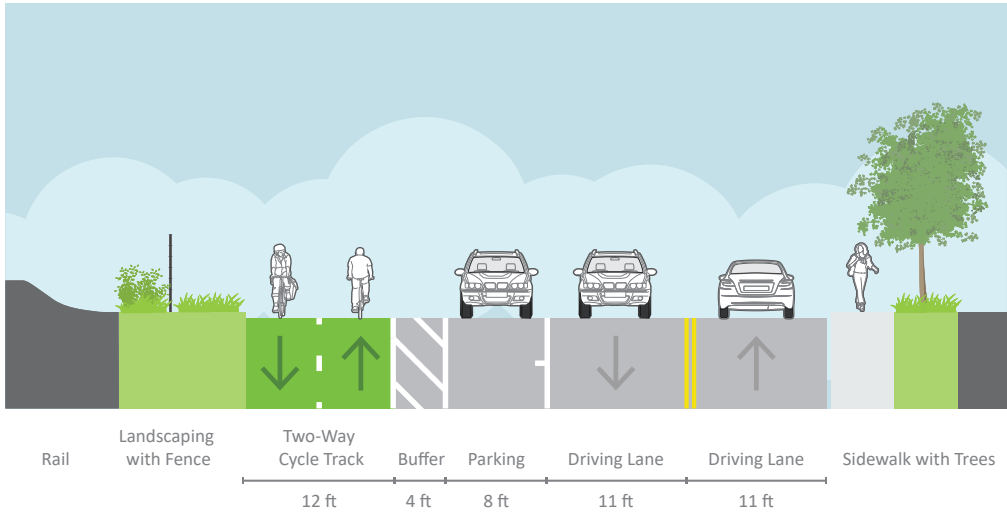


Proposed Concept A Separated Two-Way Cycle Track



04. RECOMMENDATIONS

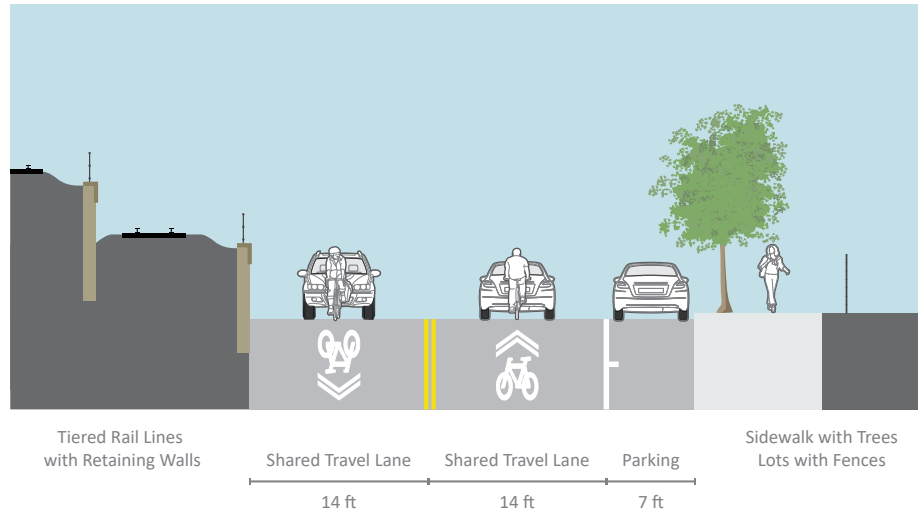
Proposed Concept B Separated Two-Way Cycle Track with Parking



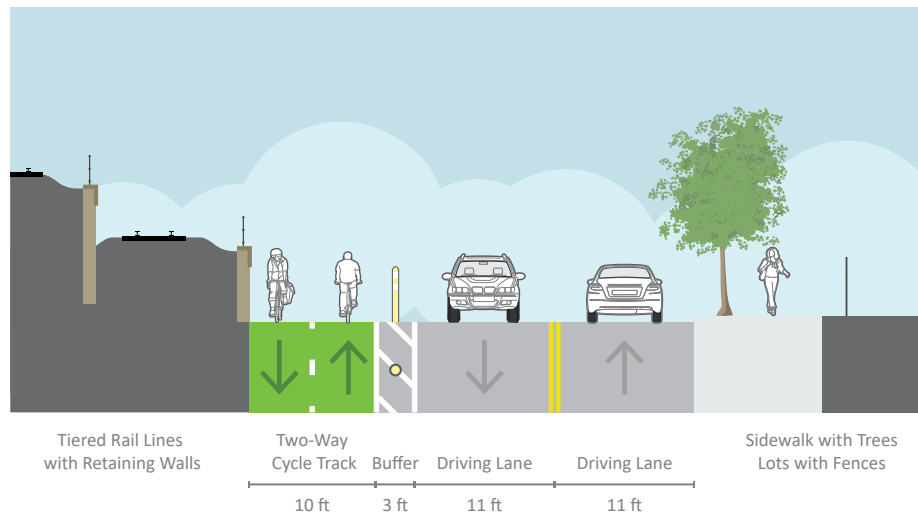
04. RECOMMENDATIONS

Figure 12:
Old County Road
35' Width
Old County Road, between
Cherry St and Terminal Way

Existing Conditions



Proposed Separated Two-way Cycle Track

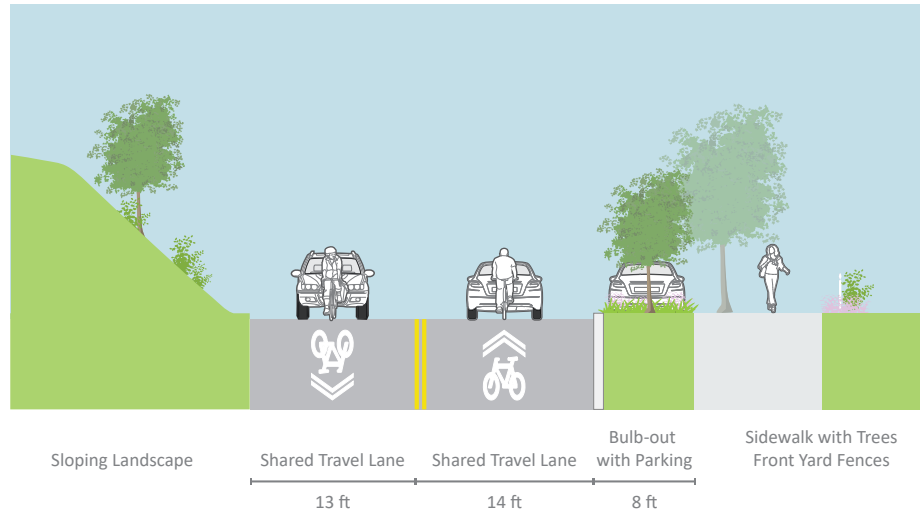


04. RECOMMENDATIONS

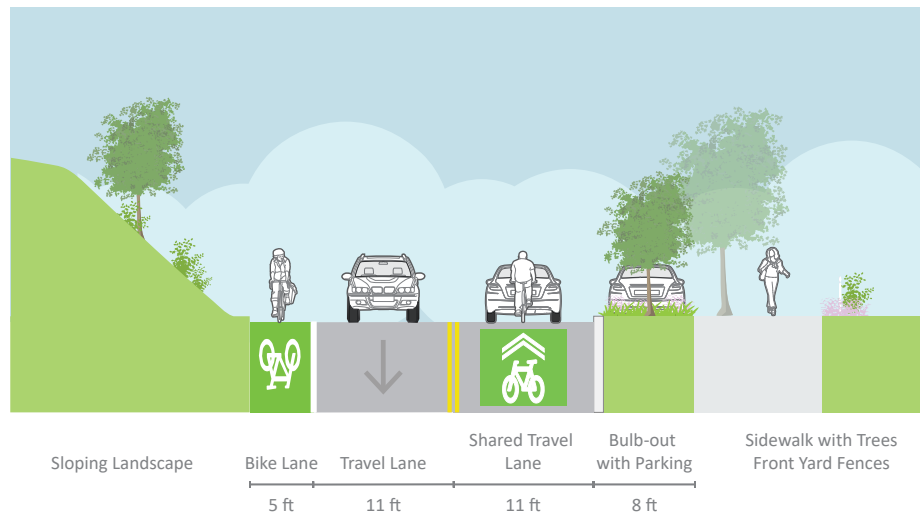
Figure 13:
Old County Road
35' Width

Old County Road,
North of Holly Rd

Existing Conditions



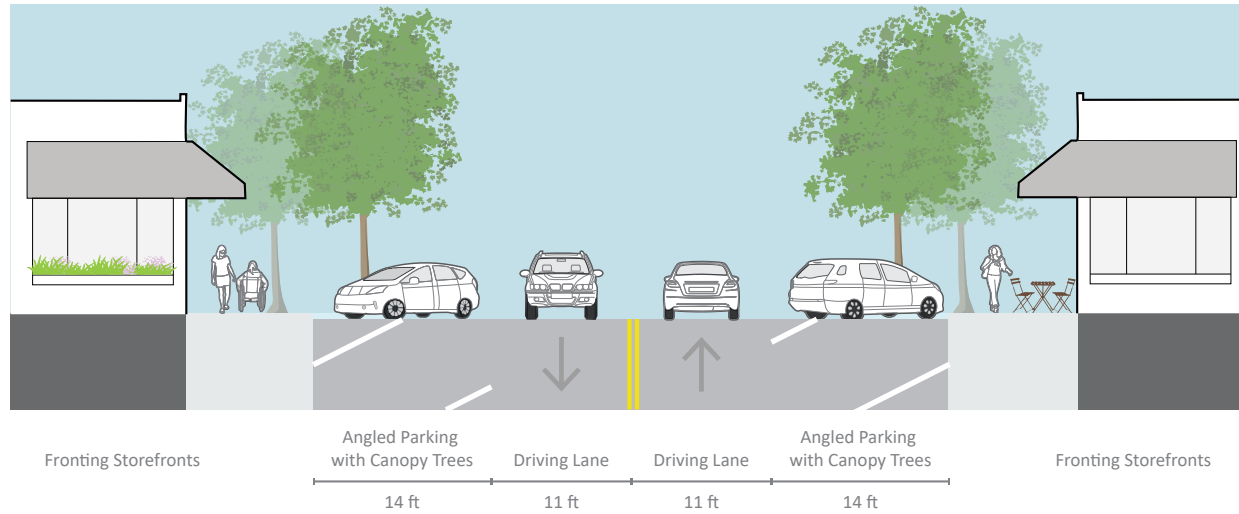
Proposed SB Bike lane and Shared Travel Lane



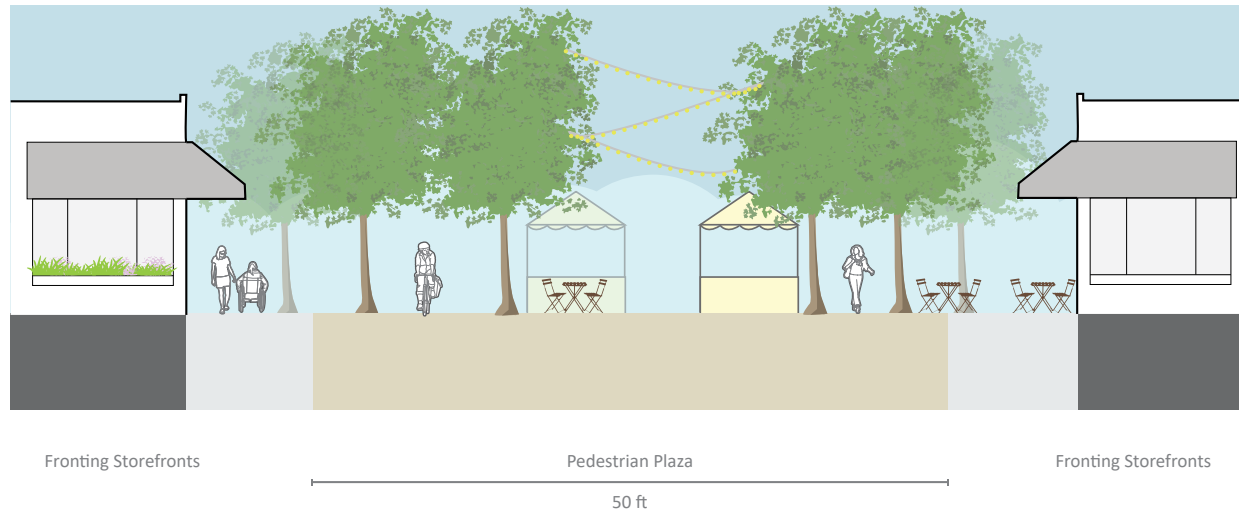
04. RECOMMENDATIONS

Figure 14:
Laurel Street
50' Width

Existing Conditions



Proposed Pedestrian Plaza



Further analysis of this proposed alternative will be conducted as part of the San Carlos Downtown planning process.

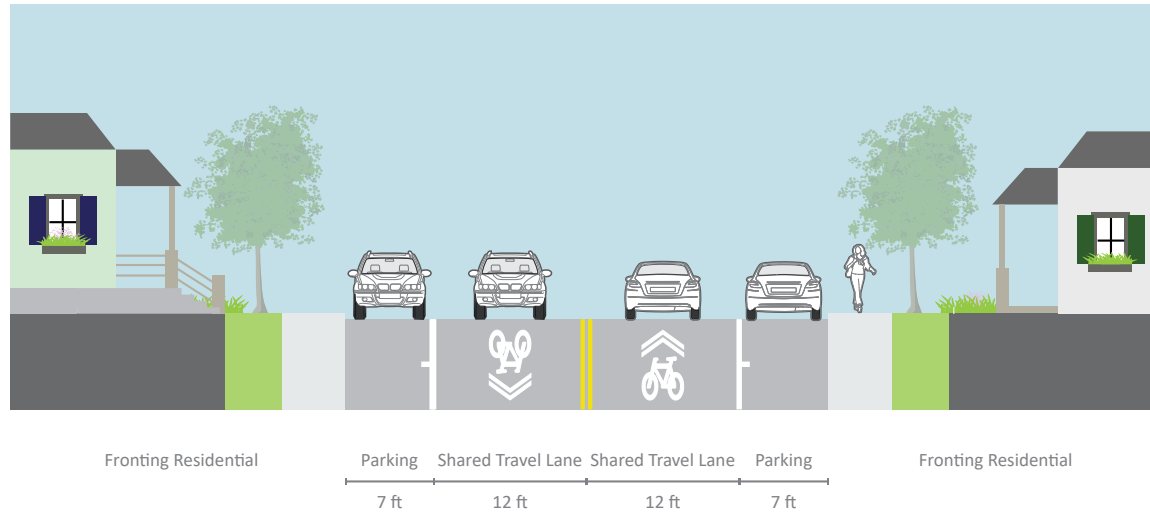
04. RECOMMENDATIONS

Figure 15:

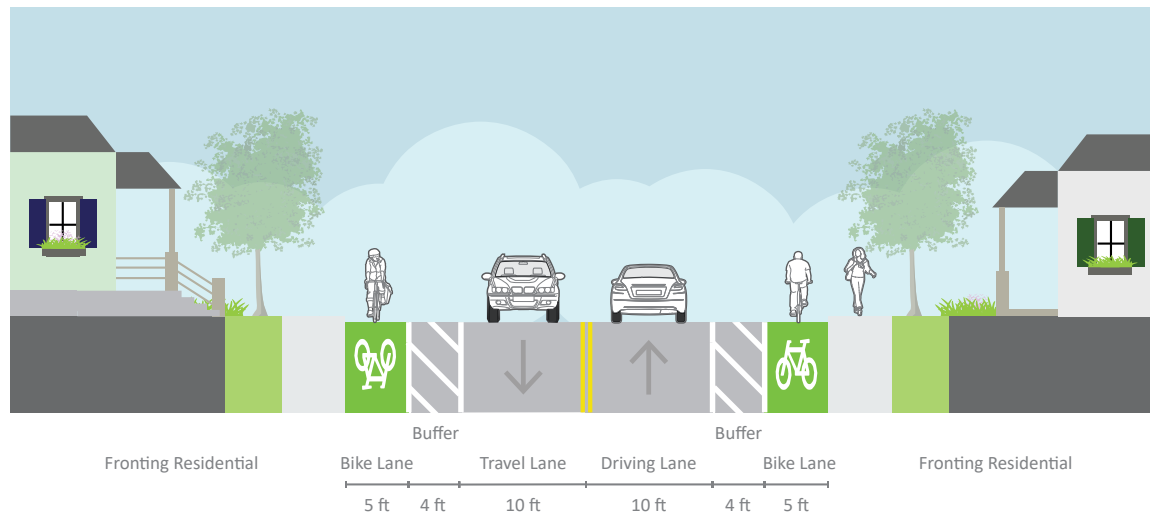
San Carlos Ave 39' Width

San Carlos Ave, between
Wellington Dr and
Dartmouth Ave

Existing Conditions



Proposed Buffered Bikeway



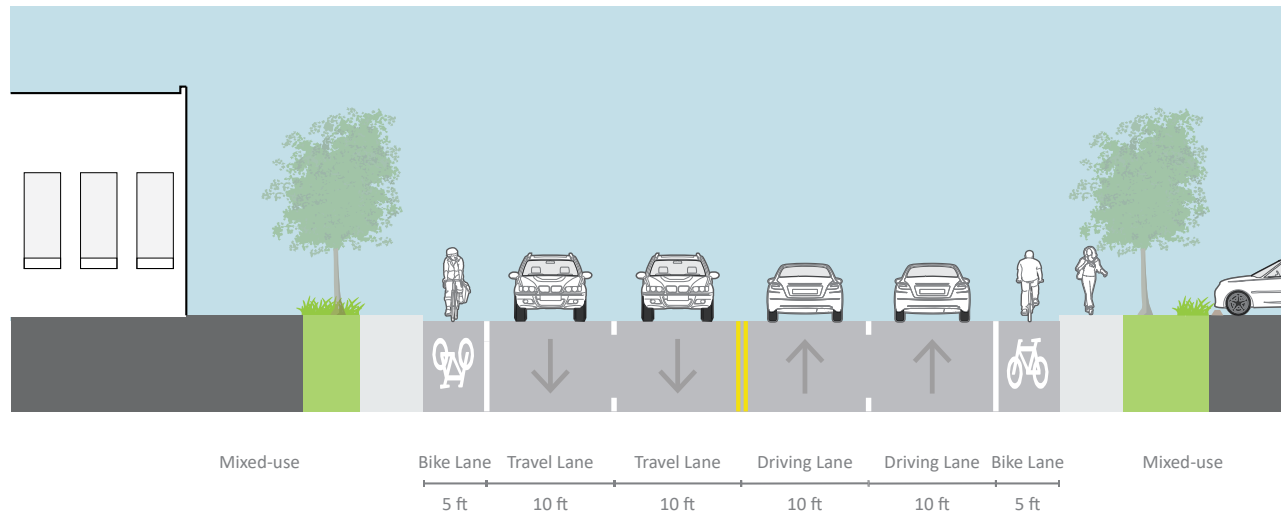
04. RECOMMENDATIONS

Figure 16:

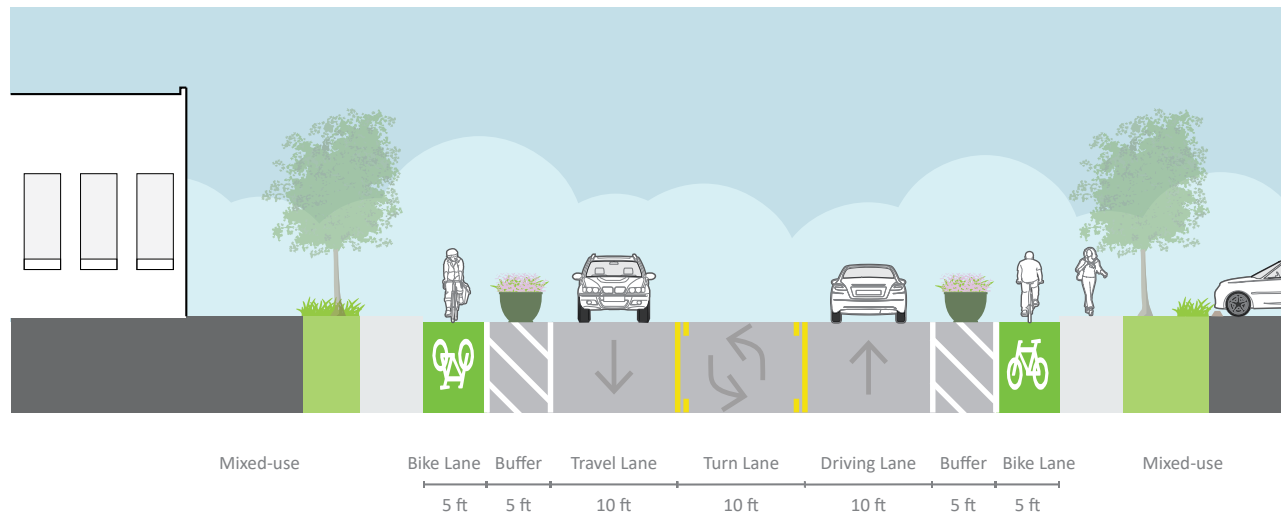
Industrial Rd 50' Width

Industrial Road, between
Commercial St and Brittain Ave

Existing Conditions



Proposed Buffered Bikeway



04. RECOMMENDATIONS

Pedestrian Network Projects

Proposed pedestrian network projects in San Carlos include building new sidewalks to close gaps in the pedestrian network, enhancing crossing conditions at intersections, improvements to signalized intersections, installing pedestrian beacons, stop sign warrant studies, and walking environment enhancements. These recommendations are identified within Priority Pedestrian Routes as shown in **Figure 18**. These routes provide important walking connections to the highest number of neighborhood destinations such as schools, parks, libraries, and major transit stops. Priority Pedestrian Routes also include streets and intersections in San Carlos where people walking have been the victim of an automobile collision.

Types of Pedestrian Improvements

There are many streets in San Carlos with sidewalks or pathways, but the network is inconsistent. Sidewalk recommendations are focused on those corridors where they are likely to serve a large number of pedestrians. Sidewalk improvements are recommended at the following locations:

ROADWAY	START	END
San Carlos Ave	Wellington Dr	Alameda De Las Pulgas
San Carlos Ave	Upland Ave	Cordilleras Ave
Brittan Ave	Milano Way	Rogers Ave

04. RECOMMENDATIONS

Figure 17:
Pedestrian
Recommendations

Proposed

- Pedestrian Improvement
- Sidewalk Improvement
- Class I Shared-Use Path
- Priority Pedestrian Route

Improvement Areas

- Downtown Improvement Area

Existing

- Class I Shared-Use Path

Destinations + Boundaries

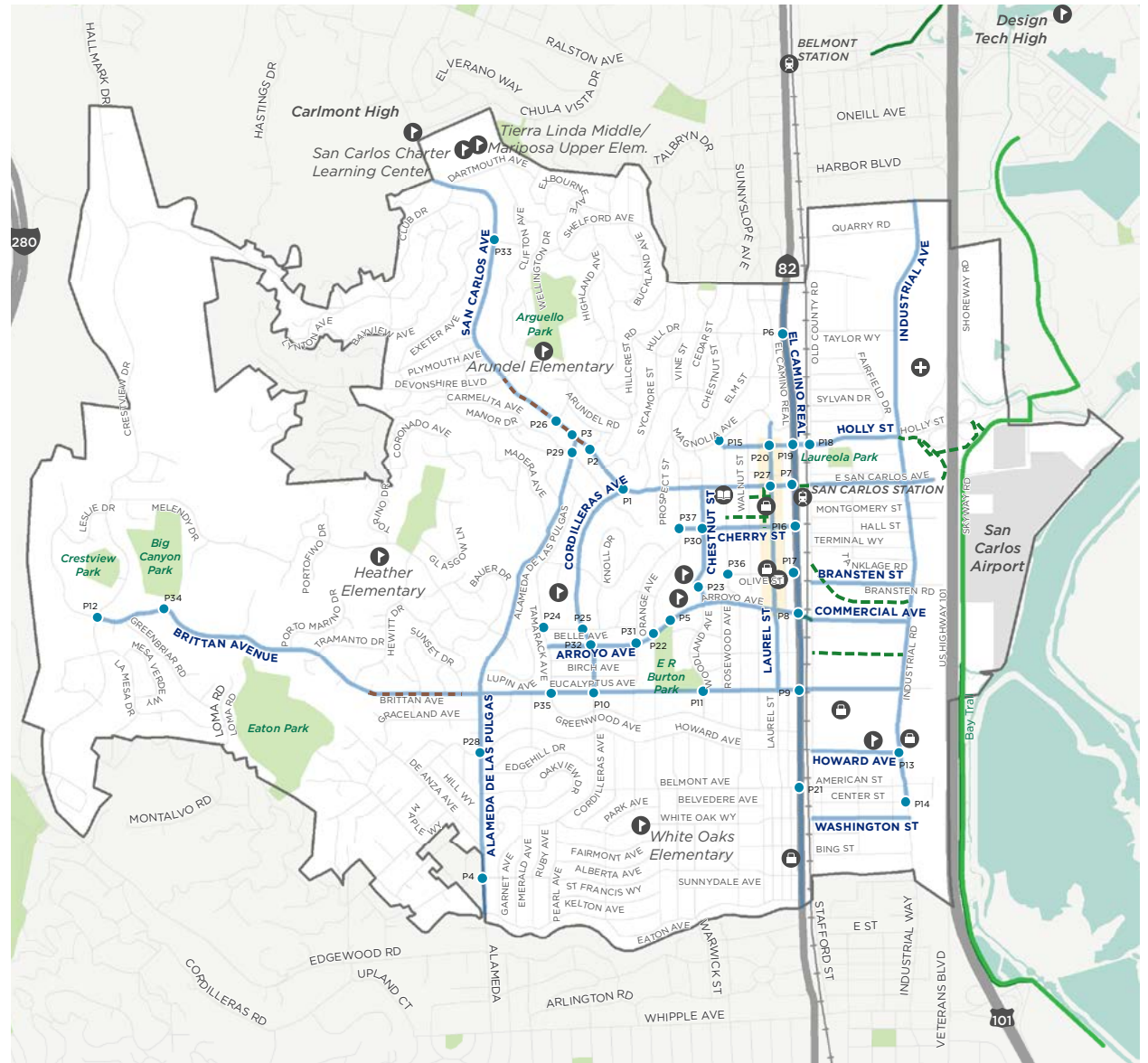
- 📧 Post Office
- 🛒 Shopping
- 🏥 Hospital
- 📖 Library
- 🎓 School
- 🚆 Caltrain Station

— Bay Trail

■ Park

□ San Carlos City Limit

0 0.25 0.5 MILES



04. RECOMMENDATIONS

In addition to sidewalk improvements, this Plan recommends 39 pedestrian safety improvements to enhance the pedestrian experience. Specific facility recommendations and designs for these locations will be developed by the City on a case-by-case basis due to the highly varied context at each location. The recommendations are broken into the following categories.

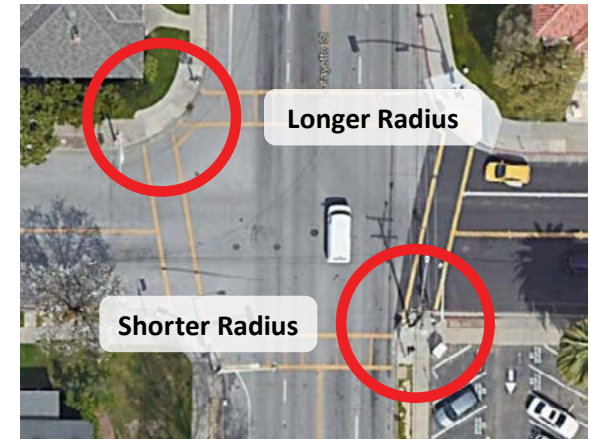
Crossing Improvements

Curb Extension



Curb extensions, or bulb-outs, are extensions of the sidewalk and curb at the corners of intersections. They normally extend out the width of the parking lane, 7' to 8'. Curb extensions shorten the roadway crossing distance and make pedestrians more visible to motorists. They can also help calm traffic by narrowing the travel lane, and provide additional space for plantings and street furnishings.

Curb Reduction



Shorter turn radii at corners shorten the crossing distance for pedestrians and require vehicles to turn more slowly, increasing safety at the intersection.

04. RECOMMENDATIONS

High Visibility Crosswalk Marking



High visibility crosswalks use marking patterns that are more visible to motorists than the standard parallel “transverse” markings, and help increase yielding behavior and deter encroachment. Ladder, zebra, and the continental markings (shown here) are commonly used patterns.

Advance Yield/Stop Lines



Advance yield and stop lines inform motorists of the correct position to wait for pedestrians at marked crossings. Advanced yield lines are triangular pavement markings (“shark teeth”) placed in advance of uncontrolled crossing locations. These markings are especially important at multi-lane, uncontrolled crossings to ensure vehicles in one lane stop sufficiently far back so that approaching vehicles in the other lane can see the pedestrian and also yield. Advance stop lines are used in advance of stop signs or signalized crossings.

Curb Ramp



Curb ramps provide a transition between the sidewalk and roadway crossings. Curb ramps are essential for pedestrians using wheeled mobility devices and provide universal access for all users including small children, adults pushing strollers, people with luggage, etc. Each corner should have two “perpendicular” ramps, one leading directly into each crosswalk. Ramps should include a tactile warning surface to inform users with visual impairments that they are transitioning between the sidewalk and the roadway.

04. RECOMMENDATIONS

Pedestrian Refuge Island



Pedestrian refuge islands are typically areas at the mid-point of a marked crossing that provide a protected waiting space for pedestrians. They allow pedestrians to cross the roadway in two separate stages.

Signal Improvements

Leading Pedestrian Interval



A Leading Pedestrian Interval (LPI) typically gives pedestrians a 3–7 second head start when entering an intersection with a corresponding green signal in the same direction of travel.

Pedestrian Beacons

Pedestrian Hybrid Beacon



Pedestrian Hybrid Beacons (PHB), also known as High-Intensity Activated Crosswalk Beacons (HAWKs), are user-activated traffic control devices that cycle through a flashing yellow, steady yellow, and then steady red light to stop vehicles and allow pedestrians to cross a road safely. They can be installed in mid-block locations or at intersections where a full traffic signal is not warranted.

04. RECOMMENDATIONS

Rectangular Rapid Flash Beacon



Rectangular Rapid Flash Beacons (RRFBs) are user-activated pedestrian beacons that use flashing high intensity LED lights to alert motorists to the presence of pedestrians in the crosswalk. They can be installed at any uncontrolled crossing location.

Stop Sign Warrant Study

Stop Sign/Signal Warrant Study



Stop sign or signal warrant studies evaluate traffic conditions, pedestrian characteristics, and physical characteristics of the location to determine whether installation of a traffic control device is justified at a particular location.

Walking Environment Enhancement

Parklets



Parklets provide public seating platforms and often convert curbside parking spaces into public community spaces.

04. RECOMMENDATIONS

Temporary Street Closures



Temporary street closures close down streets to motor vehicle access. These can include events such as Farmer's Markets, open streets events, block parties and other festivals. Temporary closures can activate the street and showcase participating businesses and communities.

Shade Trees



Shade trees can increase pedestrian comfort by providing a shaded environment, dampening noise pollution, and adding aesthetic value to the built environment. Street trees have the added benefit of contributing to stormwater management for jurisdictions.

Pedestrian Scale Lighting



Pedestrian-scale lighting can improve walking accessibility at night time by illuminating sidewalks, crosswalks, curbs, and signs as well as barriers and potential hazards. Pedestrian-scale lighting should be employed in areas of high pedestrian activity.

04. RECOMMENDATIONS

Table 10 below identifies the types of pedestrian improvements by project number as identified on **Figure 17**.

Table 10: Pedestrian Improvements by Location

PROJ- ECT	CROSS STREET A	CROSS STREET B	CROSSING IMPROVEMENTS						SIGNAL IMPROVE- MENTS	PEDESTRIAN BEACONS		STOP WARRANT STUDY	SPACE ACTIVA- TION
			CURB EXTEN- SION	CURB REDU- CTION	HIGH VISIBILITY CROSS- WALK	ADVANCED YIELD/ STOP	CURB RAMP	PEDEST- RIAN REFUGE ISLAND	LEADING PEDESTRIAN INTERVAL	PEDEST- RIAN BEACON	RRFB		
P1	San Carlos Ave	Cordilleras Ave	✓				✓						
P2	San Carlos Ave	Upland Ave					✓						
P3	San Carlos Ave	Alameda de las Pulgas					✓						
P4	Alameda de las Pulgas	St Francis Wy					✓						
P5	Arroyo Ave	Chestnut St					✓						
P6	El Camino Real	Hull Dr	✓	✓	✓		✓		✓				
P7	El Camino Real	San Carlos Ave	✓		✓		✓		✓				
P8	El Camino Real	Arroyo Ave							✓				✓

04. RECOMMENDATIONS

			CROSSING IMPROVEMENTS						SIGNAL IMPROVEMENTS	PEDESTRIAN BEACONS		STOP WARRANT STUDY	SPACE ACTIVATION
PROJ-ECT	CROSS STREET A	CROSS STREET B	CURB EXTENSION	CURB REDUCTION	HIGH VISIBILITY CROSS-WALK	ADVANCED YIELD/STOP	CURB RAMP	PEDESTRIAN REFUGE ISLAND	LEADING PEDESTRIAN INTERVAL	PEDESTRIAN BEACON	RRFB		
P9	El Camino Real	Brittan Ave	✓		✓		✓	✓	✓				
P10	Brittan Ave	Cordilleras Ave	✓		✓		✓						
P11	Brittan Ave	Woodland Ave	✓		✓	✓					✓		
P12	Brittan Ave	Crestview Dr	✓	✓	✓		✓						
P13	Industrial Ave	Howard Ave	✓	✓	✓		✓		✓				
P14	Industrial Ave	Center St	✓		✓	✓	✓			✓			
P15	Magnolia Ave	Elm St	✓		✓		✓						
P16	El Camino Real	Cherry St							✓				
P17	El Camino Real	White Oak Way	✓		✓								
P18	Holly Ave	Old County Rd	✓		✓								

04. RECOMMENDATIONS

			CROSSING IMPROVEMENTS						SIGNAL IMPROVEMENTS	PEDESTRIAN BEACONS		STOP WARRANT STUDY	SPACE ACTIVATION
PROJ-ECT	CROSS STREET A	CROSS STREET B	CURB EXTENSION	CURB REDUCTION	HIGH VISIBILITY CROSS-WALK	ADVANCED YIELD/ STOP	CURB RAMP	PEDESTRIAN REFUGE ISLAND	LEADING PEDESTRIAN INTERVAL	PEDESTRIAN BEACON	RRFB		
P19	Holly Ave	El Camino Real	✓		✓			✓	✓				
P20	Holly Ave	Laurel St	✓		✓				✓				
P21	El Camino Real	Belmont Ave								✓			
P22	Cedar St	Arroyo Ave	✓		✓								
P23	Woodland Ave	Chestnut St			✓								
P24	Tamarack Ave	Belle Ave			✓								
P25	Cordilleras Ave	Belle Ave	✓		✓								
P26	San Carlos Ave	Phelps Rd					✓						
P27	San Carlos Ave	Laurel St			✓				✓				
P28	Alameda De Las Pulgas	Howard Ave					✓					✓	

04. RECOMMENDATIONS

			CROSSING IMPROVEMENTS						SIGNAL IMPROVE- MENTS	PEDESTRIAN BEACONS		STOP WARRANT STUDY	SPACE ACTIVA- TION
PROJ- ECT	CROSS STREET A	CROSS STREET B	CURB EXTEN- SION	CURB REDU- CTION	HIGH VISIBILITY CROSS- WALK	ADVANCED YIELD/ STOP	CURB RAMP	PEDEST- RIAN REFUGE ISLAND	LEADING PEDESTRIAN INTERVAL	PEDEST- RIAN BEACON	RRFB		
P29	Alameda De Las Pulgas	Carmelita Dr			✓	✓	✓						
P30	Chestnut St	Cherry St	✓		✓	✓							
P31	Orange Ave	Arroyo Ave			✓								
P32	Cordilleras Ave	Arroyo Ave										✓	
P33	Kenton Ave	San Carlos Ave										✓	
P34	Brittan Ave	Trail Connecting Parks			✓	✓				✓			
P35	Brittan Ave	Greenwood Ave	✓								✓		
P36	Olive Ave	Elm St	✓		✓	✓							
P37	Cherry St	Cedar St	✓										

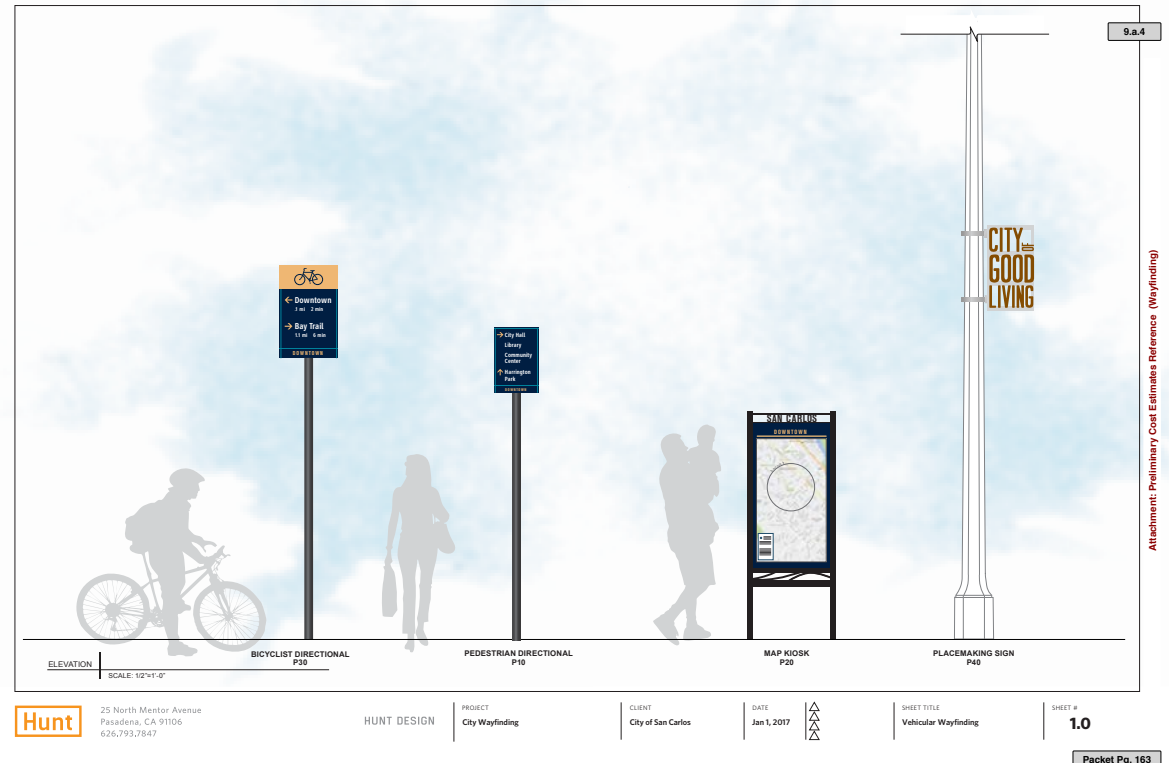
Citywide Projects

In addition to specific infrastructure projects and related programmatic efforts, some amenities are needed citywide to complete the transportation network. These amenities should be installed as a matter of policy in conjunction with any City project as opportunities arise, or when development occurs. Citywide amenities recommended in this Plan include a comprehensive wayfinding program, secure bicycle parking, pedestrian scale lighting, and amenities.

Wayfinding

Wayfinding signs direct bicyclists or pedestrians along the existing network and to key community destinations. The City has adopted a Wayfinding Program including bicycle and pedestrian signs. This Plan recommends that the City develop and implement a comprehensive wayfinding program for bicyclists and pedestrians utilizing the concepts developed from the City's Wayfinding Project.

Pedestrian and Bikeway Wayfinding



04. RECOMMENDATIONS

Bicycle Parking

No bicycling network is complete without convenient and secure bicycle parking. Bicycle parking can take many forms, from a simple bicycle rack to secure storage in a locker or gated area. This Plan recommends the City continue to expand its bicycle parking and repair stations as opportunities arise and new development occurs.

Short term bicycle parking

Bicycle parking can be categorized into short-term and long-term parking. Bicycle racks are the preferred device for short-term bicycle parking. These racks serve people who leave their bicycles for relatively short periods of time, typically for shopping or errands, dining, or recreation. Bicycle racks provide a high level of convenience and moderate security. Where possible, on-street bicycle corrals can be used to provide increased bicycle parking where high demand or limited sidewalk space exists. Repair stations with basic tools and wayfinding or guide signs help bicyclists navigate along the way.

Long term bicycle parking

Long-term bicycle parking includes bike lockers and secure parking areas (SPAs) and serves people who intend to leave their bicycles for longer periods of time. Bike lockers may vary in design and operation including keyed lockers that are rented to one individual on an annual or monthly basis or e-lockers that can be reserved online in hourly increments and unlocked with a credit card or an

access code. These facilities provide a higher level of security than bicycle racks, and are typically found at transit stations, multifamily residential buildings, and commercial buildings, though they may also be useful in Downtown San Carlos, near the Caltrain Station, or in other areas where bicyclists running multiple errands would benefit from a secure place to store parcels in addition to their bicycle.

Pedestrian Scale Lighting

Pedestrian scale lighting is a type of lighting with frequent lampposts at low height that illuminate the walking area. This typically includes poles 12 to 15 feet high spaced 25 to 30 feet apart, directly above walking areas. Pedestrian scale lighting not only increases visibility of pedestrians for drivers at night, it contributes to a more comfortable and inviting streetscape for people walking at night. Pedestrian scale lighting should be appropriately designed to illuminate only the areas needed and be no brighter than necessary. Street trees should be appropriately maintained so they do not obstruct illumination from the lighting along sidewalks and pathways. This Plan recommends the City evaluate locations where pedestrian scale lighting may improve pedestrian comfort and encourage walking, including downtown, the Industrial Arts district, and trails.

Amenities

Sidewalk and trail furnishings like benches, shade structures, restrooms, water fountains, and trash receptacles contribute to a cleaner, more comfortable, and more pedestrian-oriented public realm. These elements not only encourage the activation of sidewalk and trail networks, they contribute to a more accessible pedestrian network for all residents. Seniors and those with mobility impairments will benefit from frequent places to stop and rest. This Plan recommends the City identify and pursue opportunities to provide amenities in the downtown, near transit stops, and along trails in the community.

Programs

This section describes recommended bicycle and pedestrian related programs for the City of San Carlos. The recommendations are organized in four E's:

- Education programs are designed to improve safety and awareness. They can include programs that teach students how to safely ride or teach drivers to expect bicyclists. They may also include brochures, posters, or other information that targets bicyclists or drivers.
- Encouragement programs provide incentives and support to help people leave their car at home and try biking instead.
- Enforcement programs enforce legal and respectful bicycling and driving. They include a variety of tactics, ranging from police enforcement to neighborhood signage campaigns.
- Evaluation programs are an important component of any investment. They help measure success at meeting the goals of this plan and to identify adjustments that may be necessary.

Education

Safe Routes to School

San Carlos benefits from the robust Safe Routes to School (SRTS) program coordinated by San Mateo County Office of Education. Schools in the city participate in activities with the SRTS program, but involvement varies from school to school.

Since the program's establishment in 2011, San Mateo County's Safe Routes to School (SRTS) program has helped enable and encourage students all across the County to walk or bicycle to school. Based on the national model, San Mateo County's SRTS program focuses on five key areas: Education, Encouragement, Enforcement, Engineering, and Evaluation.

San Mateo County's SRTS has helped schools throughout the county by providing resources for events like Walk and Roll to School Days, as well as suggested walking and bicycling routes for students. In addition, San Mateo County's SRTS program has provided project coordination and helped facilitate education and enforcement programs like Bike Rodeos and Parent Education Events.

In 2013, Improvement Plans and Walk and Roll to School Maps were created for Brittan Acres Elementary, Central Middle, Arroyo, Arundel Elementary, and Heather Elementary. This Plan recommends that the City of San Carlos partner with San Mateo County Office of Education and the San Carlos Unified School District to develop improvement plans and Walk and Roll to School Maps for the remaining schools in the district.

Bike Education Classes and Repair Centers

This Plan recommends working with local community organizations such as the Silicon Valley Bicycle Coalition to offer bike education classes, which the City can support by advertising the classes and/or providing meeting space. Classes and programs could include the following:

Adult Bicycling Skills Classes

- Adults can learn riding skills, rules of the road, crash avoidance, theft prevention, how to ride at night, and how to use bike safety equipment in these classes.

Bike Mechanic Classes

- These workshops are designed to teach basic bike maintenance skills and how to use different tools.

Family Biking Workshops

- Bike workshops that teach parents with kids with their own bikes and helmets how to ride together safely on a designed course.

Bike Repair Centers

- Existing community facilities such as libraries and community centers can be adapted to offer bike repair services, fix it stations, and hydration stations.

04. RECOMMENDATIONS

Encouragement

Bicycle Friendly Business Program

Bicycle Friendly Business programs recognize businesses who make it easy and convenient for both employees and customers to arrive by bicycle. This requires different strategies to accommodate the different needs of customers and employees. To accommodate customers, providing bicycle parking and supporting City bicycling projects can make it safer and easier to travel by bicycle. Some businesses also choose to offer discounts or incentives to people who arrive by bicycle.

For employees, offering secure longterm parking for bicycles is key. This could include a secure gated bicycle parking area, or access to bicycle lockers. If space is not available for dedicated secure bicycle parking, business owners and landlords can consider allowing employees and tenants to bring bicycles inside and store them in their workspace or another designated location. Providing changing areas, showers, or lockers to store belongings can also make it easier for employees to bicycle to work.

By recognizing businesses who support bicycling, San Carlos can support their local economy while fostering partnerships with the Chamber of Commerce and business owners to build community support for bicycling projects and programs. The League of American Bicyclists has a Bicycle Friendly Business program or some communities have chosen to develop their own programs.

Bike to Work Month and Day

A day-wide campaign encouraging everyone to try commuting by bicycle for the first time and celebrate those who ride regularly. This event becomes a community-wide effort as companies, organizations, and individuals support Bike to Work Day each year by hosting one or more of the energizer stations.

International Walk and Roll to School Days

During international walk and “roll” to school days, students and families are invited to walk or “roll” to school if a safe route is available to them. This event celebrates active transportation and encourages families and students to consider alternatives to driving single-family vehicles to and from school. The City of San Carlos should continue to support the School District in their Safe Routes to School endeavors.

Transportation Awareness Campaign

Media campaigns enhance awareness of transportation related issues such as safety. They can be developed for print, social media, and video collateral for advertising on billboards, in newspapers, online, and on the radio. Campaign topics include the three-foot passing law and using shared-use paths.

Enforcement

Crossing Guard Program

The effectiveness of a crossing guard can be the deciding factor in a parent feeling comfortable enough to let their child walk or bicycle to school. The City should continue to work with the School District and the San Mateo County Office of Education to identify locations for crossing guards.

Evaluation

Bicycle Friendly Community Program

The League of American Bicyclists (LAB) recognizes communities who have put in significant effort toward bicycle-related projects and programs by awarding Bronze to Diamond certifications. Communities apply every four years with applications focusing on engineering feats, encouragement programs, enforcement policies, and evaluation and planning.

San Carlos is currently a Bronze level community. The City of San Carlos should strive to improve its status by 2026. Efforts to elevate San Carlos status should include the policy, programmatic, and infrastructure recommendations in this Plan.

04. RECOMMENDATIONS

Annual Collision Data Review

Reviewing bicycle and pedestrian related collisions and near-misses on an annual basis can help the City identify challenging intersections or corridors. This review should include an assessment of the existing infrastructure to determine whether improvements can be made to reduce the number of collisions in the community. This Plan recommends the City review bicycle and pedestrian related collision data on an annual basis to identify needed improvements.

Parent Surveys

Continue to gather school parent survey data as currently led by the School District. The National Center for Safe Routes to School provides a standard parent survey, collecting information on modes of travel, interest in walking or bicycling to school, and challenges to walking and bicycling to school. The information gathered from the parent surveys can help the City and School District provide programs that are attractive to parents. Parent surveys can also help measure parent attitudes and changes in attitude towards walking and bicycling to school.

Student Walking and Biking Counts

Student hand tallies are one way to count the number of students who walk, bicycle, take transit or carpool to school. The National Center for Safe Routes to School provides the standard tally form. It is recommended the School District continue to conduct student tallies on an annual basis. Counts can also be held on annual walking or bicycling to school events. These are an excellent way to track the number of students who walk or bicycle to school over time. Grant applications will often require this information.

Policy Changes

The following recommended changes include policies, operational changes, and municipal code revisions that support the goals of this Plan or address community-identified barriers to walking and bicycling in San Carlos.

Vision Zero Policy

Vision Zero is a traffic philosophy that rejects the idea that traffic crashes are accidents, and instead asserts that serious injuries or fatalities on the transportation system are preventable and unacceptable.

The more San Carlos understands where and why crashes happen, the more different departments can take actions to reduce them. The City can use this understanding to develop roadway designs that prioritize bicycling, walking, and other transportation facilities that enhance comfort and can be implemented quickly to adapt and respond to identified challenges.

Asking questions and analyzing questions about the causes and locations of crashes can help the City reprioritize funding for projects that target behaviors and locations that may be contributing to crashes:

- What are the total number of crashes (all modes)? Of these crashes, how many were fatal or severe injuries?

- What were the most striking contributing factors?
- How are the crashes affected by posted speed, road class, or other identifiable feature of the road?

This Plan recommends the City consider whether adoption of a Vision Zero Policy is an appropriate step to increase the City's existing commitment for all users of its diverse transportation systems. The Vision Zero Network has resources and case studies available to guide cities as they develop and implement Vision Zero Action Plans.

School Zone Speed Limits

On January 1, 2008, AB 321 took effect allowing local governments to extend school zones up to 1,000 feet and reduce speed limits within 500 feet of a school site to 15 mph in residential neighborhoods or on highways with speed limits of 30 mph or less.

At 15 mph, more than 90 percent of pedestrians are likely to survive a crash with only minor injuries. As speeds increase, however, crash severity increases dramatically. At 30 mph most crashes result in serious injuries to pedestrians, and nearly half may be fatal. At 40 mph, 90 percent of pedestrians will be killed in a crash. Reducing speeds even slightly can have a profound effect on safety for people walking and bicycling to school.

Cities that have already enacted this law include Berkeley, San Francisco, Los Angeles, and

Goleta. This Plan recommends the City enact this law around eligible schools. AB 321 requires engineering and traffic surveys to be conducted to indicate that the existing speed limit is not appropriate. In addition, San Carlos can work with local law enforcement to educate parents and drivers about the new policy and why certain school areas were selected.

Bicycle Parking for Large Events

The San Carlos Municipal Code currently addresses the procedures and requirements to apply for a Special Event Permit within the City. This Plan recommends revising the Special Events Permit Guidelines Section to require events expecting more than 5,000 attendees provide secure, attended bicycle parking for attendees at no charge. Key considerations include:

- A space that is enclosed and secured on three sides ("corral") must be provided, with the fourth side consisting of tables for checking in bicycles
- The corral must be in a visible and easily accessible location within one block of the event
- Bicycle parking must be offered for the full duration of the event, including the bicycle parking attendants having access to the location at least one hour before and one hour after the event for setup and break down
- Availability and location of free bicycle parking must be noted on all event promotion where transportation or directional information for

04. RECOMMENDATIONS

the event is advertised, in the same format and with an equal amount of space as parking and transportation information for other modes

- Bicycle parking must be attended and monitored at all times with a number of staff sufficient for the size of the event and whether attendees are expected to arrive for a single start time or arrive throughout the event
- Bicycles will be checked in and returned with a claim check to ensure the correct bicycle is released to each person, and bicycle valet attendants will record and share the number of bicycles parked at the event in order to better estimate the space needed for the following year
- The valet bicycle parking service provider shall have insurance; should the event sponsor provide the bicycle parking service, bicycles checked in must be insured against theft
- The City of Oakland operates a successful bicycle parking policy for large events, and may be a resource to San Carlos.

Shared Micromobility

Shared micromobility systems, such as bike share or scooter share, broaden the suite of choices that provide an alternative to the car. While the business model for privately-funded and operated dockless (or free-floating) programs is fairly new, early research and evaluation suggests that shared micromobility devices hold benefits for stimulating transit ridership and reducing congestion. For

example, 34 percent of scooter share riders in Portland, OR reported that they would have driven a personal vehicle or hailed a taxi/Uber/Lyft in place of their last e-scooter trip if the shared micromobility system was not in place. In Santa Monica, CA, this figure is over 50 percent according to their 2019 shared mobility device user survey¹.

² A study of Washington D.C.'s Capital Bikeshare and Metrorail systems found that every 10 percent increase in bike share ridership yields a 2.8 percent increase in transit ridership.³ A user survey conducted by scooter-company Lime, showed 39 percent of riders in the San Francisco, CA market reported that they used Lime to get to or from public transportation.⁴

The City of San Carlos would likely benefit from a responsibly deployed micromobility fleet. The City should work with one or more vendors to develop a pilot program that would allow a limited fleet of bikes and/or e-scooters within a defined geographic region for a fixed period (typically one-year or less). The pilot program would allow the city to examine and refine system characteristics including:

- Licensing and Permits: Cities have largely shifted away from procurement and exclusive contracts for establishing bike and scooter share. Licensing or permitting programs are used as a way to manage micromobility providers, including establishing the rules, responsibilities, and conduct of operators. Establishing a process for the fair and thorough vetting of vendors is an important first step in regulating for or

partnering to address the topics below.

- Vendor Fees: Cities issuing micromobility permits have the ability to set application fees, annual renewal fees, and per unit fees to establish funding for the City's oversight of the program and/or infrastructure upgrades.
- Fleet Size: Fleet size requirements allow cities to cap the number of micromobility units deployed, or develop variable fleet size policies that require vendors to remove or add units to achieve the desired average number of rides per unit per day.
- Parking Policies: Cities can build dockless mobility parking requirements into micromobility permits and contracts. These requirements can include mandatory response times to customer complaints on improperly parked units and penalty fees for failure to comply.
- Trail and Sidewalk Requirements: Cities that allow dockless mobility providers to operate have a diversity of policies on the legality of riding e-scooters and e-bikes on sidewalks and multiuse paths or trails. Prohibiting the use of electric micromobility devices on facilities shared with pedestrians is recommended to ensure safe and comfortable walking conditions.
- Investments in Bike Infrastructure: A micromobility pilot program should also consider whether safe and comfortable bike infrastructure is available for micromobility riders to use.

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- **Geographic Areas of Operation:** Cities can control where micromobility units are concentrated through two primary methods: system rebalancing requirements and geofenced operation boundaries. System rebalancing requirements allow cities to identify key locations or areas where units should be located after recharging. Geofenced operation boundaries allow cities to define zones where dockless mobility units should not be ridden. These boundaries are most often enforced by in-app warnings, accompanied by user penalty fees for riding outside of an approved area.

During the pilot phase data collection, including trip origins and destinations, routes, vehicle use, crash reports and complaints are collected and analyzed, in addition to structured community feedback. Pilot programs allow cities to remain flexible and provide an opportunity to adjust permit terms, consider proposals from different service providers, and incorporate community input into program planning before issuing a long-term permit program.

1 City of Santa Monica. (2019). City of Santa Monica Shared Mobility Device Pilot Program User Survey Results Conducted 01-25-2019 to 02-15-2019. https://www.smgov.net/uploadedFiles/Departments/PCD/Transportation/Shared-Mobility_UserSurveySummary_20190509_FINAL.PDF

2 City of Portland. (2018). 2018 E-Scooter Findings. <https://www.portlandoregon.gov/transportation/article/709719>

3 Ma, Ting & Liu, Chao & Erdogan, Sevgi. (2015). Bicycle Sharing and Transit: Does Capital Bikeshare Affect Metrorail Ridership in Washington, D.C.? https://www.researchgate.net/publication/271273828_Bicycle_Sharing_and_Transit_Does_Capital_Bikeshare_Affect_Metrorail_Ridership_in_Washington_DC

4 Lime. (2018). San Francisco Scooter Use Survey Results. <https://www.li.me/hubfs/Lime%20San%20Francisco%20Scooter%20Survey%20Findings.pdf>

05.

Implementation

Introduction

This chapter outlines a strategy towards implementation of the infrastructure projects and includes the following sections:

- Project Prioritization presents the method and data sources used to prioritize projects for implementation, along with a summary of the results
- Funding Strategy provides an overview of competitive funding sources and eligibilities for the projects in this Plan

The intent of prioritizing projects is to create a strategic list to guide implementation. The project list and prioritization results are flexible concepts that serve as guidelines. Over time as development occurs or other changes to land uses and the transportation network take place, this framework can be used to reprioritize remaining projects and continue pursuing implementation of this Plan.

Project Prioritization

With limited funding, the City of San Carlos has to decide where to prioritize building approximately 30 miles of bikeways, one mile of new and improved sidewalks, and 37 pedestrian safety improvements. The intent of evaluating projects is to create a strategic list to guide implementation. The project list and evaluation results are flexible concepts that serve as guidelines. Over time as development occurs or other changes to land uses and the transportation network take place, this framework can be used to reevaluate remaining projects and continue pursuing implementation of this Plan.

Methodology

As shown in **Table 11**, recommended projects were evaluated using four criteria that support the vision and goals of the Plan: connectivity, safety, comfort, and routes to school. Each criterion is based on one or more metric. Projects were evaluated on whether the project fully met, partially met, or not met the minimum thresholds. Projects that perform at the highest levels (fully or partially meet criteria) across multiple categories were ranking into three tiers: high, medium, and low. Projects that are in progress are not ranked.

Table 11: Criteria for Recommendation Prioritization

CATEGORY	METRIC
Connectivity	Projects that provide access to parks, library, or downtown are given priority.
	Projects that provide access to Caltrain are given priority.
Safety	Projects that are within 200 ft of reported bicycle- or pedestrian-related collisions between 2013 and 2017.
	Projects that are within 200 ft of reported severe or fatal bicycle- or pedestrian-related collisions between 2013 and 2017.
Comfort	BIKE ONLY Projects that implement a new low stress bikeway are given priority (including Class I, Class IIB, Class IV, and Class IIIB bicycle facilities).
	PED ONLY Projects that implement a pedestrian improvement along a high stress roadway (PLTS 3 or 4) are given priority.
Routes to School	Projects within ¼ mile of a school or along an identified walking/biking route to school are given priority.

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Priority Projects

Tables 12 through 14 outline the high, medium, and low priority recommended bikeways, sidewalks, and pedestrian spot improvements. **Figure 18** shows the high priority improvement and improvements that are in process. **Figure 19** shows the medium priority improvements. This Plan prioritizes building over nine miles of bikeways as high priority projects and 15 pedestrian safety improvement locations.

Table 12: Bikeway Prioritization

ROADWAY	START	END	PROPOSED CLASS	MILEAGE	PRIORITIZATION CATEGORY
Holly St Overpass	Industrial Rd	Holly St Overpass	1	0.1	In Progress
Holly St Overpass	San Carlos Ave	Airport Dr	1	0.5	In Progress
Cherry St	Cedar St	El Camino Real	3B	0.4	High
Elm St	Magnolia Ave	Eaton Ave	3B	1.4	High
Arroyo Ave	Tamarack Ave	El Camino Real	3B	0.8	High
San Carlos Ave	Vista Ave	El Camino Real	2B	0.6	High
San Carlos Ave	Club Dr	Wellington Dr	2B	0.7	High
San Carlos Ave	Cranfield Ave	Club Dr	2	0.1	High
Cedar St	Eaton Ave	San Carlos City Limits	3B	2.0	High
Old County Rd	San Carlos City Limit	San Carlos City Limits	4	1.9	High
Alley	Elm St	Laurel St	1	0.1	High
Saint Francis Way	Alameda de las Pulgas	El Camino Real	3B	1.0	High
El Camino Real	San Carlos City Limits	San Carlos City Limits	4	1.9	Medium
San Carlos Ave	El Camino Real	Old County Rd	1	0.04	Medium

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ROADWAY	START	END	PROPOSED CLASS	MILEAGE	PRIORITIZATION CATEGORY
Alameda de las Pulgas	San Carlos Ave	San Carlos City Limits	2B	1.6	Medium
Laurel St	San Carlos Ave	Cherry St	1	0.1	Medium
Laurel St Paseo	Laurel St	Walnut St	1	0.07	Medium
Crestview Dr	Brittan Ave	Club Dr	2B	1.3	Medium
Eaton Ave	Alameda De Las Pulgas	El Camino Real	3B	1.1	Medium
Howard Ave	El Camino Real	Industrial Rd	4	0.3	Medium
Howard Ave	Alameda De Las Pulgas	Laurel St	3B	0.9	Medium
Howard Ave	Laurel St	El Camino Real	2	0.09	Medium
Industrial Rd	San Carlos City Limits	Bing St	4	1.4	Medium
Brittan Ave	Cedar St	Elm St	2B	0.3	Low
Brittan Ave	Milano Wy	Crestview Dr	2	0.9	Low
Magnolia Ave/ Laurel St	Hull Dr	Cedar St	3B	0.6	Low
Hull Dr	Cedar St	El Camino Real	3B	0.3	Low
Melendy Dr	Alameda De Las Pulgas	Crestview Dr	3	1.4	Low
Club Dr	San Carlos Ave	Crestview Dr	2	1.2	Low

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ROADWAY	START	END	PROPOSED CLASS	MILEAGE	PRIORITIZATION CATEGORY
Pedestrian Pathway	Industrial St	Old County Rd	1	0.3	Low
Railroad Bend	Old County Rd	Industrial Rd	1	0.3	Low
Cordilleras Ave	Brittan Ave	San Carlos Ave	3	0.7	Low
Taylor Way	Old County Rd	Industrial Rd	3	0.3	Low
E San Carlos Ave	Industrial Rd	Holly St Overpass	3	0.1	Low
Brittan Ave	Milano Ave	Alameda de las Pulgas	2	0.3	Low
Commercial St	Old County Rd	Industrial Rd	2	0.3	Low

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Table 13: Sidewalk Prioritization

ROADWAY	START	END	PRIORITIZATION CATEGORY
San Carlos Ave	Wellington Dr	Alameda De Las Pulgas	In Progress
San Carlos Ave	Upland Ave	Cordilleras Ave	Medium
Brittan Ave	Milano Way	Rogers Ave	Low

Table 14: Pedestrian Improvement Prioritization

PROJECT	CROSS STREET A	CROSS STREET B	PRIORITIZATION CATEGORY
P2	San Carlos Ave	Upland Ave	In Progress
P3	San Carlos Ave	Alameda de las Pulgas	In Progress
P26	San Carlos Ave	Phelps Rd	In Progress
P1	San Carlos Ave	Cordilleras Ave	In Progress
P27	San Carlos Ave	Laurel St	High
P14	Industrial Ave	Center St	High
P13	Industrial Ave	Howard Ave	High
P12	Brittan Ave	Crestview Dr	High
P10	Brittan Ave	Cordilleras Ave	High
P33	Kenton Ave	San Carlos Ave	High
P5	Arroyo Ave	Chestnut St	High
P7	El Camino Real	San Carlos Ave	High
P6	El Camino Real	Hull Dr	High
P8	El Camino Real	Arroyo Ave	High
P9	El Camino Real	Brittan Ave	High
P19	Holly Ave	El Camino Real	High

05. IMPLEMENTATION

PROJECT	CROSS STREET A	CROSS STREET B	PRIORITIZATION CATEGORY
P28	Alameda De Las Pulgas	Howard Ave	High
P36	Olive Ave	Elm St	High
P16	El Camino Real	Cherry St	Medium
P4	Alameda de las Pulgas	St Francis Wy	Medium
P29	Alameda De Las Pulgas	Carmelita Dr	Medium
P16	El Camino Real	Belmont Ave	Medium
P17	El Camino Real	White Oak Way	Medium
P18	Holly Ave	Old County Rd	Medium
P22	Cedar St	Arroyo Ave	Medium
P30	Chestnut St	Cherry St	Medium
P31	Orange Ave	Arroyo Ave	Medium
P32	Cordilleras Ave	Arroyo Ave	Medium
P34	Brittan Ave	Trail Connecting Parks	Medium
P35	Brittan Ave	Greenwood Ave	Medium
P37	Cherry St	Cedar St	Medium
P11	Brittan Ave	Woodland Ave	Low
P25	Cordilleras Ave	Belle Ave	Low
P15	Magnolia Ave	Elm St	Low
P23	Woodland Ave	Chestnut St	Low
P24	Tamarack Ave	Belle Ave	Low
P20	Holly Ave	Laurel St	Low

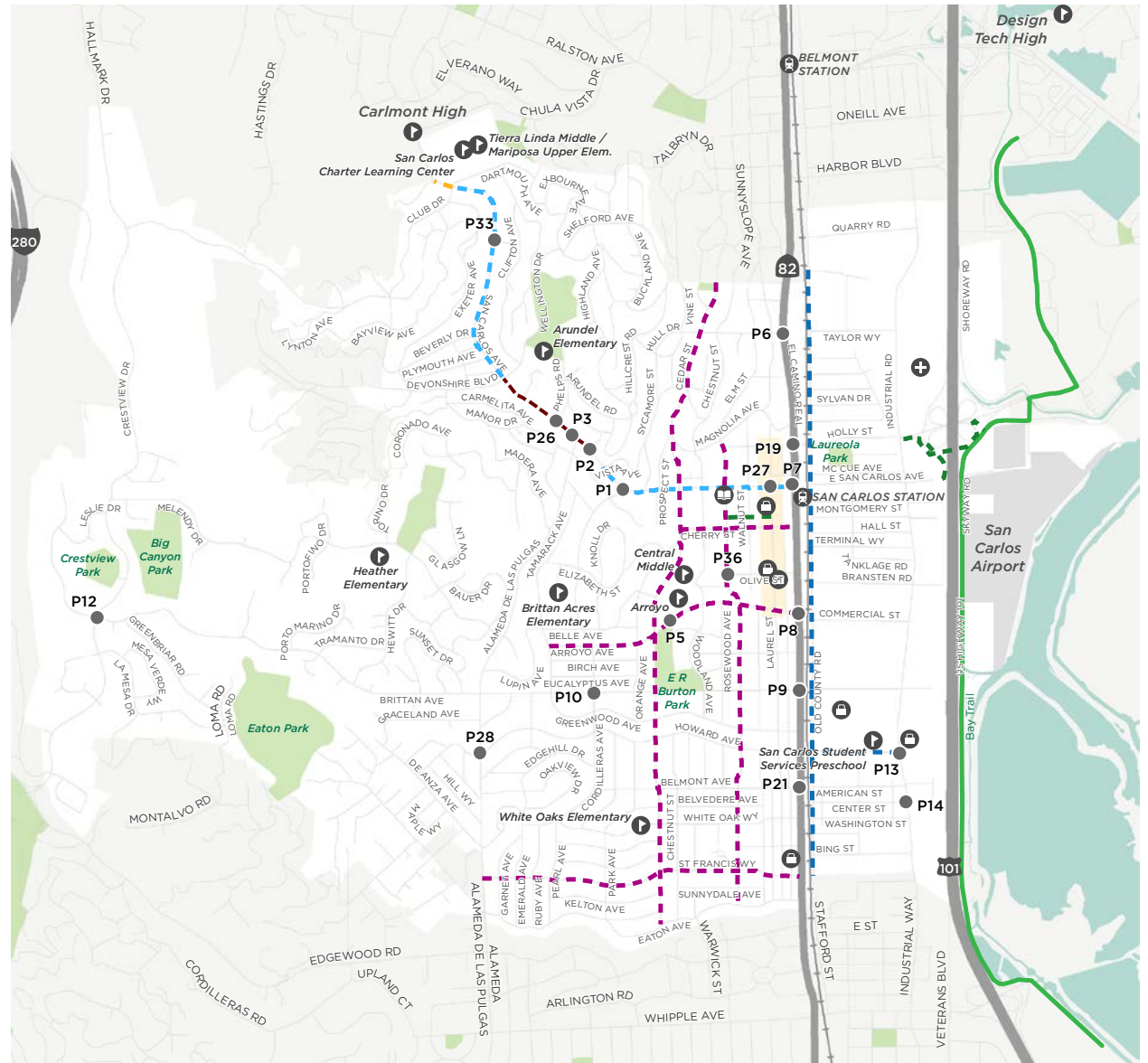
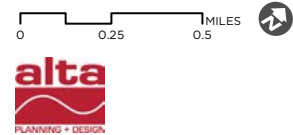
05. IMPLEMENTATION

Figure 18:
High Priority Projects

- Spot Improvement
- Class I Shared-Use Path
- Class II Bicycle Lane
- Class IIB Buffered Bike Lane
- Class IIIB Bicycle Boulevard
- Class IV Separated Bikeway
- Sidewalk

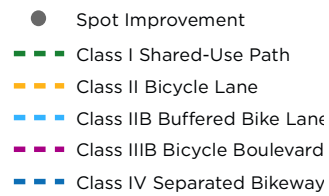
Destinations + Boundaries

- ✉ Post Office
- 🛒 Shopping
- 🏥 Hospital
- 📖 Library
- 🎓 School
- 🚉 Caltrain Station
- 🌿 Bay Trail
- 🌳 Park
- 🏘️ Downtown



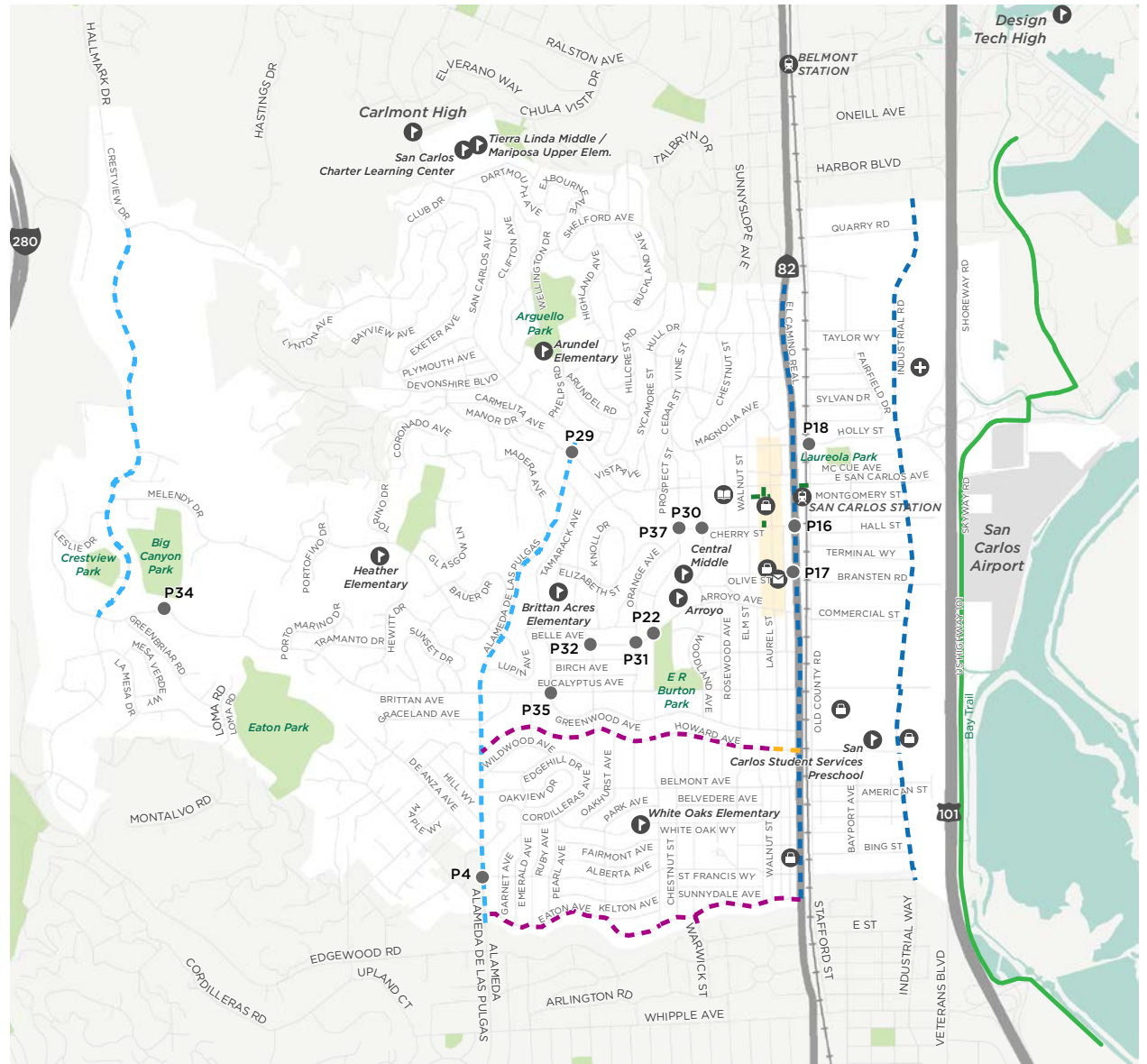
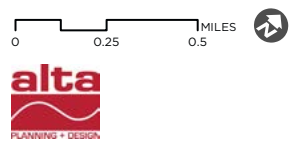
05. IMPLEMENTATION

Figure 19:
Medium Priority Projects



Destinations + Boundaries

- Post Office
- Shopping
- Hospital
- Library
- School
- Caltrain Station
- Bay Trail
- Park
- Downtown



Funding Strategy

There are a variety of potential funding sources including local, regional, state, and federal. The City should also take advantage of private contributions in developing the proposed system. This could include a variety of resources such as volunteer labor during construction or monetary donations towards specific improvements. The funding sources considered appropriate for San Carlos are listed in **Table 15** and discussed in detail in **Appendix B: Funding Sources**.

Table 15: Funding Sources

FUNDING SOURCES
Federal Funding Sources
TIGER (Transportation Investment Generating Economic Recovery)
State Funding Sources
Active Transportation Program (ATP)
Solutions for Congested Corridors Programs
Highway Safety Improvement Program (HSIP)
Sustainable Transportation Planning Grant Program (STP)
Office of Traffic Safety: National Safety Program 405(h) Nonmotorized Safety
Recreational Trails Program
Affordable Housing and Sustainable Communities Program
Highway Safety Improvement Program
Urban Greening Grants
Local Partnership Program
Road Maintenance and Rehabilitation Program
Regional Funding Sources
Bicycle Facilities Grant Program
One Bay Area Grant Program 2
Measure A Pedestrian and Bicycle Program
Transportation Funds for Clean Air

FUNDING SOURCES

Local Funding Sources

Assessment Districts

Impact Fees

Open Space District

Non-Traditional and Private Funding Sources

California Conservation Corps (CCC)

Rails to Trails Conservancy (RTC)

Grant and Foundation Opportunities

Adopt-A-Trail/Path Programs

Memorial Funds

Revenue-Producing Operations

APPENDIX A

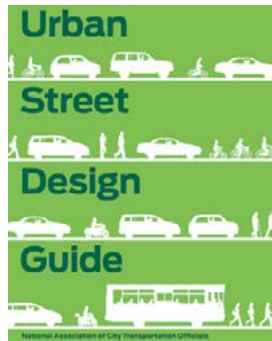
Design Guidelines

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Guidance Basis

The sections that follow serve as an inventory of pedestrian and bicycle design treatments and provide guidelines for their development. These treatments and design guidelines are important because they represent the tools for creating a bicycle-friendly, safe, accessible community. The guidelines are not, however, a substitute for a more thorough evaluation by a professional upon implementation. The following standards and guidelines are referred to in this guide:

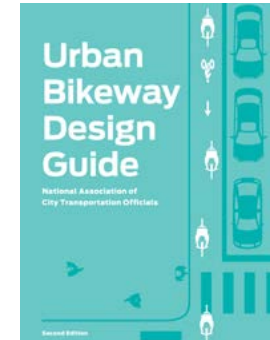
National Guidance



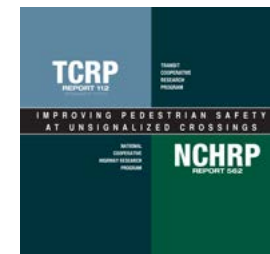
A blueprint for designing 21st century streets, the NACTO Urban Street Design Guide (2013) unveils the toolbox and tactics cities use to make streets safer, more livable, and more economically vibrant. The Guide outlines both a clear vision for complete streets and a basic road map for how to bring them to fruition. The document charts the principles and practices of the nation's foremost engineers, planners, and designers working in cities.



Separated Bike Lane Planning and Design Guide (2015) provides national guidance on the planning and design of separated bike lane facilities. Released by the Federal Highway Administration (FHWA), this guide documents best practices as demonstrated around the U.S., and offers ideas on future areas of research, evaluation, and design flexibility.



The National Association of City Transportation Officials' (NACTO) Urban Bikeway Design Guide (2012) provides cities with state-of-the-practice solutions that can help create complete streets that are safe and enjoyable for bicyclists. The designs were developed by cities for cities, since unique urban streets require innovative solutions. In August 2013, the Federal Highway Administration issued a memorandum officially supporting use of the document.

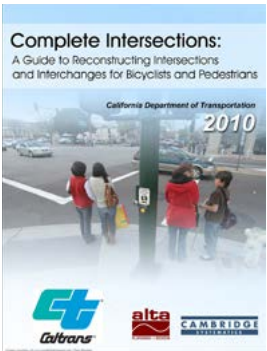


NCHRP's Improving Pedestrian Safety at Unsignalized Crossings Report recommends engineering treatments to improve pedestrian safety at unsignalized locations with high speeds and traffic volumes.

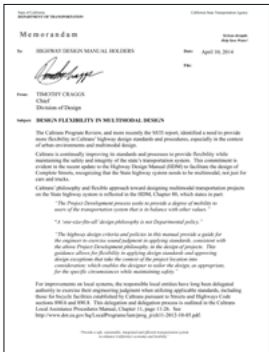
California Guidance



The California Manual on Uniform Traffic Control Devices (CAMUTCD) (2014) is an amended version of the FHWA MUTCD 2009 edition modified for use in California. While standards presented in the CA MUTCD substantially conform to the FHWA MUTCD, the state of California follows local practices, laws and requirements with regards to signing, striping and other traffic control devices.



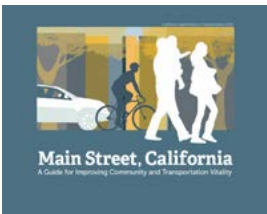
Complete Intersections: A Guide to Reconstructing Intersections and Interchanges for Bicyclists and Pedestrians (2010) is a reference guide that presents information and concepts related to improving conditions for bicyclists and pedestrians at major intersections and interchanges. The guide can be used to inform minor signage and striping changes to intersections, as well as major changes and designs for new intersections.



The Caltrans Memo: Design Flexibility in Multimodal Design (2014) encourages flexibility in highway design. The memo stated that “Publications such as the NACTO “Urban Street Design Guide” and “Urban Bikeway Design Guide,” ... are resources that Caltrans and local entities can reference when making planning and design decisions on the State highway system and local streets and roads.”



The California Highway Design Manual (HDM) (Updated 2015) establishes uniform policies and procedures to carry out highway design functions for the California Department of Transportation.



Main Street, California: A Guide for Improving Community and Transportation Vitality (2013) reflects California’s current manuals and policies that improve multi-modal access, livability and sustainability within the transportation system. The guide recognizes the overlapping and sometimes competing needs of main streets.



The Caltrans resource Class IV Bikeway Guidance (2018) provides enhanced guidance for two-way separated bikeways, with added information on transit stops and separated bikeways adjacent to street parking. It also provides a discussion of maintenance using Caltrans equipment.

User Design Dimensions

The purpose of this section is to provide the facility designer with an understanding of how bicyclists operate and how their bicycle influences that operation. Bicyclists, by nature, are much more affected by poor facility design, construction, and maintenance practices than motor vehicle drivers.

Bicyclists lack the protection from the elements and roadway hazards provided by an automobile's structure and safety features. By understanding the unique characteristics and needs of bicyclists, a facility designer can provide quality facilities and minimize user risk.

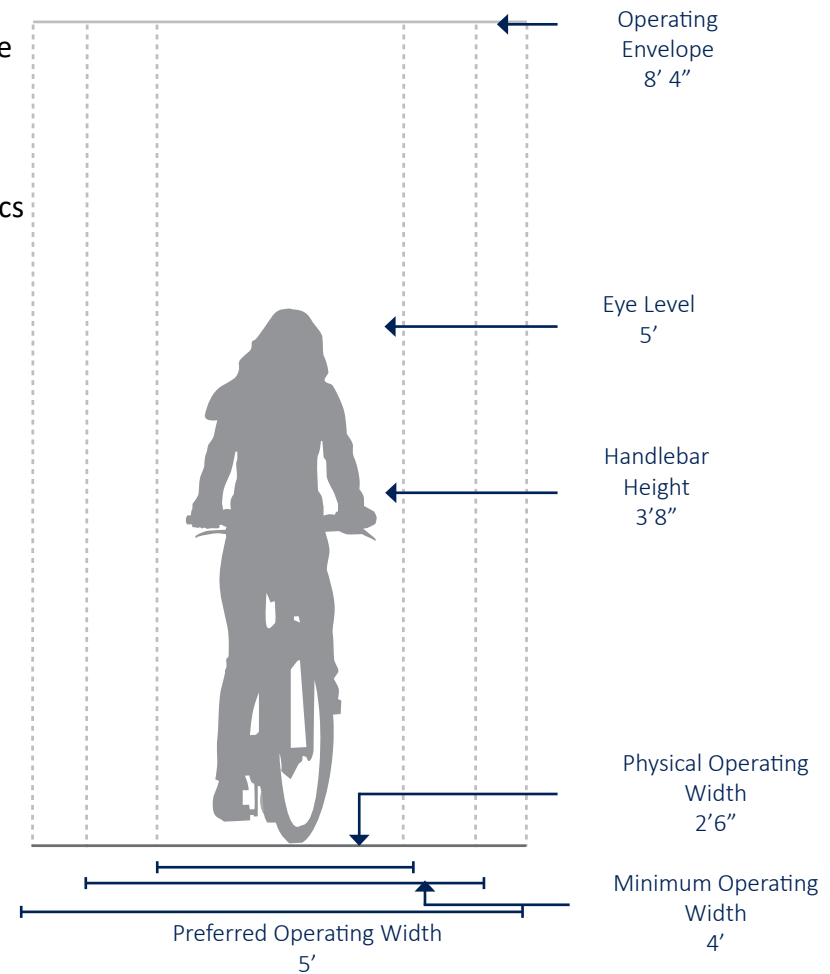
Bicycle as a Design Vehicle

Similar to motor vehicles, bicyclists and their bicycles exist in a variety of sizes and configurations. These variations occur in the types of vehicle (such as a conventional bicycle, a recumbent bicycle or a tricycle), and behavioral characteristics (such as the comfort level of the bicyclist). The design of a bikeway should consider reasonably expected bicycle types on the facility and utilize the appropriate dimensions.

The figure to the right illustrates the operating space and physical dimensions of a typical adult bicyclist, which are the basis for typical facility design. Bicyclists require clear space to operate within a facility. This is why the minimum operating width is greater than the physical dimensions of the bicyclist. Bicyclists prefer five feet or more operating width, although four feet may be minimally acceptable.

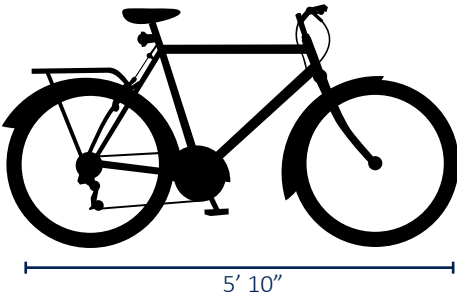
In addition to the design dimensions of a typical bicycle, there are many other commonly used pedal-driven cycles and accessories to consider when planning and designing bicycle facilities. The most common types include tandem bicycles, recumbent bicycles, and trailer accessories. The figure to the left summarizes the typical dimensions for bicycle types.

Bicycle Rider - Typical Dimensions

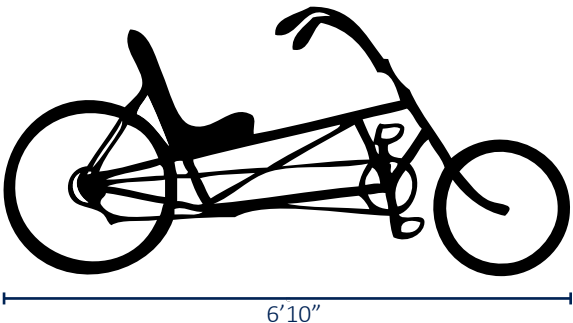
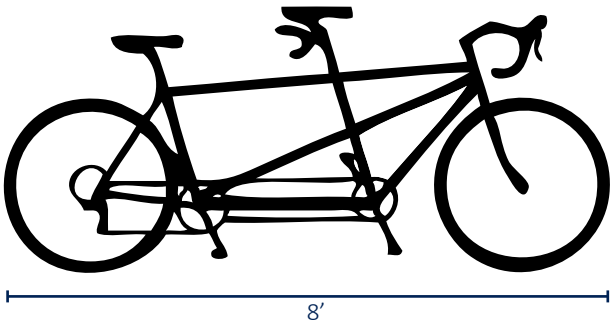


DESIGN GUIDELINES

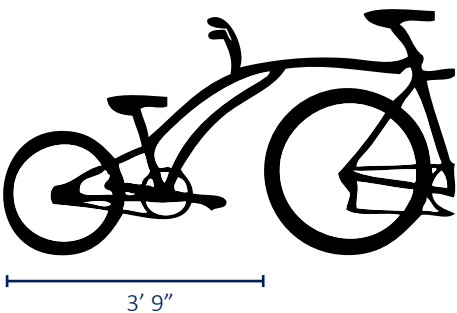
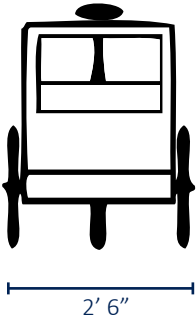
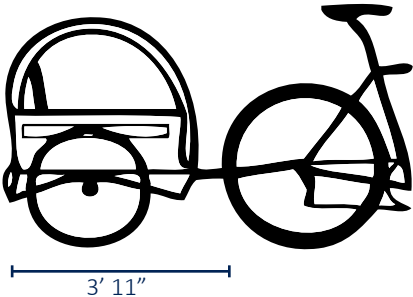
The expected speed that different types of bicyclists can maintain under various conditions also influences the design of facilities such as shared-use paths. The table below provides typical bicyclist speeds for a variety of conditions.



Bicycle as Design Vehicle - Design Speed Expectations



Bicycle Type	Feature	Typical Speed
Upright Adult Bicyclist	Paved level surfacing	8-12 mph*
	Crossing Intersections	10 mph
	Downhill	30 mph
	Uphill	5 -12 mph
Recumbent Bicyclist	Paved level surfacing	18 mph



Source: AASHTO Guide for the Development of Bicycle Facilities, 4th Edition

Bicyclist User Type

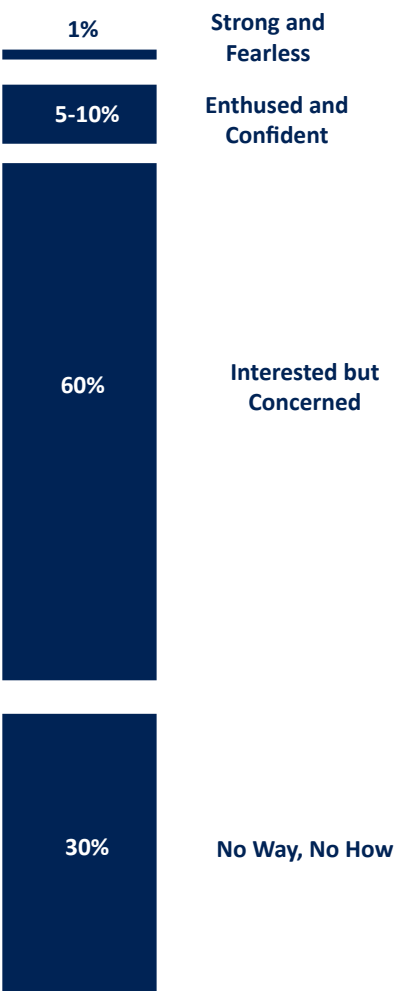
The 2015 FHWA Separated Bike Lane Planning and Design Guide encourages designers to identify their rider type based on the level of comfort and skill of the rider (Casual vs Experienced). A user-type framework for understanding a potential rider’s willingness to bike is illustrated in the figure below. Developed by planners in Portland, OR and supported by research, this classification identifies four distinct types of bicyclists.

Strong and Fearless – Characterized by bicyclists that will typically ride anywhere regardless of roadway conditions or weather. These bicyclists can ride faster than other user types, prefer direct routes and will typically choose roadway connections (even if shared with vehicles) over separate bicycle facilities such as shared-use paths.

Enthusied and Confident - This user group encompasses bicyclists who are fairly comfortable riding on all types of bikeways but usually choose low traffic streets or shared-use paths when available. These bicyclists may deviate from a more direct route in favor of a preferred facility type. This group includes all kinds of bicyclists such as commuters, recreationalists, racers and utilitarian bicyclists.

Interested but Concerned – This user type comprises the bulk of the cycling population and represents bicyclists who typically only ride a bicycle on low traffic streets or shared-use paths under favorable weather conditions. These bicyclists perceive significant barriers to their increased use of cycling, specifically traffic and other safety issues. These people may become “Enthusied & Confident” with encouragement, education and experience.

No Way, No How – Persons in this category are not bicyclists, and perceive severe safety issues with riding in traffic. Some people in this group may eventually become more regular cyclists with time and education. A significant portion of these people will not ride a bicycle under any circumstances.



Pedestrian Design Needs

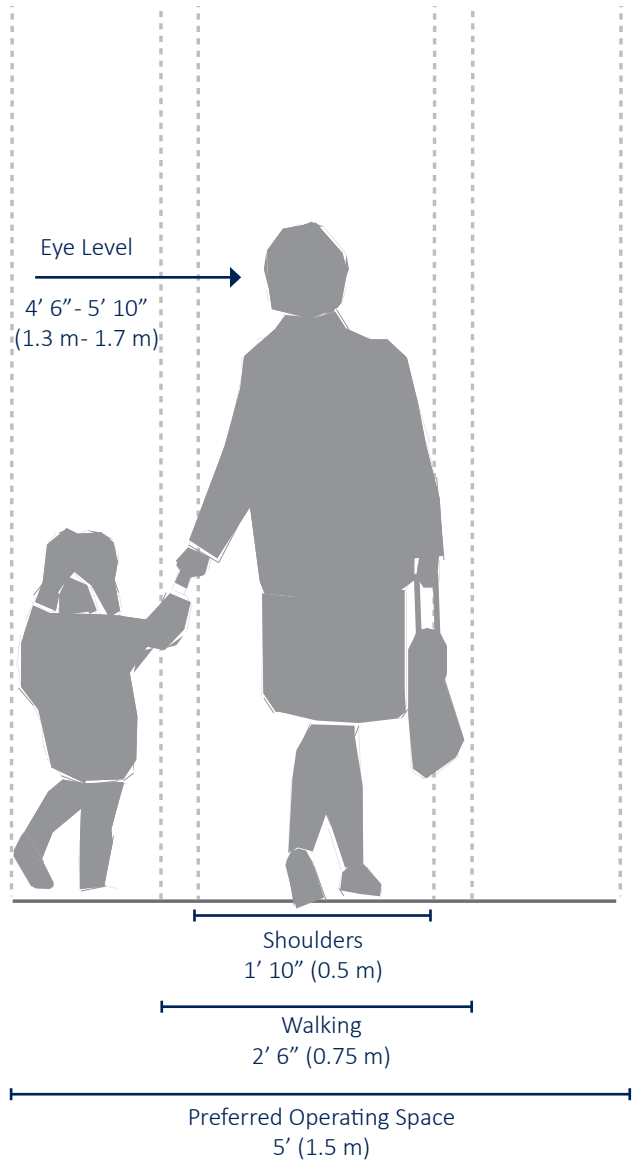
Types of Pedestrians

Pedestrians have a variety of characteristics and the transportation network should accommodate a variety of needs, abilities, and possible impairments. Age is one major factor that affects pedestrians’ physical characteristics, walking speed, and environmental perception. Children have low eye height and walk at slower speeds than adults. They also perceive the environment differently at various stages of their cognitive development. Older adults walk more slowly and may require assistive devices for walking stability, sight, and hearing. The table to the right summarizes common pedestrian characteristics for various age groups.

The MUTCD recommends a normal walking speed of 3.5 feet per second when calculating the pedestrian clearance interval at traffic signals. The walking speed can drop to 3 feet per second for areas with older populations and persons with mobility impairments. While the type and degree of mobility impairment varies greatly across the population, the transportation system should accommodate these users to the greatest reasonable extent.

Additional References and Guidelines

AASHTO. *Guide for the Planning, Design, and Operation of Pedestrian Facilities*, Exhibit 2-1. 2004.



Pedestrian Characteristics by Age

Age	Characteristics
0-4	Learning to walk Requires constant adult supervision Developing peripheral vision and depth perception
5-8	Increasing independence, but still requires supervision Poor depth perception
9-13	Susceptible to “darting out” in roadways Insufficient judgment Sense of invulnerability
14-18	Improved awareness of traffic environment Insufficient judgment
19-40	Active, aware of traffic environment
41-65	Slowing of reflexes
65+	Difficulty crossing street Vision loss Difficulty hearing vehicles approaching from behind

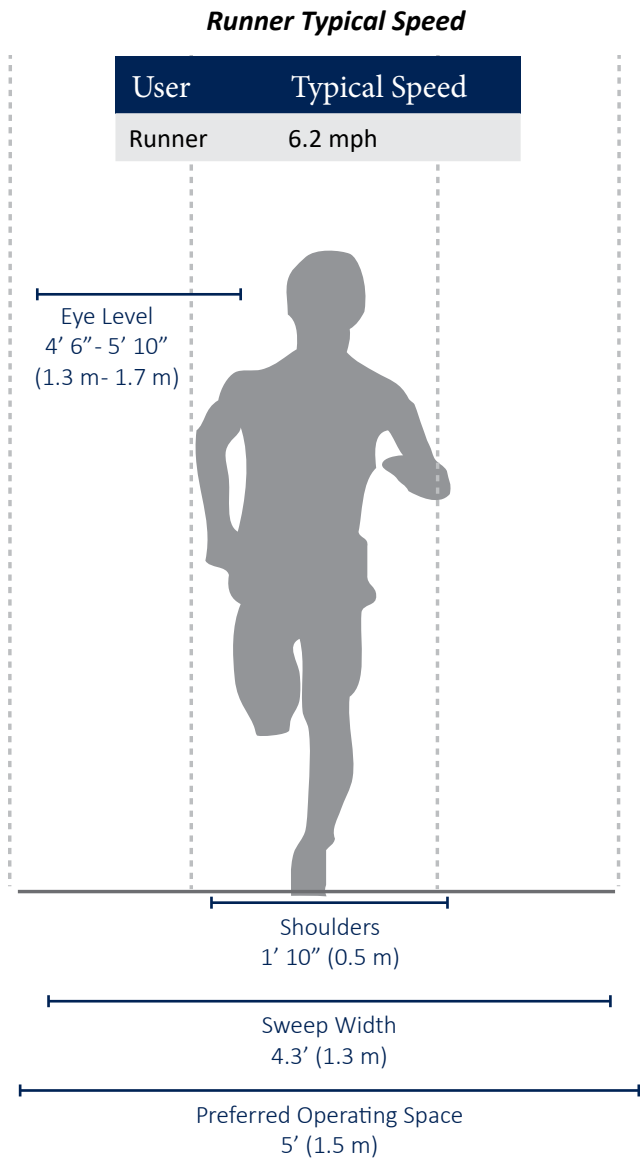
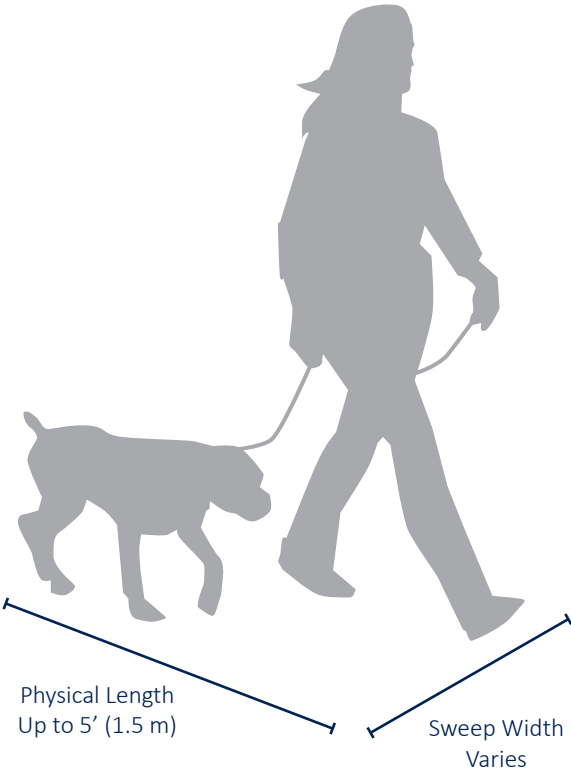
Design Needs of Dog Walkers

Dog walking is a common and anticipated use on shared use paths. Dog sizes vary largely, as does leash length and walking style, leading to wide variation in possible design dimensions.

Shared-use paths designed to accommodate wheelchair users are likely to provide the necessary dimensions for the average dog walker. Amenities such as dog waste stations may enhance conditions for dog walkers.

Design Needs of Runners

Running is an important recreation and fitness activity commonly performed on shared use paths. Many runners prefer softer surfaces (such as rubber, bare earth or crushed rock) to reduce impact. Runners can change their speed and direction frequently. If high volumes are expected, controlled interaction or separation of different types of users should be considered.



Additional References and Guidelines

FHWA. *Characteristics of Emerging Road and Trail Users and Their Safety*. (2004).

DESIGN GUIDELINES

Design Needs of Wheelchair Users

As the American population ages, the number of people using mobility assistive devices (such as manual wheelchairs, powered wheelchairs) increases.

Manual wheelchairs are self-propelled devices. Users propel themselves using push rims attached to the rear wheels. Braking is done through resisting wheel movement with the hands or arm. Alternatively, a second individual can control the wheelchair using handles attached to the back of the chair.

Power wheelchairs use battery power to move the wheelchair. The size and weight of power wheelchairs limit their ability to negotiate obstacles without a ramp. Various control units are available that enable users to control the wheelchair movement, based on their ability (e.g., joystick control, breath controlled, etc).

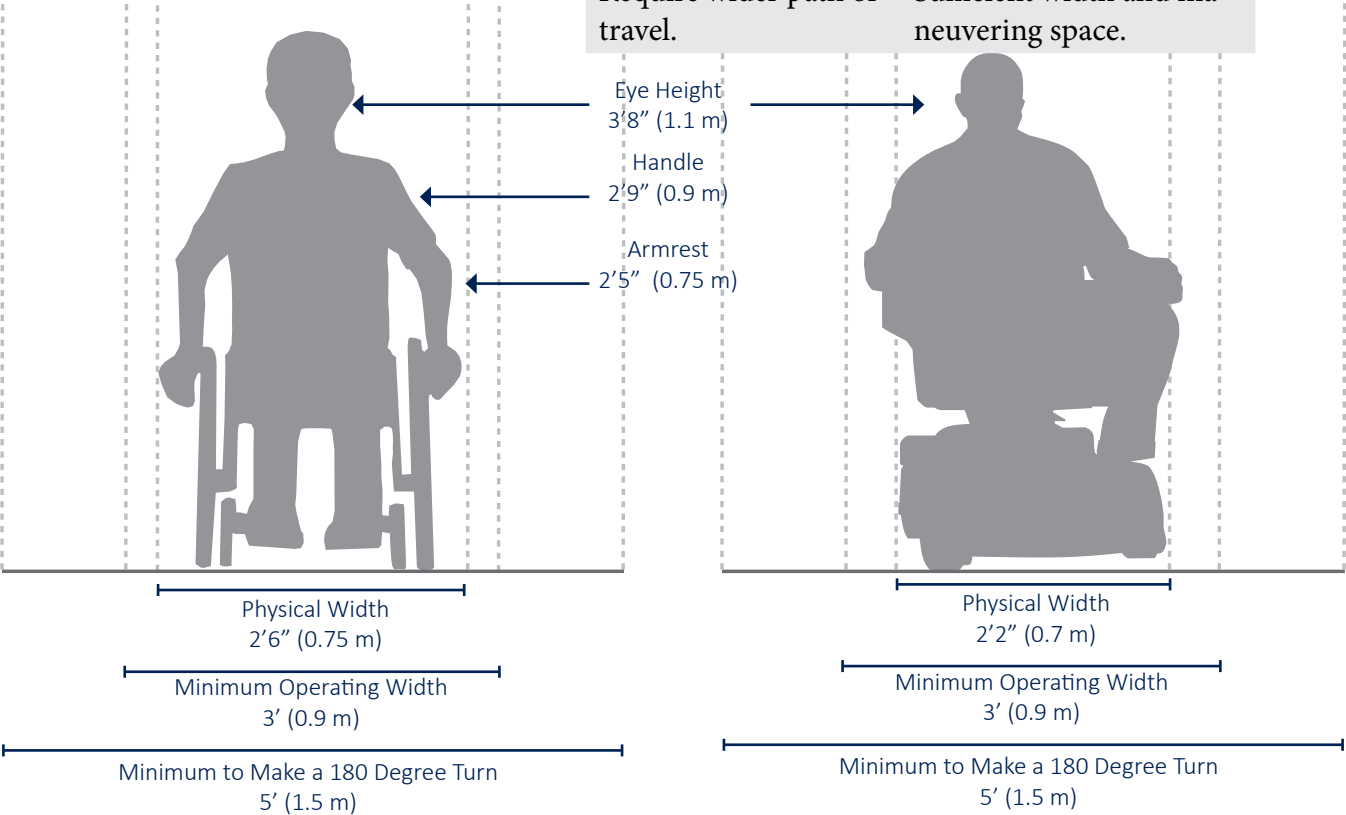
Maneuvering around a turn requires additional space for wheelchair devices. Providing adequate space for 180 degree turns at appropriate locations is an important element for accessible design.

Wheelchair User Typical Speed

User	Typical Speed
Manual Wheelchair	3.6 mph
Power Wheelchair	6.8 mph

Wheelchair User Design Considerations

Effect on Mobility	Design Solution
Difficulty propelling over uneven or soft surfaces.	Firm, stable surfaces and structures, including ramps or beveled edges.
Cross-slopes cause wheelchairs to veer downhill.	Cross-slopes of less than two percent.
Require wider path of travel.	Sufficient width and maneuvering space.



Additional References and Guidelines

FHWA. *Characteristics of Emerging Road and Trail Users and Their Safety*. 2004. USDOJ. *2010 ADA Standards for Accessible Design*. 2010.

PEDESTRIAN FACILITIES

Pedestrian Crossing Location and Facility Selection

Crossing Treatment Selection

The specific type of treatment at a crossing may range from a simple marked crosswalk to full traffic signals or grade separated crossings. Crosswalk lines should not typically be used by themselves, and appropriate selection of crossing enhancements should be evaluated in an engineering study. The engineering study should consider the number of lanes, the presence of a median, the distance from adjacent signalized intersections, the pedestrian volumes and delays, the average daily traffic (ADT), the posted or statutory speed limit or 85th-percentile speed, the geometry of the location, the possible consolidation of multiple crossing points, the availability of street lighting, and other appropriate factors.

Midblock Crossings

Midblock crossings are an important street design element for pedestrians. They can provide a legal crossing at locations where pedestrians want to travel, and can be safer than crossings at intersections because traffic is only moving in two directions. Locations where midblock crossings should be considered include:

- Long blocks (longer than 600 feet) with destinations on both sides of the street.
- Locations with heavy pedestrian traffic, such as schools, shopping centers.
- At midblock transit stops, where transit riders must cross the street on one leg of their journey.

PEDESTRIAN CROSSING CONTEXTUAL GUIDANCE <small>At unsignalized locations</small>	Local Streets 15-25 mph		Collector Streets 25-30 mph			Arterial Streets 30-45 mph							
	2 lane	3 lane	2 lane	2 lane with median refuge	3 lane	2 lane	2 lane with median refuge	3 lane	4 lane	4 lane with median refuge	5 lane	6 lane	6 lane with median refuge
FACILITY TYPE													
Crosswalk Only (high visibility)	✓	✓	EJ	EJ	X	EJ	EJ	X	X	X	X	X	X
Crosswalk with warning signage and yield lines	EJ	✓	✓	✓	✓	EJ	EJ	EJ	X	X	X	X	X
Active Warning Beacon (RRFB)	X	EJ	✓	✓	✓	✓	✓	✓	X	✓	X	X	X
Hybrid Beacon	X	X	EJ	EJ	EJ	EJ	✓	✓	✓	✓	✓	✓	✓
Full Traffic Signal	X	X	EJ	EJ	EJ	EJ	EJ	EJ	✓	✓	✓	✓	✓
Grade separation	X	X	EJ	EJ	EJ	X	EJ	EJ	EJ	EJ	EJ	✓	✓

LEGEND	
Most Desirable	✓
Engineering Judgement	EJ
Not Recommended	X

Sidewalk Zones & Widths

Sidewalks are the most fundamental element of the walking network, as they provide an area for pedestrian travel separated from vehicle traffic. Providing adequate and accessible facilities can lead to increased numbers of people walking, improved accessibility, and the creation of social space.



The curbside lane can act as a flexible space to further buffer the sidewalk from moving traffic, and may be used for a bike lane. Curb extensions and bike corrals may occupy this space where appropriate.

The buffer zone, also called the furnishing or landscaping zone, buffers pedestrians from the adjacent roadway, and is also the area where elements such as street trees, signal poles, signs, and other street furniture are properly located.

The through zone is the area intended for pedestrian travel. This zone should be entirely free of permanent and temporary objects.

Wide through zones are needed in downtown areas or where pedestrian flows are high.

The frontage zone allows pedestrians a comfortable “shy” distance from the building fronts. It provides opportunities for window shopping, to place signs, planters, or chairs.

Curbside Lane

Buffer Zone

Pedestrian Through Zone

Frontage Zone

Typical Application

- Wider sidewalks should be installed near schools, at transit stops, in downtown areas, or anywhere high concentrations of pedestrians exist.
- At transit stops, an 8 ft by 5 ft clear space is required for accessible passenger boarding/alighting at the front door location per ADA requirements.
- Sidewalks should be continuous on both sides of urban commercial streets, and should be required in areas of moderate residential density.
- When retrofitting gaps in the sidewalk network, locations near transit stops, schools, parks, public buildings, and other areas with high concentrations of pedestrians should be the highest priority.

Street Classification	Parking Lane/Enhancement Zone	Buffer Zone	Pedestrian Through Zone	Frontage Zone
Local Streets	Varies	4 - 6 ft	6 ft	N/A
Downtown and Pedestrian Priority Areas	Varies	4 - 6 ft	12 ft	2.5 - 10 ft
Arterials and Collectors	Varies	4 - 6 ft	6 - 8 ft	2.5 - 5 ft

Materials and Maintenance

Sidewalks are typically constructed out of concrete and are separated from the roadway by a curb or gutter and sometimes a landscaped boulevard. Less expensive walkways constructed of asphalt, crushed stone, or other stabilized surfaces may be appropriate. Ensure accessibility and properly maintain all surfaces regularly. Surfaces must be firm, stable, and slip resistant. Colored, patterned, or stamped concrete can add distinctive visual appeal.

Approximate Cost

Cost of standard sidewalks range from about \$25 per square foot for concrete sidewalk. This cost can increase with additional right-of-way acquisition or addition of landscaping, lighting or other aesthetic features. As an interim measure, an asphalt concrete path can be placed until such time that a standard sidewalk can be built. The cost of asphalt path can be less than half the cost of a standard sidewalk.

Green Infrastructure

Green infrastructure treats and slows runoff from impervious surface areas, such as roadways, sidewalks, and buildings. Sustainable stormwater strategies may include bioretention swales, rain gardens, tree box filters, and pervious pavements (pervious concrete, asphalt and pavers). Green infrastructure should be considered as a part of all bike and pedestrian improvements such as bike boulevards or other streetscape improvements as they also have the potential to improve the level of comfort and attractiveness of the street.

Typical Application

- Install in areas without conventional stormwater systems that are prone to flooding to improve drainage and reduce costs compared to installing traditional gutter and drainage systems.
- Use green infrastructure to provide an ecological and aesthetic enhancement of traditional traffic speed and volume control measures, such as along a bicycle boulevard corridor.
- Bioswales and rain gardens are appropriate at curb extensions and along planting strips.
- Street trees and plantings can be placed in medians, chicanes, and other locations.
- Pervious pavers can be used along sidewalks, street furniture zones, parking lanes, gutter strips, or entire roadways. They are not likely to provide traffic calming benefit on bicycle boulevards.

Design Features

Bioswales

- Bioswales are shallow depressions with vegetation designed to capture, treat, and infiltrate stormwater runoff by reducing velocity and purifying the water while recharging the underlying groundwater table.
- In order to meet the minimum criteria for infiltration rates, bioswales are designed to pass 5-10 inches of rain water per hour. The overflow/ bypass drain system should be approximately 6 inches above the soil surface to manage heavier rainfall.

Pervious Pavement

- In areas where landscaping such as swales are less desired or feasible, pervious pavement can also effectively capture and treat stormwater runoff.

- The desired storage volume and intended drain time is determined by the depth of the pervious layer, void space, and the infiltration rate of underlying soils. An underdrain system must be used to treat overflow, or drain excess runoff to the municipal sewer system, and allow the facility to drain within 48 hours.



Pedestrian-Only Street

A pedestrian-only street is designed for exclusive use by pedestrians and experienced at a human scale with various amenities and pedestrian-oriented spaces.

Typical Application

- Streets in which pedestrian volume is high and vehicular traffic is restricted
- In corridors with commercial activity on both edges of the street
- Should be situated in high-density, mixed-use office or commercial areas where pedestrian volumes are high

Design Features

- Minimum clear paths should be maintained to allow emergency vehicle access
- Prohibit parking and vehicular traffic to ensure that clear paths remain unobstructed
- Use durable and slip-resistant materials. Provide accessibility ramps and tactile paving to assist the visually impaired
- Add street furniture, artwork, seating, tables, benches, trees, landscaping, cycle racks, and water fountains to add character and support a range of activities
- Must be well connected to collective transit, cycle routes, and walking paths; access from side streets or through streets should offer multiple options to move in and out of the corridor, keeping the space permeable
- Provide signs that encourage cyclists to disembark and walk with their cycles



3rd Street in Santa Monica

Marked Crosswalks

A marked crosswalk signals to motorists that they must stop or yield to pedestrians and encourages pedestrians to cross at designated locations. Installing crosswalks alone will not necessarily make crossings safer; especially on multi-lane roadways.

At mid-block locations, crosswalks must be marked to establish a legal crossing.

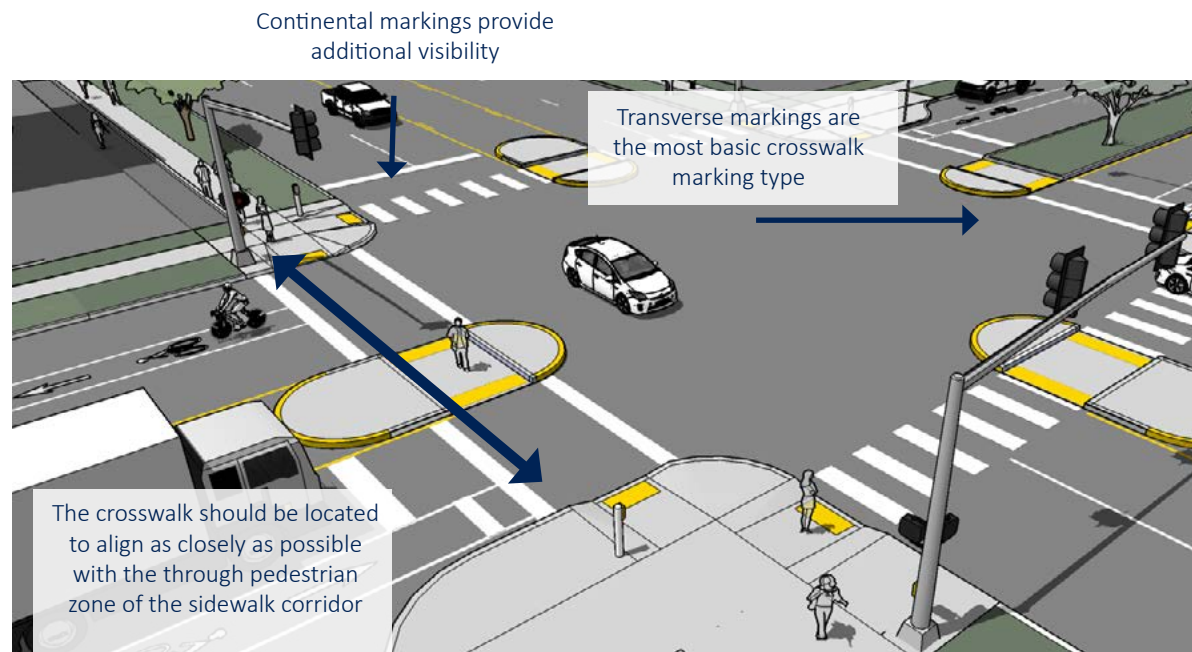
Typical Application

At signalized intersections, all crosswalks should be marked. At unsignalized intersections, crosswalks may be marked under the following conditions:

- At a complex intersection, to orient pedestrians in finding their way across.
- At an offset intersection, to show pedestrians the shortest route across traffic with the least exposure to vehicular traffic and traffic conflicts.
- At an intersection with visibility constraints, to position pedestrians where they can best be seen by oncoming traffic.
- At an intersection within a school zone on a walking route.

Design Features

- Because the effectiveness of marked crossings depends entirely on their visibility, maintaining marked crossings should be a high priority.
- Thermoplastic markings offer increased durability than conventional paint.



Further Considerations

Pedestrians are sensitive to out-of-direction travel, and reasonable accommodations should be made to make crossings both convenient at locations with adequate visibility.

High-visibility crosswalk markings are easier for drivers to see and can be easier to maintain than standard transverse striped markings. They should be used at crossings with high pedestrian use or where vulnerable pedestrians are expected, including: school crossings, across arterial streets for pedestrian-only signals, at mid-block crosswalks, at intersections where there is expected high pedestrian use, and where the crossing is not controlled by signals or stop signs. High-visibility crosswalks are not appropriate for all locations. Other crosswalk marking patterns are provided for in the CA MUTCD.

Some cities prohibit omitting or removing a marked crosswalk at intersections in order to require a three-stage pedestrian crossing. Intersections with three-stage crossings lead to arduous and increased crossing distances, pedestrian frustration, encourages jaywalking, and exhibits modal bias favoring motor vehicle level-of-service over other modes. There are circumstances when only three crosswalks are utilized and typically occur at or near interchanges and freeway ramps.



High-visibility marked “Continental” crosswalk at Stony Point Road and Stony Circle

Materials and Maintenance

Because the effectiveness of marked crossings depends entirely on their visibility, maintaining marked crossings should be a high priority. Thermoplastic markings offer increased durability than conventional paint.

Approximate Cost

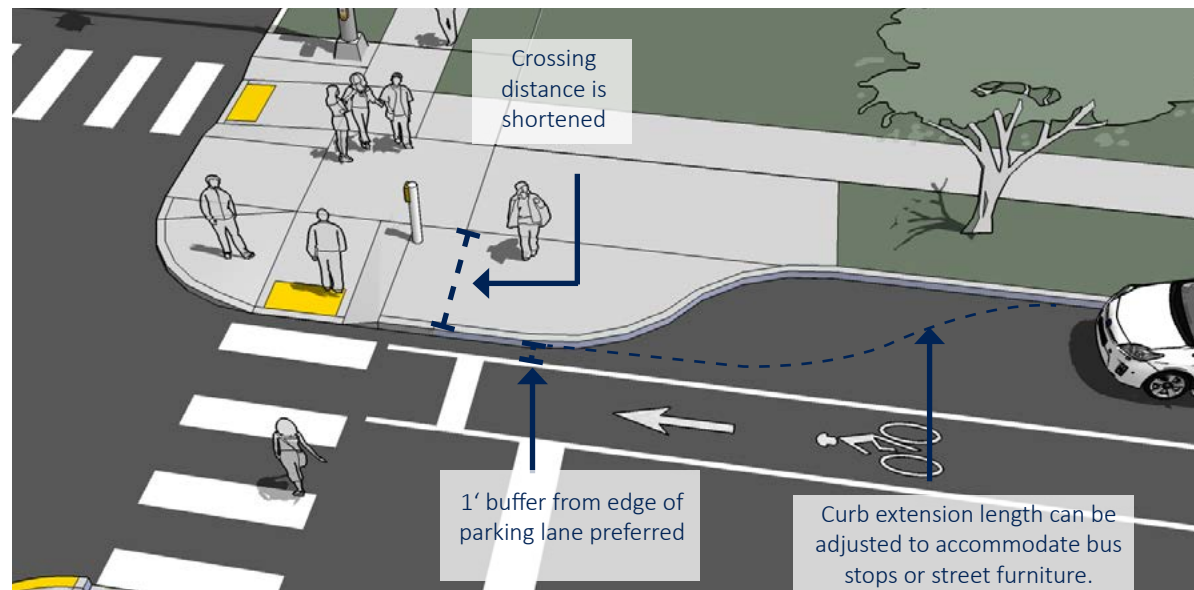
Depending on the type of material used, width of the crossing and width of the roadway, approximate installation costs are \$500 for a regular striped crosswalk, \$1,000 for a Continental crosswalk, and \$8,000 for a patterned concrete crosswalk. In addition, the cost of a curb ramp is about \$5,000-\$10,000 per ramp.

Due to various number of crosswalk styles in use, signing standards, color and aesthetics, other factors will affect the final cost.

Maintenance of markings should also be considered.

Curb Extensions

Curb extensions minimize pedestrian exposure during crossing by shortening crossing distance and giving pedestrians a better chance to see and be seen before committing to crossing. They are appropriate for any crosswalk where it is desirable to shorten the crossing distance and there is a parking lane adjacent to the curb.



Design Features

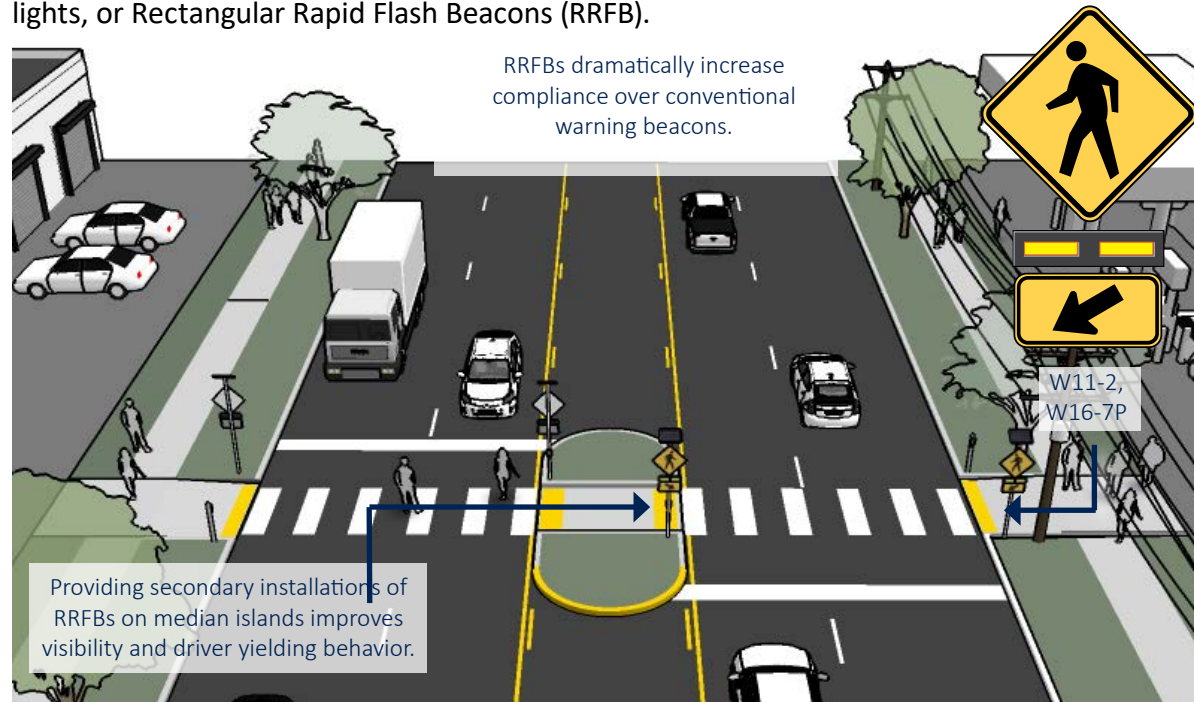
- In most cases, the curb extensions should be designed to transition between the extended curb and the running curb in the shortest practicable distance.
- For purposes of efficient street sweeping, the minimum radius for the reverse curves of the transition is 10 feet and the two radii should be balanced to be nearly equal.
- Curb extensions should terminate one foot short of the parking lane to maximize bicyclist safety.
- Planted curb extensions may be designed as a bioswale, a vegetated system for stormwater management.
- Turning performance of larger vehicles including buses may be impacted by curb extensions

Typical Application

- At signalized intersections with marked crosswalks should be marked.
- At unsignalized intersections with marked crosswalks.
- At an intersection with visibility constraints, to position pedestrians where they can best be seen by oncoming traffic.
- At an intersection within a school zone on a walking route.

Active Warning Beacons

Active warning beacons are user actuated illuminated devices designed to increase motor vehicle yielding compliance at crossings of multi-lane or high volume roadways. Types of active warning beacons include conventional circular yellow flashing beacons, in-roadway warning lights, or Rectangular Rapid Flash Beacons (RRFB).



Design Features

- Warning beacons shall not be used at crosswalks controlled by YIELD signs, STOP signs, or traffic signals.
- Warning beacons shall initiate operation based on pedestrian or bicyclist actuation and shall cease operation at a predetermined time after actuation or, with passive detection, after the pedestrian or bicyclist clears the crosswalk.

Typical Application

- At marked crosswalks where increased pedestrian visibility is needed. RRFBs are a preferred beacon treatment for enhanced crossings on bike and pedestrian networks.
- RRFBs have the most increased compliance of all the warning beacon enhancement options. A study of the effectiveness of going from a no-beacon arrangement to a two-beacon RRFB installation increased yielding from 18 percent to 81 percent.

BICYCLE FACILITIES

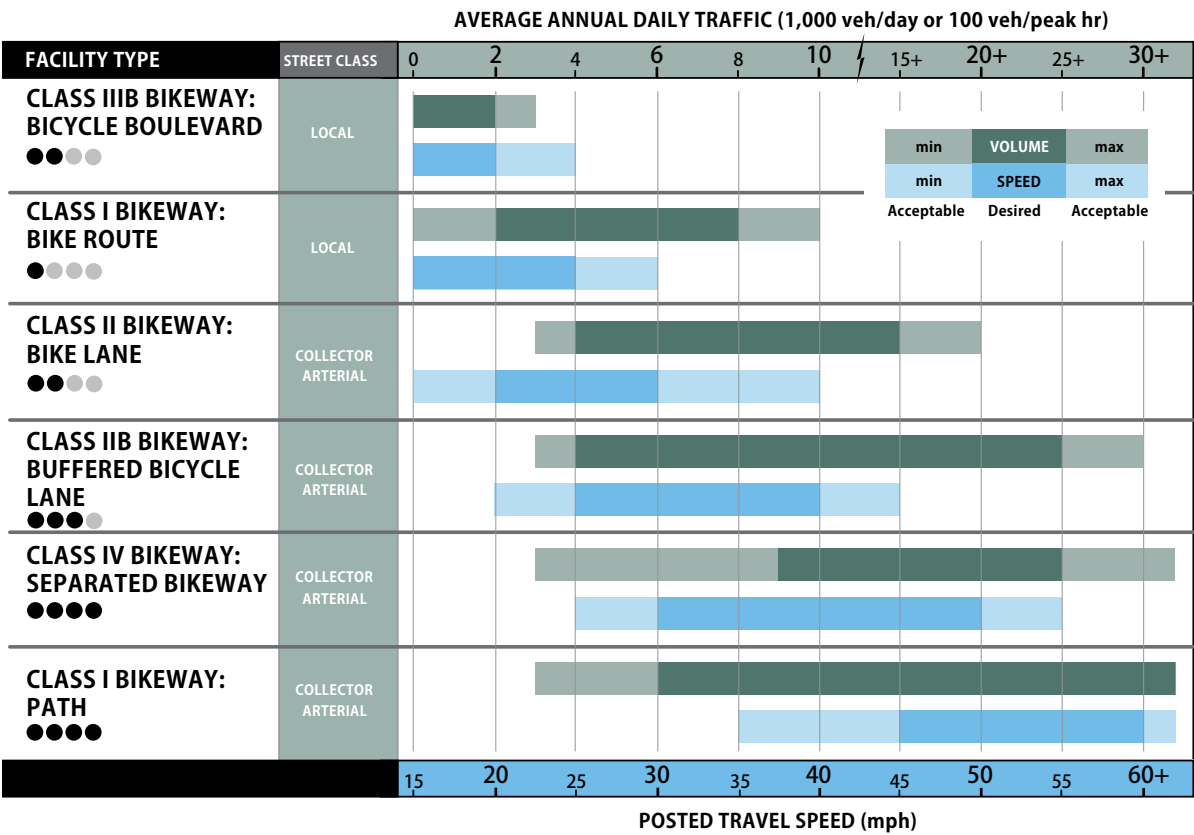
Facility Selection

Selecting the best bikeway facility type for a given roadway can be challenging, due to the range of factors that influence bicycle users’ comfort and safety. There is a significant impact on bicycling comfort when the speed differential between bicyclists and motor vehicle traffic is high and motor vehicle traffic volumes are high.

Facility Selection Table

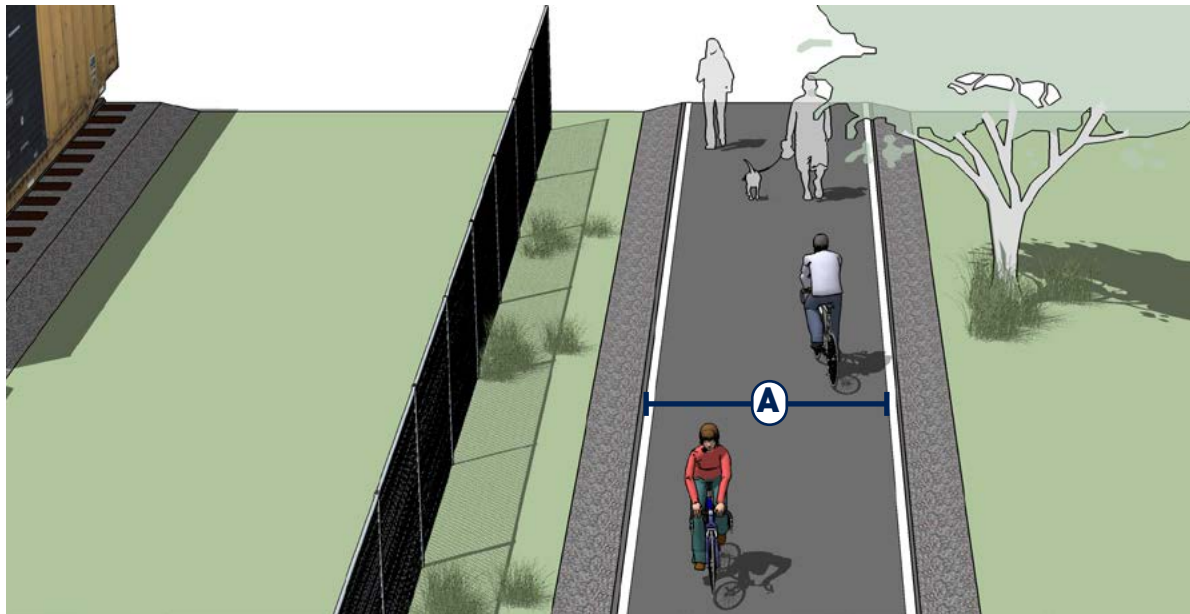
As a starting point to identify a preferred facility, the chart below can be used to determine the recommended type of bikeway to be provided in particular roadway speed and volume situations. To use this chart, identify the appropriate daily traffic volume and travel speed on the existing or proposed roadway, and locate the facility types indicated by those key variables.

Other factors beyond speed and volume which affect facility selection include traffic mix of automobiles and heavy vehicles, the presence of on-street parking, intersection density, surrounding land use, and roadway sight distance. These factors are not included in the facility selection chart below, but should always be considered in the facility selection and design process.



Shared Use Path (Class I)

Shared use paths (Class I) are off-street facilities that can provide a desirable transportation and recreation connection for users of all skill levels who prefer separation from traffic. They often provide low-stress connections to local and regional attractions that may be difficult, or not be possible on the street network.



Design Features

- Ⓐ 8 feet is the absolute minimum width (with 2 foot shoulders) allowed for a two-way bicycle path and is only recommended for constrained situations (Caltrans Design Manual).
- 10 feet is recommended in most situations and will be adequate for moderate to heavy use.
- 12 feet is recommended for heavy use situations with high concentrations of multiple users. A separate track (5 foot minimum) can be provided for pedestrian use.

Typical Application

- In abandoned rail corridors (commonly referred to as Rails-to-Trails or Rail-Trails).
- In active rail corridors, trails can be built adjacent to active railroads (referred to as Rails-with-Trails).
- In utility corridors, such as powerline and sewer corridors.
- In waterway corridors, such as along canals, drainage ditches, rivers, and creeks.
- Along roadways.

DESIGN GUIDELINES

Lateral Clearance

- A 2 foot or greater shoulder on both sides of the path should be provided. An additional foot of lateral clearance (total of 3 feet) is required by the MUTCD for the installation of signage or other furnishings.
- If bollards are used at intersections and access points, they should be colored brightly and/or supplemented with reflective materials to be visible at night.

Overhead Clearance

- Clearance to overhead obstructions should be an 8 foot minimum, with 10 feet recommended.

Striping

- When striping is required, use a 4 inch dashed yellow centerline stripe with 4 inch solid white edge lines.
- Solid centerlines can be provided on tight or blind corners, and on the approaches to roadway crossings.

Materials and Maintenance

Shared use paths must be regularly maintained so that they are free of potholes, cracks, root lift, and debris. Signage and lighting should also be regularly maintained to ensure shared use path users feel comfortable, especially where visibility is limited.

Adjacent landscaping should be regularly pruned, to allow adequate sightlines, daylight, and pedestrian-scale lighting, and so as not to obstruct the path of travel of trail users.

Approximate Cost

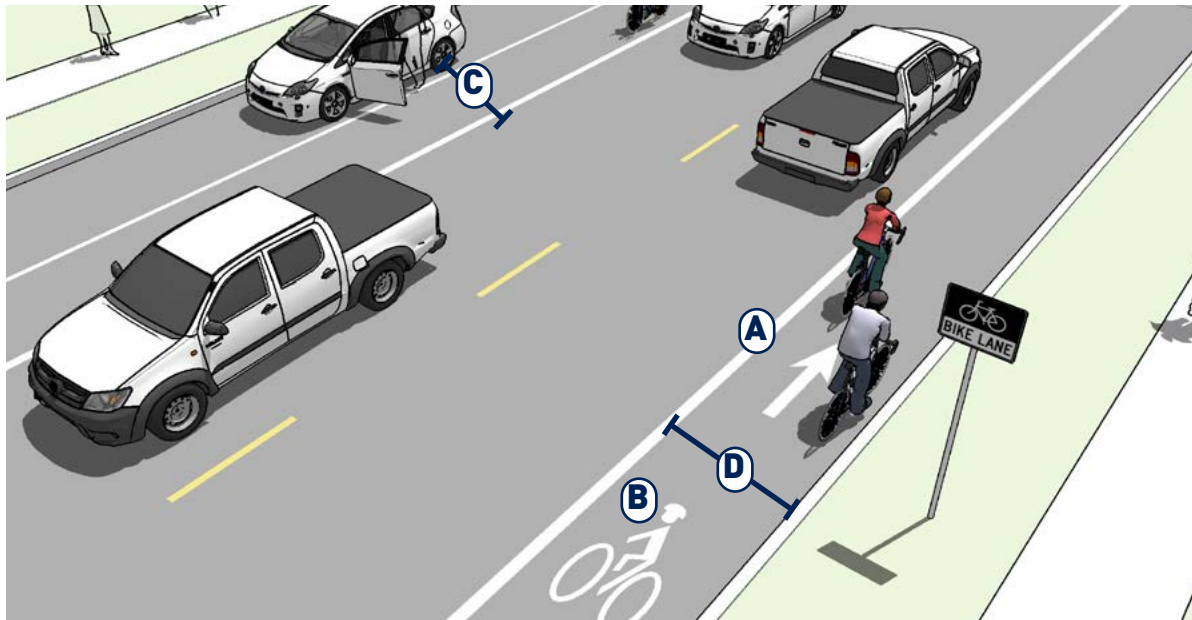
The cost of a shared use path can vary widely, but typical costs are between \$750,000 per mile to \$4 million per mile. These costs vary with materials, such as asphalt, concrete, boardwalk and other paving materials, lighting, and ROW acquisition.



Prince Memorial Greenway connects users to downtown Santa Rosa. Source: Peter Stetson.

On-Street Bicycle Lanes (Class II)

On-street bike lanes (Class II) designate an exclusive space for bicyclists through the use of pavement markings and signs. The bike lane is located directly adjacent to motor vehicle travel lanes and is used in the same direction as motor vehicle traffic. Bike lanes are typically on the right side of the street, between the adjacent travel lane and curb, road edge or parking lane.



Design Features

- (A)** Mark inside line with 6" stripe. Mark 4" parking lane line or "Ts".
- (B)** Include a bicycle lane marking (CA MUTCD FIGURE 9C-3) at the beginning of blocks and at regular intervals along the route (CA MUTCD 9C.04).
- (C)** 6 feet width preferred adjacent to on-street parking (5 feet min.).
- (D)** 6 feet preferred adjacent to curb and gutter (5 feet min.) or 4 feet more than the gutter pan width.

Typical Application

- Bike lanes may be used on any street with adequate space, but are most effective on streets with moderate traffic volumes greater than or equal to 6,000 ADT (with a greater than 3,000 ADT min.).
- Bike lanes are most appropriate on streets with low to moderate speeds of 25 mph or more.
- Appropriate for skilled adult riders on most streets.
- May be appropriate for children when configured as 6+ feet wide lanes on lower-speed, lower-volume streets with one lane in each direction.

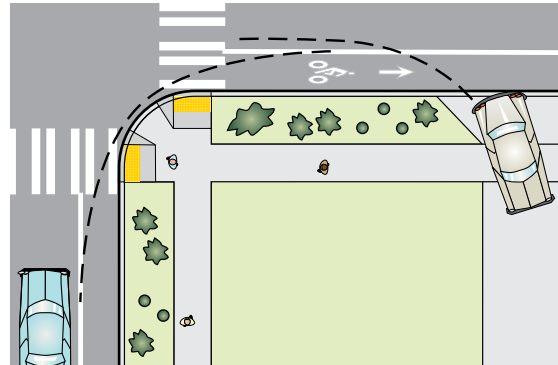
Further Considerations

- On high speed streets (greater than or equal to 40 mph) the minimum bike lane should be 6 feet.
- On streets where bicyclists passing each other is to be expected, where high volumes of bicyclists are present, or where added comfort is desired, consider providing extra wide bike lanes up to 7 feet wide, or configure as a buffered bicycle lane.
- It may be desirable to reduce the width of general purpose travel lanes in order to add or widen bicycle lanes.
- On multi-lane and/or high speed streets, the most appropriate bicycle facility to provide for user comfort may be buffered bicycle lanes or physically separated bicycle lanes.

Manhole Covers and Grates

- Manhole surfaces should be manufactured with a shallow surface texture in the form of a tight, nonlinear pattern.
- If manholes or other utility access boxes are to be located in bike lanes within 50 feet of intersections or within 20 feet of driveways or other bicycle access points, special manufactured permanent nonstick surfaces will be required to ensure a controlled travel surface for cyclists breaking or turning.

Place Bike Lane Symbols to Reduce Wear



Bike lane word, symbol, and/or arrow markings (MUTCD Figure 9C-3) shall be placed outside of the motor vehicle tread path in order to minimize wear from the motor vehicle path (NACTO 2012).

- Manholes, drainage grates, or other obstacles should be set flush with the paved roadway. Roadway surface inconsistencies pose a threat to safe riding conditions for bicyclists. Construction of manholes, access panels or other drainage elements will be constructed with no variation in the surface. The maximum allowable tolerance in vertical roadway surface will be 1/4 of an inch.

Bicycle Lane



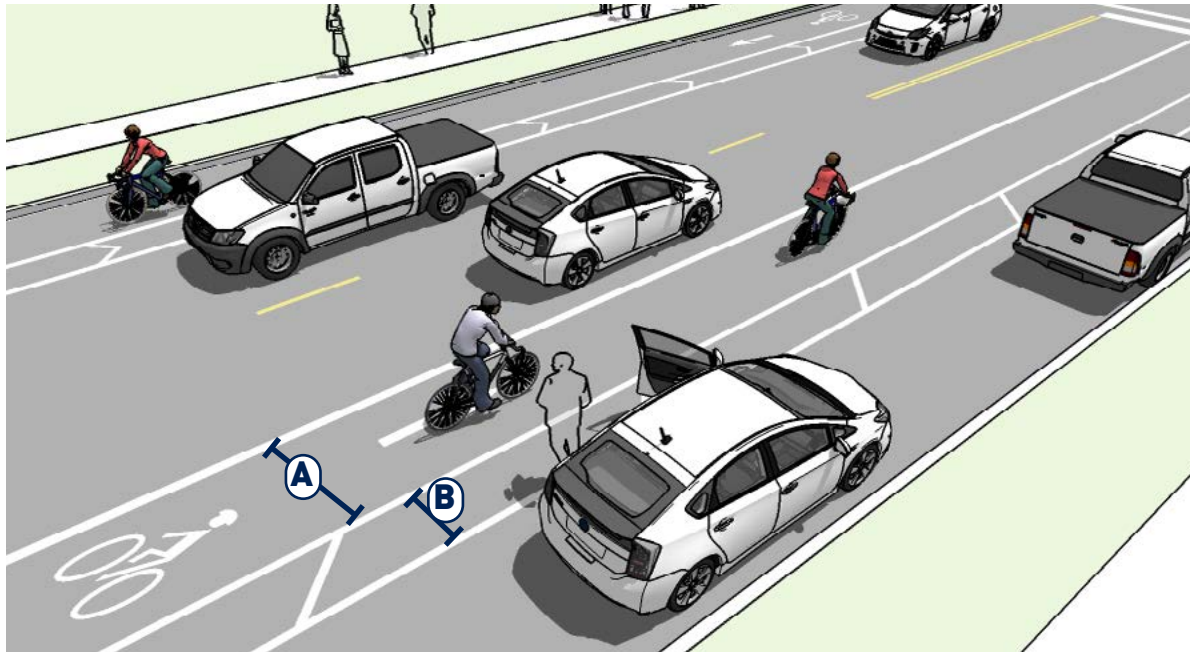
Bicycle lanes provide an exclusive space, but may be subject to unwanted encroachment by motor vehicles.

Approximate Cost

The cost for installing bicycle lanes will depend on the implementation approach. Typical costs are \$80,000 per mile for restriping up to \$425,000 for roadway grinding, overlay and slurry seal.

Buffered Bicycle Lanes (Class IIB)

Buffered bike lanes (Class IIB) are conventional bicycle lanes paired with a designated buffer space, separating the bicycle lane from the adjacent motor vehicle travel lane and/or parking lane.



Design Features

- A** The minimum bicycle travel area (not including buffer) is 5 feet wide.
- B** Buffers should be at least 18" wide. If buffer area is 4 feet or wider, white chevron or diagonal markings should be used (CA MUTCD 9C-104).
 - For clarity at driveways or minor street crossings, consider dashed striping with green fill to highlight potential conflict areas.
 - There is no standard for whether the buffer is configured on the parking side, the travel side, or a combination of both.

Typical Application

- Anywhere a conventional bike lane is being considered.
- On streets with high speeds and high volumes or high truck volumes.
- On streets with extra lanes or lane width.
- Appropriate for skilled adult riders on most streets.

Further Considerations

- Color may be used within the lane to discourage motorists from entering the buffered lane.
- A study of buffered bicycle lanes found that, in order to make the facilities successful, there needs to also be driver education, improved signage and proper pavement markings.
- On multi-lane streets with high vehicle speeds, the most appropriate bicycle facility to provide for user comfort may be physically separated bike lanes.
- NCHRP Report #766 recommends, when space is limited, installing a buffer space between the parking lane and bicycle lane where on-street parking is permitted rather than between the bicycle lane and vehicle travel lane.

Approximate Cost

The cost for installing buffered bicycle lanes will depend on the implementation approach. Typical costs are \$130,000 per mile for restriping, up to \$425,000 for roadway grinding, overlay, and slurry seal. However, the cost of large-scale bicycle treatments will vary greatly due to differences in project specifications and the scale and length of the treatment.

Buffered Bicycle Lanes



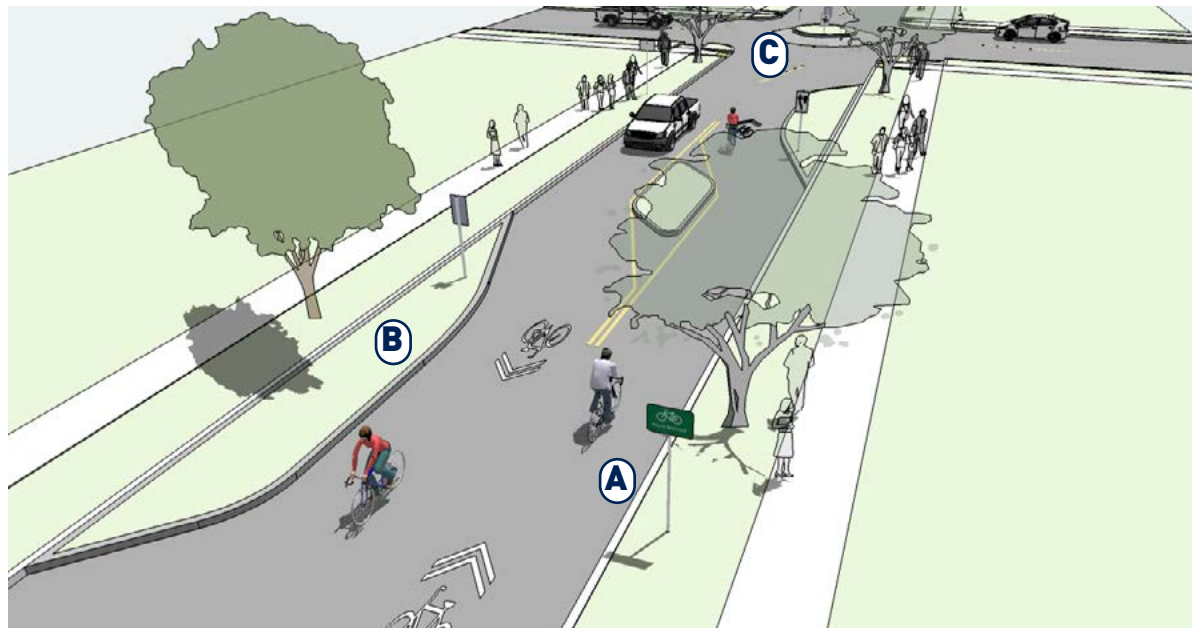
The use of pavement markings delineates space for cyclists to ride in a comfortable facility.



The use of pavement markings delineates space for cyclists to ride in a comfortable facility.

Bicycle Boulevards (Class IIIB)

Bicycle boulevards (Class IIIB) are low-volume, low-speed streets modified to enhance bicyclist comfort by using treatments such as signage, pavement markings, traffic calming and/or traffic reduction, and intersection modifications. These treatments allow through movements of bicyclists while discouraging similar through-trips by non-local motorized traffic.



Design Features

- A** Signs and pavement markings are the minimum treatments necessary to designate a street as a bicycle boulevard.
- B** Implement volume control treatments based on the context of the bicycle boulevard, using engineering judgment. Target motor vehicle volumes typically below 2,000 vehicles per day.
- C** Intersection crossings should be designed to enhance safety and minimize delay for bicyclists and pedestrians.

Typical Application

- Parallel with and in close proximity to major thoroughfares (1/4 mile or less).
- Follow a desire line for bicycle travel that is ideally long and relatively continuous (2-5 miles).
- Avoid alignments with excessive zigzag or circuitous routing. The bikeway should have less than 10 percent out of direction travel compared to shortest path of primary corridor.
- Streets with travel speeds at 25 mph or less and with traffic volumes of fewer than 3,000 vehicles per day.

Further Considerations

- Bicycle boulevards are typically located on streets without existing signalized accommodation at crossings of collector and arterial roadways. Without treatments for bicyclists and pedestrians, these intersections can become major barriers along the bicycle boulevard and compromise safety.
- Traffic calming can lower speeds along bicycle boulevards and even deter motorists from driving on a street. Anticipate and monitor vehicle volumes on adjacent streets to determine whether traffic calming results in inappropriate volumes. Traffic calming can be implemented on a trial basis.

Approximate Cost

Costs vary depending on the type of treatments proposed for the corridor. Simple treatments such as wayfinding signage and markings are most cost-effective, but more intensive treatments will have greater impact at lowering speeds and volumes, at a higher cost. A general estimate range is \$75,000-\$140,000 excluding new enhanced crossing treatments at crossings, such as RRFBs, PHBs, or full traffic signals.

Bicycle Boulevards



Bicycle boulevards are established on streets that improve connectivity to key destinations and provide a direct, low-stress route for bicyclists, with low motorized traffic volumes and speeds, designated and designed to give bicycle travel priority over other modes.

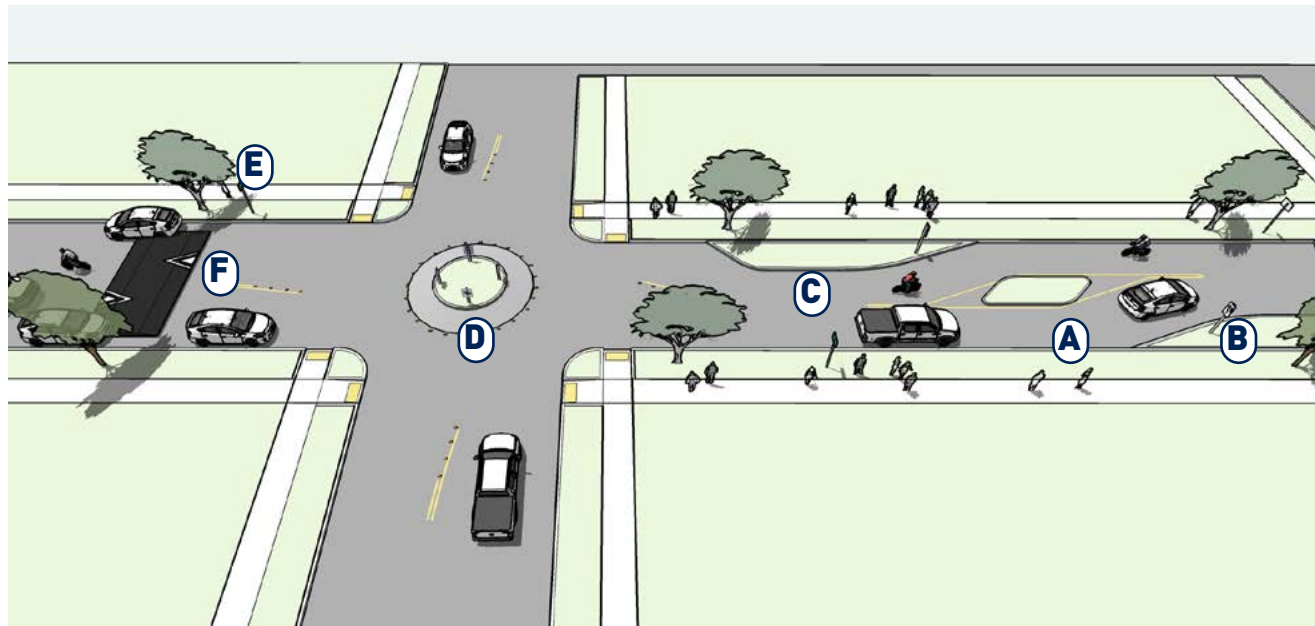
Traffic Calming



Neighborhood bikeways may require additional traffic calming measures to discourage through trips by motor vehicles.

Traffic Calming for Bike Boulevards

Traffic calming may include elements intended to reduce the speeds of motor vehicle traffic to be closer to bicyclist travel speeds, or may include design elements that restrict certain movements for motorized travel to discourage the use of bicycle boulevard corridors for through travel by automobiles. Traffic calming treatments can cause drivers to slow down by constricting the roadway space or by requiring careful maneuvering. Such measures may reduce the design speed of a street, and can be used in conjunction with reduced speed limits to reinforce the expectation of lowered speeds. They can also lower vehicle volumes by physically or operationally reconfiguring corridors and intersections along the route.



Typical Application

- Bicycle boulevards should have a maximum posted speed of 25 mph. Use traffic calming to maintain an 85th percentile speed below 20 mph (25 mph maximum). Bikeways with average speeds above this limit should be considered for traffic calming measures.
- Maintain a minimum clear width of 14 feet with a constricted length of at least 20 feet in the direction of travel.
- Bring traffic volumes down to 1,500 cars per day (2,000 cars per day maximum). Bikeways with daily volumes above this limit should be considered for traffic calming measures.

Design Features (Speed Reduction)

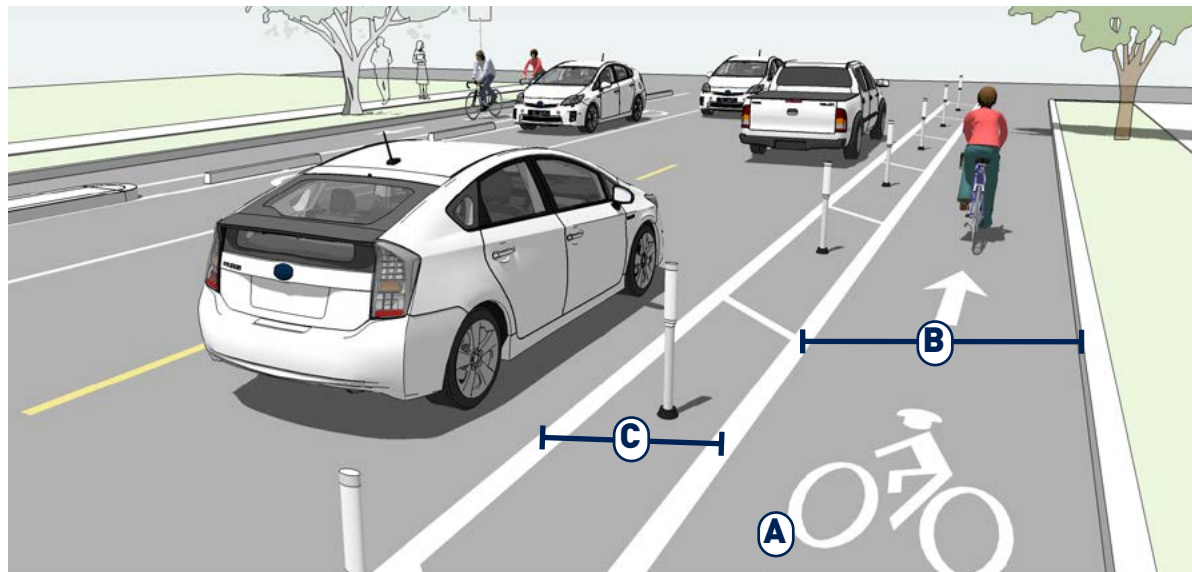
- A** Median islands create pinchpoint for traffic in the center of the roadway and offers shorter crossing distances for pedestrians when used in tandem with a marked crossing.
- B** Chicanes slow drivers by requiring vehicles to shift laterally through narrowed lanes and which avoids uninterrupted sightlines.
- C** Pinchpoints, chokers, or curb extensions restrict motorists from operating at high speeds on local streets by visually narrowing the roadway.
- D** Neighborhood traffic circles reduce speed of traffic at intersections by requiring motorists to move cautiously through conflict points.
- E** Street trees narrow a driver's visual field, subconsciously queuing drivers to slow down.
- F** Speed humps slow drivers through vertical deflection. To minimize impacts to bicycles, use a sinusoidal profile and leave a gap along curb so that bicyclists may bypass the hump when appropriate. Speed cushions operate in a similar fashion to speed humps, but allow for unimpeded travel by emergency vehicles.

Design Features (Volume Reduction)

- Partial closure diverters allows bicyclists to proceed straight across the intersection but forces motorists to turn left or right. All turns from the major street onto the bikeway are prohibited. Can incorporate curb extensions with stormwater management features and/or a mountable island.
- Right-in/right-out diverters force motorists to turn right while bicyclists can continue straight through the intersection. The island can provide a through bike lane or bicycle access to reduce conflicts with right-turning vehicles. Left turns from the major street onto the bikeway are prohibited, while right turns are still allowed.
- Median refuge island diverters restrict through and left-turn vehicle movements along the bikeway while providing refuge for bicyclists to cross one direction of traffic at a time. This treatment prohibits left turns from the major street onto the bikeway, while right turns are still allowed.
- Full diverters block all motor vehicles from continuing on a neighborhood bikeway, while bicyclists can continue unrestricted. Full closures can be constructed to be permeable to emergency vehicles.

One-Way Separated Bike Lanes (Class IV)

When retrofitting separated bike lanes onto existing streets, a one-way street-level design may be most appropriate. This design provides protection through physical barriers and can include flexible delineators, curbs, on-street parking or other barriers. A street level separated bike lane shares the same elevation as adjacent travel lanes..



Design Features

- A** Pavement markings, symbols and/or arrow markings must be placed at the beginning of the separated bike lane and at intervals along the facility (CA MUTCD 9C.04).
 - B** 7 foot width preferred to allow passing (5 foot minimum).
 - C** 3 foot minimum buffer width adjacent to parking. 18 inch minimum adjacent to travel lanes. Channelizing devices should be placed in the buffer area (NACTO, 2012).
- If buffer area is 4 feet or wider, white chevron or diagonal markings should be used.

Typical Application

- Street retrofit projects with limited funds for relocating curbs and drainage.
- Streets with high motor vehicle volumes and/or speeds and high bicycle volumes.
- Streets for which conflicts at intersections can be effectively mitigated using parking lane setbacks, bicycle markings through the intersection, and other signalized intersection treatments.
- Appropriate for most riders on most streets.

Further Considerations

- Separated bike lane buffers and barriers are covered in the MUTCD as preferential lane markings (section 3D.01) and channelizing devices (section 3H.01). Curbs may be used as a channeling device, see the section on islands (section 3I.01).
- A retrofit separated bike lane has a relatively low implementation cost compared to road reconstruction by making use of existing pavement and drainage and by using parking lane as a barrier.
- Gutters, drainage outlets and utility covers should be designed and configured as not to impact bicycle travel.
- Special consideration should be given at transit stops to manage bicycle and pedestrian interactions.



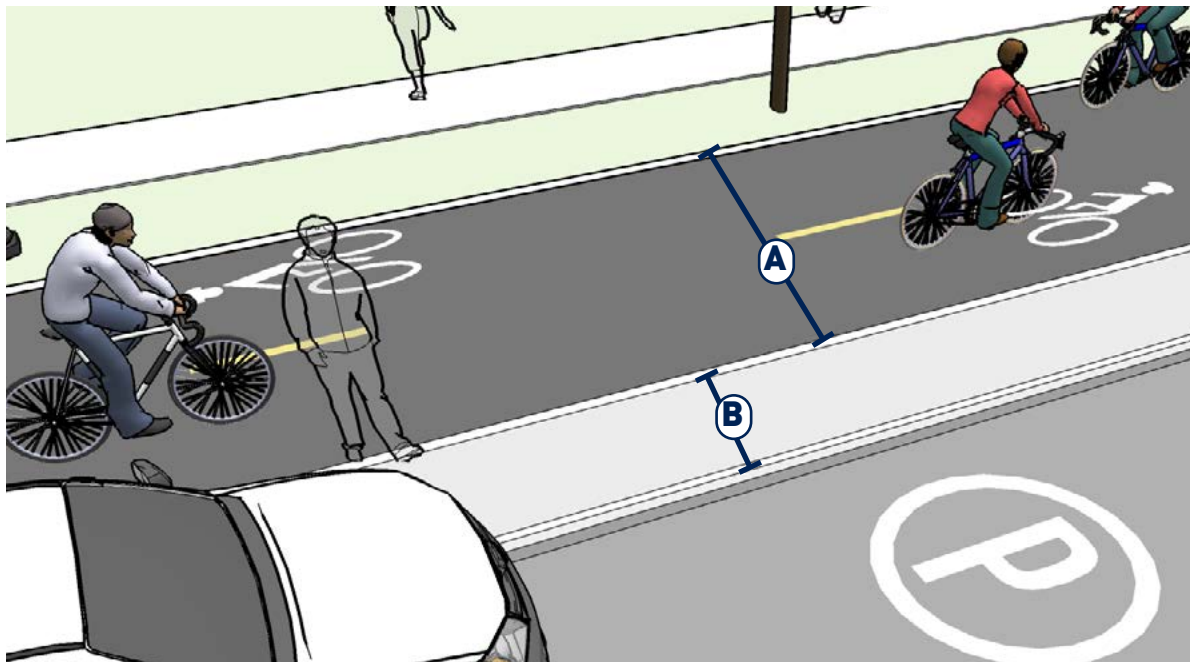
Street Level Separated Bicycle Lanes can be separated from the street with parking, planters, bollards, or other design elements.

Approximate Cost

The implementation cost is low if the project uses existing pavement and drainage, but the cost significantly increases if curb lines need to be moved. A parking lane is the low-cost option for providing a barrier. Other barriers might include concrete medians, bollards, tubular markers, or planters. A general cost estimate range for Class IV bikeways is \$250,000 to \$350,000 depending on the barrier materials used.

Two-Way Separated Bike Lanes (Class IV)

Two-Way Separated Bikeways are bicycle facilities that allow bicycle movement in both directions on one side of the road. Two-way separated bikeways share some of the same design characteristics as one-way separated bikeways, but often require additional considerations at driveway and side-street crossings, and intersections with other bikeways.



Design Features

- A** 12 foot operating width preferred (10 ft minimum) width for two-way facility.
 - In constrained locations an 8 foot minimum operating width may be considered (HDM 1003.1(1)).
- B** Adjacent to on-street parking a 3 foot minimum width channelized buffer or island shall be provided to accommodate opening doors (NACTO, 2012) (CA MUTCD 3H.01, 3I.01).
 - A separation narrower than 5 feet may be permitted if a physical barrier is present (AASHTO, 2013).
 - Additional signalization and signs may be necessary to manage conflicts.

Typical Use

- Works best on the left side of one-way streets.
- Streets with high motor vehicle volumes and/or speeds and streets with high bicycle volumes.
- Streets with a high incidence of wrong-way bicycle riding.
- Streets with few conflicts such as driveways or cross-streets on one side of the street.
- Streets that connect to shared use paths.

Further Considerations

- On-street bikeway buffers and barriers are covered in the CA MUTCD as preferential lane markings (section 3D.01) and channelizing devices, including flexible delineators (section 3H.01). Curbs may be used as a channeling device, see the section on islands (section 3I.01).
- A two-way separated bikeway on one way street should be located on the left side.
- A two-way separated bikeway may be configured at street level or as a raised separated bikeway with vertical separation from the adjacent travel lane.
- Two-way separated bikeways should ideally be placed along streets with long blocks and few driveways or mid-block access points for motor vehicles.
- See Caltrans Design Information Bulletin No. 89 for more details.



A two-way facility can accommodate cyclists in two directions of travel.

Materials and Maintenance

Bikeway striping and markings will require higher maintenance where vehicles frequently traverse over them at intersections, driveways, parking lanes, and along curved or constrained segments of roadway. Green conflict striping (if used) will also generally require higher maintenance due to vehicle wear.

Bikeways should be maintained so that there are no pot holes, cracks, uneven surfaces or debris.

Access points along the facility should be provided for street sweeper vehicles to enter/exit the separated bikeway.

Approximate Cost

Separated bikeway construction costs can vary drastically depending on the type of separation used, the amount of new curb and gutter, stormwater mitigation, and crossing treatments. On the lower end of the scale, construction of a striped parking protected bikeway with delineators or other vertical elements can cost as little as \$300,000 to \$400,000 per mile, not including signal improvements.

BICYCLE FACILITIES AT INTERSECTIONS

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Intersection Crossing Markings

Bicycle pavement markings through intersections guide bicyclists on a safe and direct path through the intersection and provide a clear boundary between the paths of through bicyclists and vehicles in the adjacent lane.



Design Features

- A** Intersection markings should be the same width and in line with leading bike lane.
 - Dotted lane line extensions should be 2 foot line segments with 2 to 6 foot gaps between them (CAMUTCD 3B.08).
 - All markings should be white, skid resistant and retro reflective (CAMUTCD 9C.02.02).
- B** Dotted lines may be enhanced with solid green, or dashed green with the same extents as the dotted line itself.

Typical Application

- Streets with conventional, buffered, or separated bike lanes.
- At direct paths through intersections.
- Streets with high volumes of adjacent traffic.
- Where potential conflicts exist between through bicyclist and adjacent traffic.

Further Considerations

The National Committee on Uniform Traffic Control Devices has submitted a request to include additional options for bicycle lane extensions through intersections as a part of future MUTCD updates. Their proposal includes the following options for striping elements within the crossing:

- Bicycle lane markings
- Double chevron markings, indicating the direction of travel.
- Green colored pavement.

Approximate Cost

The cost for installing intersection crossing markings will depend on the implementation approach. On roadways with adequate width for reconfiguration or restriping, costs may be negligible when provided as part of routine overlay or repaving projects.

Typical thermoplastic shared lane markings cost \$180 each.



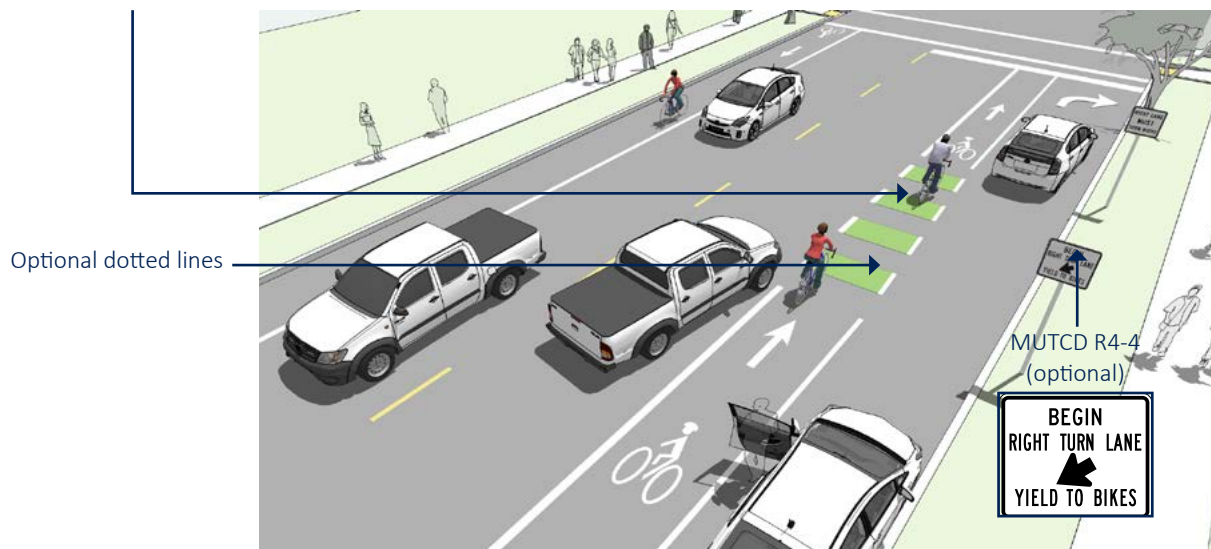
Intersection crossing markings can be used at signalized intersections or high volume minor street and driveway crossings.

Bike Lanes at Right Turn Lanes

The appropriate treatment at right-turn lanes is to place the bike lane between the right-turn lane and the right-most through lane or, where right-of-way is insufficient, to use a shared bike lane/turn lane.

The design (below) illustrates conflict markings, with signage indicating that motorists should yield to bicyclists through the conflict area.

Colored pavement may be used in the weaving area to increase visibility and awareness of potential conflict



Design Features

At auxiliary right turn only lanes (add lane):

- Continue existing bike lane width; standard width of 5 to 6 feet or 4 feet in constrained locations.
- Use signage to indicate that motorists should yield to bicyclists through the conflict area.
- Consider using colored conflict areas to promote visibility of the mixing zone.

Where a through lane becomes a right turn only lane:

- Do not define a dotted line merging path for bicyclists.
- Drop the bicycle lane in advance of the merge area.
- Use shared lane markings to indicate shared use of the lane in the merging zone.

Typical Application

- At auxiliary right turn only lanes (add lane)
- Where a through lane becomes a right turn only lane



Drivers wishing to enter the right turn lane must transition across the bicycle lane in advance of the turn.

Further Considerations

- The bicycle lane maintains a straight path, and drivers must weave across, providing clear right-of-way priority to bicyclists.
- Maintaining a straight bicycle path reinforces the priority of bicyclists over turning cars. Drivers must yield to bicyclists before crossing the bike lane to enter the turn lane.
- Through lanes that become turn only lanes are difficult for bicyclists to navigate and should be avoided.
- The use of dual right-turn-only lanes should be avoided on streets with bike lanes (AASHTO, 2013). Where there are dual right-turn-only lanes, the bike lane should be placed to the left of both right-turn lanes; however, this merge is uncomfortable for most bicyclists. Keeping the bike lane to the right of the turn lanes is possible if a bicycle signal phase is implemented to separate bicyclists from turning vehicles.

Materials and Maintenance

Because the effectiveness of markings depends entirely on their visibility, maintaining the visibility of markings should be a high priority.

Approximate Cost

The cost for installing bicycle lanes will depend on the implementation approach. On roadways with adequate width for reconfiguration or restriping, costs may be negligible when provided as part of routine overlay or repaving projects.

Typical costs are \$16,000 per mile for restriping.

Bike Box

A bike box is a designated area located at the head of a traffic lane at a signalized intersection that provides bicyclists with a safe and visible space to get in front of queuing traffic during the red signal phase. Motor vehicles must queue behind the white stop line at the rear of the bike box. On a green signal, all bicyclists can quickly clear the intersection.



Design Features

- A** 14 foot minimum depth from back of crosswalk to motor vehicle stop bar (NACTO, 2012).
- B** A “No Turn on Red” (MUTCD R10-11) sign shall be installed overhead to prevent vehicles from entering the Bike Box. A “Stop Here on Red” (MUTCD R10-6) sign should be post mounted at the stop line to reinforce observance of the stop line.
 - A 50 foot ingress lane should be used to provide access to the box.
- C** Use of green colored pavement is optional but recommended.

Typical Application

- At potential areas of conflict between bicyclists and turning vehicles, such as a right or left turn locations.
- At signalized intersections with high bicycle volumes.
- At signalized intersections with high vehicle volumes.

Further Considerations

- This treatment positions bicycles together and on a green signal, all bicyclists can quickly clear the intersection, minimizing conflict and delay to transit or other traffic.
- Pedestrians also benefit from bike boxes, as they experience reduced vehicle encroachment into the crosswalk.
- Two stage turn boxes are better treatments to facilitate bicycle turns as they are available for queuing during a parallel green signal indication.



A bike box allows for cyclists to wait in front of queuing traffic, providing high visibility, and a head start over motor vehicle traffic.

Materials and Maintenance

Because the effectiveness of markings depends entirely on their visibility, maintaining the visibility of markings should be a high priority.

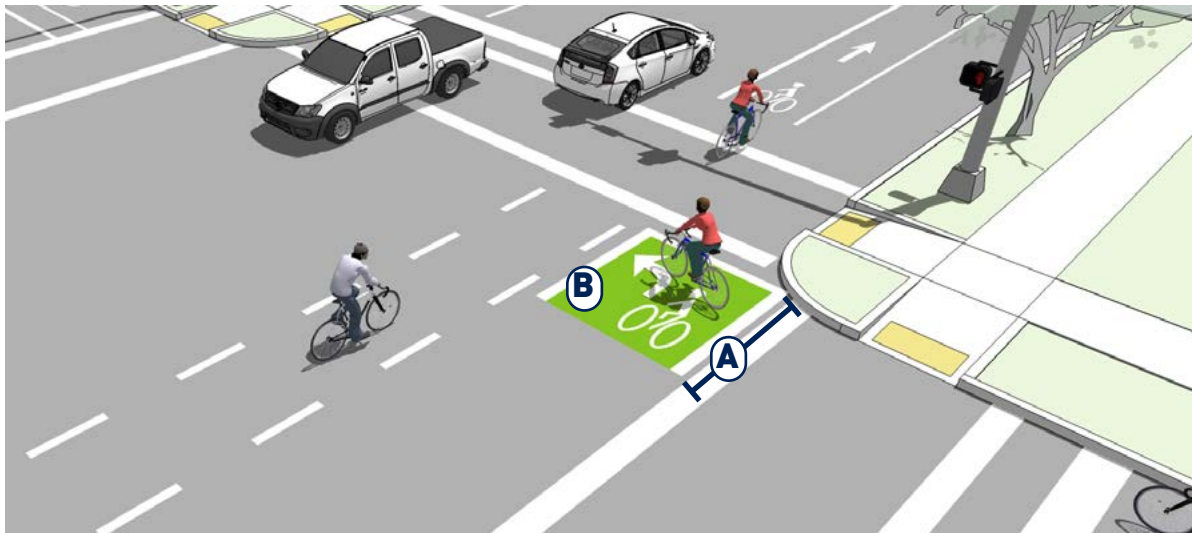
Approximate Cost

Costs will vary due to the type of paint used and the size of the bike box, as well as whether the treatment is added at the same time as other road treatments.

The typical cost for painting a bike box is \$11.50 per sq. foot.

Two-Stage Turn Boxes

Two-stage turn boxes offer bicyclists a safe way to make turns at multi-lane signalized intersections from a physically separated or conventional bike lane. On physically separated bike lanes, bicyclists are often unable to merge into traffic to turn due to physical separation, making the provision of two-stage turn boxes critical.



Design Features

- The two-stage turn box shall be placed in a protected area. Typically this is within the shadow of an on-street parking lane or separated bike lane buffer area and should be placed in front of the crosswalk to avoid conflict with pedestrians.
- Ⓐ 6.5 feet deep by 10 feet wide is the required minimum dimensions of the box to accommodate three bicyclists side by side (FHWA).
- Ⓑ Bicycle stencil and turn arrow pavement markings shall be used to indicate proper bicycle direction and positioning (NACTO, 2012).

Typical Application

- Streets with high vehicle speeds and/or traffic volumes.
- At intersections locations of multi-lane roads with signalized intersections.
- At signalized intersections with a high number of bicyclists making a left turn from a right side facility.

Further Considerations

- Provide a “No Turn on Red” sign (MUTCD R10-11) on the cross street if turning vehicles come into conflict with the placement of the turn box.
- This design formalizes a maneuver called a “box turn” or “pedestrian style turn.”
- Design guidance for two-stage turns apply to both bike lanes and separated bike lanes.
- Two-stage turn boxes reduce conflicts in multiple ways; from keeping bicyclists from queuing in a bike lane or crosswalk and by separating turning bicyclists from through bicyclists.
- Bicyclist capacity of a two-stage turn box is influenced by physical dimension (how many bicyclists it can contain) and cycle length (how frequently the box clears).
- More information on two stage turn boxes is available:
 - FHWA Separated Bike Lane Planning and Design Guide
 - NACTO. Urban Bikeway Design Guide. 2012
 - FHWA Interim Approval-20

Jughandle Turn Box



This MUTCD compliant design carves a jughandle out of the sidewalk to provide space for waiting bicyclists.

Separated Bike Lane Turn Box



On separated bike lanes, the two-stage turn box can be located in the protected buffer/parking area.

Materials and Maintenance

Because the effectiveness of markings depends entirely on their visibility, maintaining the visibility of markings should be a high priority.

Approximate Cost

Costs will vary due to the type of paint used and the size of the two-stage turn box, as well as whether the treatment is added at the same time as other road treatments.

The typical cost for painting a two-stage turn box is \$11.50 per square foot.

Bicycle Detection and Actuation

At fully signalized intersections, bicycle crossings are typically accomplished through the use of a standard green signal indication for Class II and III bikeways. A number of traffic signal enhancements can be made to improve detection and actuation and better accommodate bicyclists. An exclusive bicycle phase provided by bicycle signals offers the highest level of service and protection, especially for Class I and IV bikeways, but feature the same detection and actuation devices used at intersections with standard traffic signals. For more information on bicycle signals, see Protected Bicycle Signal Phase.

Typical Application

- Bicycle detection and actuation is used to alert the signal controller of bicycle crossing demand on a particular approach. Proper bicycle detection should meet at least two primary criteria: 1) accurately detect bicyclists, and 2) provide clear guidance to bicyclists on how to actuate detection (e.g. what button to push or where to stand). Additionally, new technologies are being developed to provide feedback to bicyclists once they have been detected to increase the likelihood of stop compliance.
- Detection mechanisms can also provide bicyclists with an extended green time before the signal turns yellow so that bicyclists of all abilities can reach the far side of the intersection.
- All new or modified traffic signals in California must be equipped for bicyclist detection, or be placed on permanent recall or fixed time operation (CalTrans Traffic Operations Policy Directive 09-06).
- Detection shall be placed where bicyclists are intended to travel and/or wait.
- On bicycle priority corridors with on-street bike lanes or separated bikeways, consider the use of advance detection placed 100-200 feet upstream of the intersection to provide an early trigger to the signal system and reduce bicyclist delay.

Design Features

- Bicycle detection and actuation systems include user-activated buttons mounted on a pole facing the street, in-pavement loop detectors that trigger a change in the traffic signal when a bicycle is detected, video detection cameras that use digital image processing to detect a change in the image at a location, and/or Remote Traffic Microwave Sensor Detection (RTMS) which uses frequency modulated continuous wave radio signals to detect objects in the roadway.
- 6 foot by 6 foot Type C loop conductors should be used.
- A linear pavement marking should be used to indicate where cyclists should stand to actuate the signal.
- Signal heads should depict green, yellow, and red cyclist icons to communicate when the exclusive bicycle phase is in progress.

Further Considerations

- The location of pushbuttons should not require bicyclists to dismount or be rerouted out of the way or onto the sidewalk to activate the phase. Signage should supplement the signal to alert bicyclists of the required activation to prompt the green phase.
- In-pavement Type C Loop detectors are induction circuits installed within the roadway surface to detect bicyclists as they wait for the signal. This allows the bicyclists to stay within the lane of travel. Loop detectors should be sufficiently sensitive to detect bicyclists and be marked with pavement markings instructing bicyclists on where to stand. CAMUTCD provides guidance on stencil markings and signage related to loop detectors.
- Remote Traffic Microwave Sensor Detection (RTMS) is unaffected by temperature and lighting which can affect standard video detection.
- Bicyclists typically need more time to travel through an intersection than motor vehicles. Green light times should be determined using the bicycle crossing time for standing bicycles.

Push Button Actuation

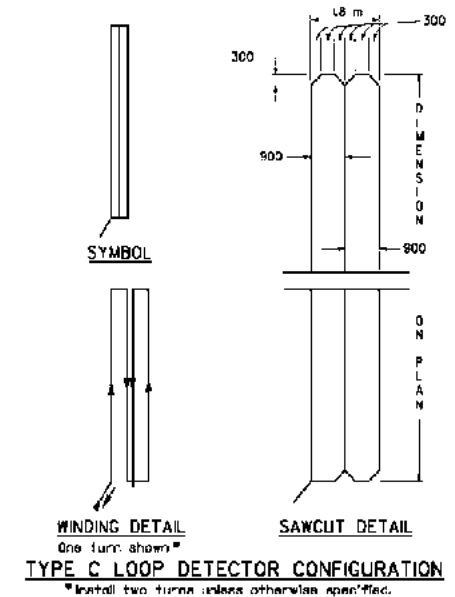


Bicycle push button actuators are positioned to allow bicycle riders in roadway to stop traffic on busy cross-streets.

Materials and Maintenance

Bicycle signal detection equipment should be inspected and maintained regularly, especially if detection relies on manual actuation. Pushbuttons and loop detectors will tend to have higher maintenance needs than other passive detection equipment.

Type C Loop Detector



Type C loop detector have been shown to most reliably detect bicyclists at all points over their surface.

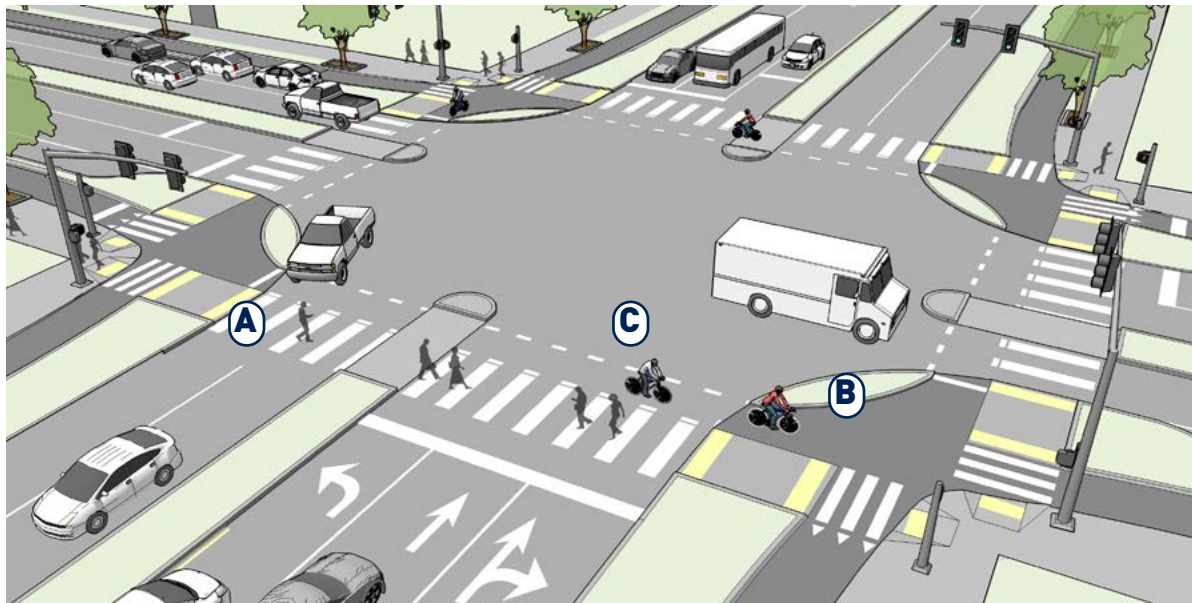
Approximate Cost

Costs vary depending on the type of technology used, but bicycle loop detectors embedded in the pavement typically cost from \$1,000-\$2,000. Video detection camera systems typically range from \$20,000 to \$30,000 per intersection.

Other traffic signal programming enhancements can be made to existing traffic signal hardware with relatively little to no additional hardware costs.

Protected Intersection

A protected intersection, or “Bend Out” uses a collection of intersection design elements to maximize user comfort within the intersection and promote a high rate of motorists yielding to people bicycling. The design maintains a physical separation within the intersection to define the turning paths of motor vehicles, slow vehicle turning speed, and offer a comfortable place for people bicycling to wait at a red signal.



Design Features

- A** Setback bicycle crossing of 19.5 feet allows for one passenger car to queue while yielding. Smaller setback distance is possible in slow-speed, space constrained conditions.
- B** Corner island with a 15-20 foot corner radius slows motor vehicle speeds. Larger radius designs may be possible when paired with a deeper setback or a protected signal phase, or small mountable aprons. Two-stage turning boxes are provided for queuing bicyclists adjacent to corner islands.
- C** Use intersection crossing markings.

Typical Use

- Streets with separated bikeways protected by wide buffer or on-street parking.
- Where two separated bikeways intersect and two-stage left-turn movements can be provided for bicycle riders.
- Helps reduce conflicts between right-turning motorists and bicycle riders by reducing turning speeds and providing a forward stop bar for bicycles.
- Where it is desirable to create a curb extension at intersections to reduce pedestrian crossing distance.

Further Considerations

- Pedestrian crosswalks may need to be further set back from intersections in order to make room for two-stage turning queue boxes.
- Wayfinding and directional signage should be provided to help bicycle riders navigate through the intersection.
- Colored pavement may be used within the corner refuge area to clarify use by people bicycling and discourage use by people walking or driving.
- Intersection approaches with high volumes of right turning vehicles should provide a dedicated right turn only lane paired with a protected signal phase. Protected signal phasing may allow different design dimensions than are described here.



Protected intersections feature a corner safety island and intersection crossing markings.



Protected intersections incorporate queuing areas for two-stage left turns.

Materials and Maintenance

- Green conflict striping (if used) will also generally require higher maintenance due to vehicle wear.
- Bikeways should be maintained so that there are no pot holes, cracks, uneven surfaces or debris.
- Bikeways protected by concrete islands or other permanent physical separation, can be swept by street sweeper vehicles with narrow widths.

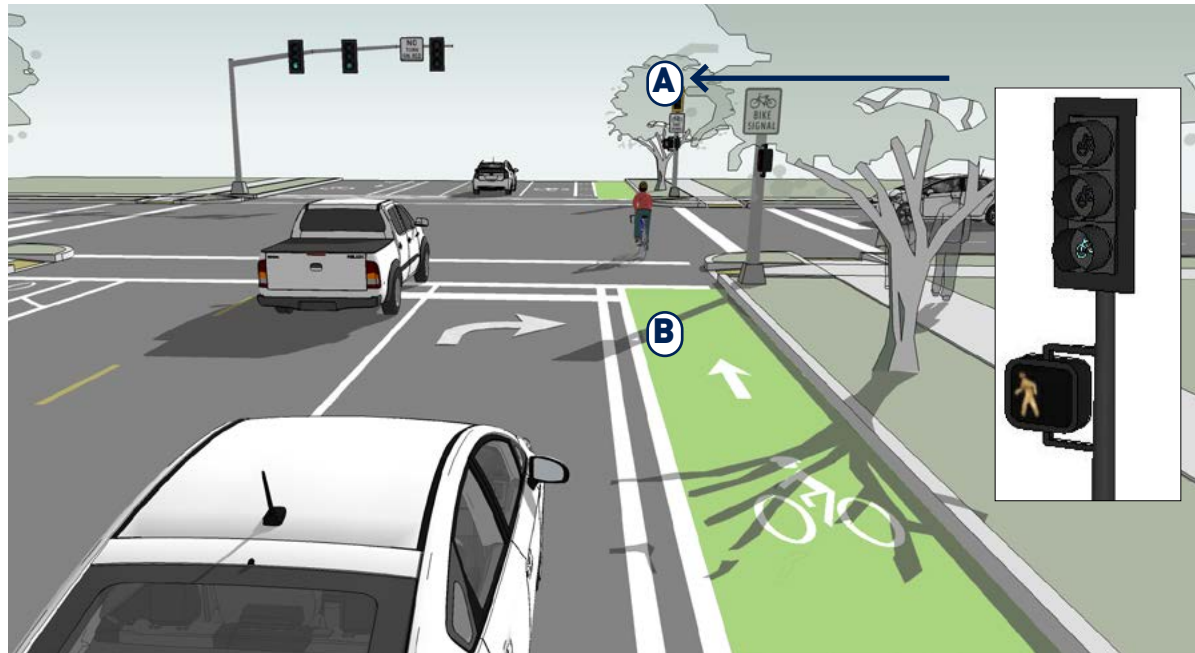
Approximate Cost

The cost of protected intersection elements vary depending on materials used and degree of implementation desired.

- Complete reconstruction costs comparable to a full intersection.
- Retrofit implementation may be possible at lower costs if existing curbs and drainage are maintained. Inexpensive materials can be used, such as paint, concrete planters, and bollards.

Separated Bicycle Signal Phase

Separated bicycle lane crossings of signalized intersections can be accomplished through the use of a bicycle signal phase which reduces conflicts with motor vehicles by separating bicycle movements from any conflicting motor vehicle movements. Bicycle signals are traditional three lens signal heads with green, yellow and red bicycle stenciled lenses.



Design Features

- A** An additional “Bicycle Signal” sign should be installed below the bicycle signal head.
- B** Designs for bicycles at signalized crossings should allow bicyclists to trigger signals via pushbutton, loop detectors, or other passive detection, to navigate the crossing.
- On bikeways, signal timing and actuation shall be reviewed and adjusted to consider the needs of bicyclists (CA MUTCD 9D.02).

Typical Use

- Two-way protected bikeways where contraflow bicycle movement or increased conflict points warrant protected operation.
- Bicyclists moving on a green or yellow signal indication in a bicycle signal shall not be in conflict with any simultaneous motor vehicle movement at the signalized location.
- Right (or left) turns on red should be prohibited in locations where such operation would conflict with a green bicycle signal indication.

Further Considerations

- A bicycle signal should be considered for use only when the volume/collision or volume/geometric warrants have been met (CA MUTCD 4C.102).
- The Federal Highway Administration (FHWA) has approved bicycle signals for use, if they comply with requirements from Interim Approval 16 (I.A. 16). Bicycle Signals are not approved for use in conjunction with Pedestrian Hybrid Beacons.
- Bicyclists typically need more time to travel through an intersection than motor vehicles. Green light times should be determined using the bicycle crossing time for standing bicycles.
- Bicycle detection and actuation systems include user-activated buttons mounted on a pole, loop detectors that trigger a change in the traffic signal when a bicycle is detected and video detection cameras, that use digital image processing to detect a change in the image at a location.



A bicycle signal head at a signalized crossing creates a protected phase for cyclists to safely navigate an intersection.

Materials and Maintenance

Bicycle signal detection equipment should be inspected and maintained regularly, especially if detection relies on manual actuation. Pushbuttons and loop detectors will tend to have higher maintenance needs than other passive detection equipment.



A bicycle detection system triggers a change in the traffic signal when a bicycle is detected.

Approximate Cost

Bicycle signal heads have an average cost of \$12,800.

Video detection camera system costs range from \$15,000 to \$25,000 per intersection.

BICYCLE FACILITY AMENITIES

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Wayfinding Sign Types

The ability to navigate through a city is informed by landmarks, natural features, and other visual cues. Signs throughout the city should indicate to bicyclists the direction of travel, the locations of destinations and the travel time/distance to those destinations. A bicycle wayfinding system consists of comprehensive signing and/or pavement markings to guide bicyclists to their destinations along preferred bicycle routes.



D11-1c



D11-1/D1-3a



D1-1

Typical Application

- Wayfinding signs will increase users' comfort and accessibility to the bicycle network.
- Signage can serve both wayfinding and safety purposes including:
 - Helping to familiarize users with the bicycle network
 - Helping users identify the best routes to destinations
 - Helping to address misconceptions about time and distance
 - Helping overcome a "barrier to entry" for people who are not frequent bicyclists (e.g., "interested but concerned" bicyclists)

Design Features

- A** Confirmation signs indicate to bicyclists that they are on a designated bikeway. Make motorists aware of the bicycle route. Can include destinations and distance/time but do not include arrows.
- B** Turn signs indicate where a bikeway turns from one street onto another street. These can be used with pavement markings and include destinations and arrows.
- C** Decision signs indicate the junction of two or more bikeways and inform bicyclists of the designated bike route to access key destinations. These include destinations, arrows and distances. Travel times are optional but recommended.

Further Considerations

- Bicycle wayfinding signs also visually cue motorists that they are driving along a bicycle route and should use caution. Signs are typically placed at key locations leading to and along bicycle routes, including the intersection of multiple routes.
- Too many road signs tend to clutter the right-of-way, and it is recommended that these signs be posted at a level most visible to bicyclists rather than per vehicle signage standards.
- A community-wide bicycle wayfinding signage plan would identify:
 - Sign locations
 - Sign type – what information should be included and design features
 - Destinations to be highlighted on each sign – key destinations for bicyclists
 - Approximate distance and travel time to each destination
- Green is the color used for directional guidance and is the most common color of bicycle wayfinding signage in the US, including those in the MUTCD.
- Check wayfinding signage along bikeways for signs of vandalism, graffiti, or normal wear and replace signage along the bikeway network as-needed.

Community Logos on Signs



Wayfinding signs can include a local community identification logo, as this example from Oakland, CA.

Custom Street Signs (Berkeley, CA)



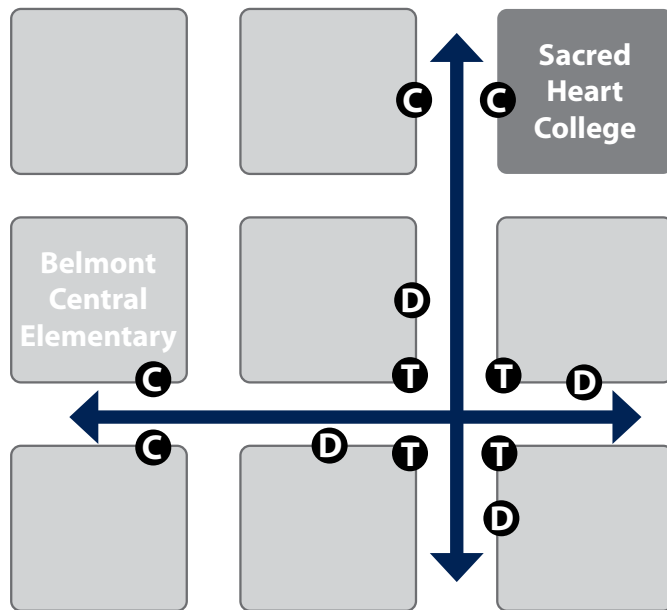
Custom street signs can also act as a type of confirmation sign, to let all users know the street is prioritized for bicyclists.

Approximate Cost

Wayfinding signs range from \$150 to \$500

Wayfinding Sign Placement

Signs are placed at decision points along bicycle routes – typically at the intersection of two or more bikeways and at other key locations leading to and along bicycle routes.



D Decision Sign



C Confirmation Sign



T Turn Sign



Typical Application

Confirmation Signs

- Placed every ¼ to ½ mile on off-street facilities and every 2 to 3 blocks along on-street bicycle facilities, unless another type of sign is used (e.g., within 150 feet of a turn or decision sign).
- Should be placed soon after turns to confirm destination(s). Pavement markings can also act as confirmation that a bicyclist is on a preferred route.

Turn Signs

- Near-side of intersections where bike routes turn (e.g., where the street ceases to be a bicycle route or does not go through).
- Pavement markings can also indicate the need to turn to the bicyclist.

Decision Signs

- Near-side of intersections in advance of a junction with another bicycle route.
- Along a route to indicate a nearby destination.



Some cities use pavement markings to indicate required turns along the bicycle route.

Design Features

- MUTCD guidelines should be followed for wayfinding sign placement, which includes mounting height and lateral placement from edge of path or roadway.
- Pavement markings can be used to reinforce routes and directional signage.

Further Considerations

- It can be useful to classify a list of destinations for inclusion on the signs based on their relative importance to users throughout the area. A particular destination's ranking in the hierarchy can be used to determine the physical distance from which the locations are signed. For example, primary destinations (such as the downtown area) may be included on signage up to 5 miles away. Secondary destinations (such as a transit station) may be included on signage up to two miles away. Tertiary destinations (such as a park) may be included on signage up to one mile away.

Approximate Cost

The cost of a wayfinding sign placement plan depends on the scale and scope of the approach. Trail wayfinding signage range from \$500-\$2000.

Bike Parking

Bicyclists expect a safe, convenient place to secure their bicycle when they reach their destination. This may be short-term parking of two hours or less, or long-term parking for employees, students, residents, and commuters.



Typical Application

- Bicycle parking facilities shall be located in highly visible well-lighted areas. In order to maximize security, whenever possible short-term bicycle parking facilities shall be located in areas highly visible from the street and from the interior of the building they serve (i.e. placed adjacent to windows).
- Bike racks provide short-term bicycle parking and is meant to accommodate visitors, customers, and others expected to depart within two hours. It should be an approved standard rack, appropriate location and placement, and weather protection.
- On-street bike corrals (also known as on-street bicycle parking) consist of bicycle racks grouped together in a common area within the street traditionally used for automobile parking. Bicycle corrals are reserved exclusively for bicycle parking and provide a relatively inexpensive solution to providing high-volume bicycle parking. Bicycle corrals can be implemented by converting one or two on-street motor vehicle parking spaces into on-street bicycle parking. Each motor vehicle parking space can be replaced with approximately 6-10 bicycle parking spaces.

DESIGN GUIDELINES

Design Features

- All bicycle facilities shall provide a minimum 4 foot aisle to allow for unobstructed access to the designated bicycle parking area.
- Bicycle parking facilities within auto parking facilities shall be protected from damage by cars by a physical barrier such as curbs, wheel stops, poles, bollards, or other similar features capable of preventing automobiles from entering the designated bicycle parking area.
- Bicycle parking facilities should be securely anchored so they cannot be easily removed and shall be of sufficient strength and design to resist vandalism and theft.

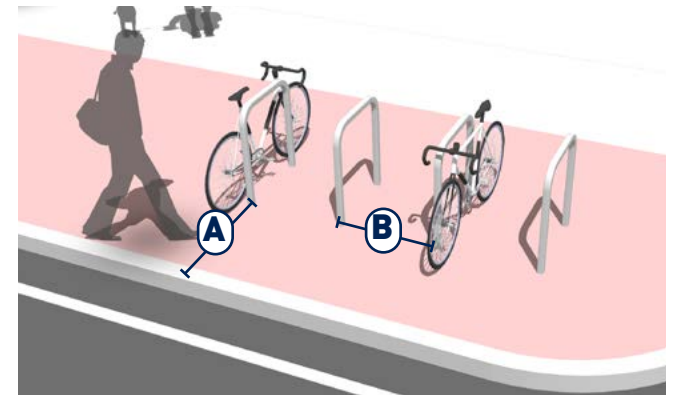
Bike Racks

- A** 2 foot minimum from the curb face to avoid 'dooring.'
- B** 4 feet between racks to provide maneuvering room.
- Locate close to destinations; 50 foot maximum distance from main building entrance.
- Minimum clear distance of 6 feet should be provided between the bicycle rack and the property line.

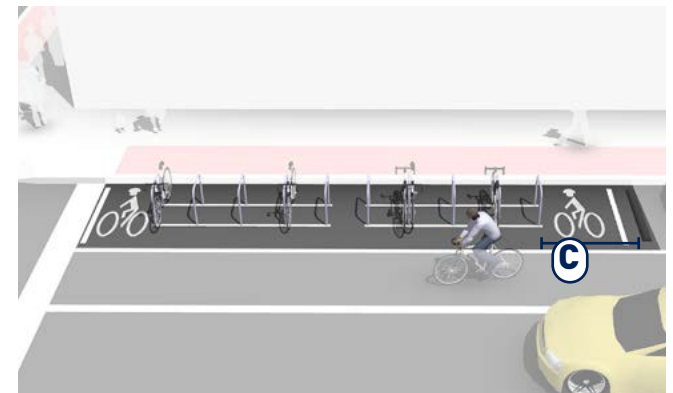
Bike Corrals

- C** Bicyclists should have an entrance width from the roadway of 5-6 feet for on-street corrals.
- Can be used with parallel or angled parking.
- Parking stalls adjacent to curb extensions are good candidates for on-street bicycle corrals since the concrete extension serves as delimitation on one side.
- Off-street bike corrals are appropriate where there is a wide sidewalk furnishing zone (7 feet or greater), or as part of a curb extension.

Perpendicular Bike Racks



Bike Corral



Approximate Cost

Costs can vary based on the design and materials used. Bicycle rack costs can range from approximately \$60 to \$3,600, depending on design and materials used. On average the cost is approximately \$660. Bicycle lockers costs range from \$1,280 to \$2,680.

APPENDIX B

Funding Sources

Appendix B. Funding Sources

In order to facilitate the implementation of recommended projects as part of the San Carlos Bicycle and Pedestrian Master Plan, a list of potential funding sources is provided below.

Federal Funding (Competitive Grants)

TIGER (Transportation Investment Generating Economic Recovery)

United States Department of Transportation (USDOT)

TIGER is a highly competitive, annual discretionary grant program that funds innovative, multimodal, and multi-jurisdictional transportation projects that are difficult to fund through traditional federal programs. Successful TIGER projects leverage resources, encourage partnership, catalyze investment and growth, fill a critical void in the transportation system or provide a substantial benefit to the nation, region or metropolitan area in which the project is located.

Eligible projects for TIGER Discretionary Grants are capital projects that include, but are not limited to: 1) highway or bridge projects eligible under title 23, United States Code (including bicycle and pedestrian related projects); 2) public transportation projects eligible under chapter 53 of title 49, United States Code; 3) passenger and freight rail transportation projects; 4) port infrastructure investments (including inland port infrastructure); and 5) intermodal projects.

Minimum/Maximum Grant Amounts: \$5 Million/\$100 Million

Required Local Match: 20 percent

Website: <https://www.transportation.gov/tiger>

State Funding* (Competitive Grants)

Active Transportation Program (ATP)

California Transportation Commission (CTC)

The Active Transportation Program was created to encourage increased use of active modes of transportation, such as biking and walking. The ATP consolidates various transportation programs, including the federal Transportation Alternatives Program, state Bicycle Transportation Account, and federal and state Safe Routes to School Programs, into a single program. Program funding is segregated into three components and is distributed as follows:

- 50 percent to the state for a statewide competitive program (25 percent of which must benefit disadvantaged communities)
- 10 percent to small urban and rural regions with populations of 200,000 or less for the small urban and rural area competitive program (25 percent of which must benefit disadvantaged communities)
- 40 percent to MPOs in urban areas with populations greater than 200,000 for the large urbanized area competitive program (25 percent of which must benefit disadvantaged communities)

Infrastructure Projects: SRTS that improve safety of children, Safe Routes to Transit, Bikeways and Walkways (new, improved, hazard elimination, maintenance), Traffic control devices (new

FUNDING SOURCES

pedestrian signals, RRFBs, protected left turn movements, road diets, etc.), Secure bike parking, Bikes on transit; Recreational trails/trailheads, Park linkages to corridors, and Rails-to-trails. Non-Infrastructure: Educational Programs and other non-infrastructure projects that demonstrate effectiveness in increasing active transportation. SRTS Projects in accordance with Section 1404 of Public Law 109-59. Plans: ATP, Bike, Pedestrian, and SRTS.

Minimum/Maximum Grant Amounts: \$250,000 minimum

Required Local Match: 11.47 percent

Website: <http://www.dot.ca.gov/hq/LocalPrograms/atp/>

Solutions for Congested Corridors Programs

California Transportation Commission (CTC)

Solutions for Congested Corridors Program (SCC) is funded by SB 1. Grant applications will be weighted based on the following criteria: 1) Safety 2) Congestion 3) Accessibility 4) Economic Development & Job Creation and Retention 5) Air Quality & Greenhouse Gas Emissions 6) Efficient Land Use 7) Matching Funds 8) Project Deliverability 9) Collaboration.

Minimum/Maximum Grant Amounts: Varies

Required Local Match: Varies

Website: <http://catc.ca.gov/programs/sb1/sccp/>

Highway Safety Improvement Program (HSIP)

Caltrans

The FAST Act continues the HSIP program to achieve a significant reduction in traffic fatalities and serious injuries on all public roads, including non-state-owned public roads on tribal lands. The HSIP requires a data-driven, strategic approach to improving highway safety on all public roads that focuses on performance. Items on the inclusion list include: 1) Installation of vehicle-to-infrastructure communication equipment, 2) Pedestrian hybrid beacons, 3) Roadway improvements that provide separation between pedestrians and motor vehicles, including medians and pedestrian crossing islands, 4) Other physical infrastructure projects not specifically enumerated in the list of eligible projects.

Federal Agency: Federal Highway Administration

Minimum/Maximum Grant Amounts:

\$100,000/\$10,000,000

Required Local Match: 10 percent

Website: <http://dot.ca.gov/hq/LocalPrograms/hsip.html>

Sustainable Transportation Planning Grant Program (STP)

Caltrans

The Sustainable Transportation Planning Grant Program was created to support the California Department of Transportation's (Caltrans') Mission: Provide a safe, sustainable, integrated and efficient transportation system to enhance California's economy and livability. The Grant program's overarching objectives were also identified to ensure consideration of these major efforts in transportation planning, including: sustainability, preservation, mobility, safety, innovation, economy, health, and equity. The Caltrans Division of Transportation Planning provides the following transportation planning grants: strategic partnerships, sustainable communities for MPOs & RTPAs, and sustainable communities for cities, counties, transit agencies, and tribal governments.

Strategic Partnerships:

Federal Agency: Federal Highway Administration

Minimum/Maximum Grant Amounts:

\$100,000/\$500,000

Required Local Match: 11.47 percent or 20 percent dependent on grant

Website: <http://www.dot.ca.gov/hq/tpp/grants.html>

Sustainable Communities:

Required Local Match: 11.47 percent or 20 percent dependent on grant

Website: <http://www.dot.ca.gov/tpp/grants.html>

FUNDING SOURCES

Office of Traffic Safety: National Safety Program 405(h) Nonmotorized Safety

California State Transportation Agency

Under FAST Act Section 405 is the National Priority Safety Program, which provides grant funding to address selected national priorities for reducing highway deaths and injuries. 5 percent of Section 405 funds are earmarked for nonmotorized safety incentive grants. States can submit their Section 405 application on July 1 as part of the consolidated application process. Grant funds may only be used for: 1) Training law enforcement on state laws applicable to pedestrian and bicycle safety 2) Enforcement mobilizations and campaigns designed to enforce those state laws 3) Public education and awareness programs designed to inform motorists, pedestrians, and bicyclists of those laws.

Federal Agency: National Highway Traffic Safety Administration

Minimum/Maximum Grant Amounts: Varies

Required Local Match: 20 percent

Website: <http://www.ghsa.org/about/federal-grant-programs/405>

The Office of Traffic Safety also offers additional grants in a number of categories including Pedestrian and Bicycle Safety. The goals of the office and grant program both focus around reducing collisions, injuries, and fatalities for pedestrian and bicyclists in addition to increase bicycle helmet compliance for youth.

Minimum/Maximum Grant Amounts: Varies

Required Local Match: Varies

Website: http://www.ots.ca.gov/Grants/Pedestrian_and_Bicycle_Safety.asp

Recreational Trails Program

Department of Parks & Recreation

The RTP is a Federal-aid assistance program of the U.S. Department of Transportation's Federal Highway Administration (FHWA) to help the states provide and maintain recreational trails for both motorized and nonmotorized trail use. Eligible projects include: Trail maintenance and restoration, trailside and trailhead facilities, equipment for construction and maintenance, construction of new recreational trails, acquisition of trail corridors, assessment of trail conditions, safety and environmental education, and administration.

Federal Agency: Federal Highway Administration

Minimum/Maximum Grant Amounts: Varies

Required Local Match: 12 percent

Website: http://www.parks.ca.gov/?page_id=24324

Affordable Housing and Sustainable Communities Program

Department of Housing and Community Development

The AHSC program funds land-use, housing, transportation, and land preservation projects that support infill and compact development that reduces greenhouse gas emissions. Projects must fall within one of three project area types: transit-oriented development, integrated connectivity project, or rural innovation project areas. Fundable activities include: affordable housing developments, sustainable transportation infrastructure, transportation-related amenities,

and program costs. Funds are programmed by the Strategic Growth Council and implemented by the Department of Housing and Community Development.

Minimum/Maximum Grant Amounts: \$500,000/ \$20 Million

Website: <http://www.hcd.ca.gov/grants-funding/active-funding/ahsc.shtml>

Highway Safety Improvement Program

Caltrans

Caltrans offers Highway Safety Improvement Program (HSIP) grants every one to two years. Projects on any publicly owned road or active transportation facility are eligible,

including bicycle and pedestrian improvements. HSIP focuses on projects that explicitly address documented safety challenges through proven countermeasures, are implementation-ready, and demonstrate cost-effectiveness. Funds are programmed by Caltrans.

Federal Agency: Federal Highway Administration
Minimum/Maximum Grant Amounts: \$100,000 / \$10 Million.

Required Local Match: 10 percent

Website: <https://safety.fhwa.dot.gov/hsip/>

FUNDING SOURCES

Urban Greening Grants

California Natural Resources Agency

Urban Greening Grants support the development of green infrastructure projects that reduce GHG emissions and provide multiple benefits. Projects must include one of three criteria, most relevantly: reduce commute vehicle miles traveled by constructing bicycle paths, bicycle lanes or pedestrian facilities that provide safe routes for travel between residences, workplaces, commercial centers, and schools. Eligible projects include green streets and alleyways and non-motorized urban trails that provide safe routes for travel between residences, workplaces, commercial centers, and schools. Funds are programmed by the California Natural Resources Agency.

Minimum/Maximum Grant Amounts: N/A

Required Local Match: No match funds required

Website: <http://resources.ca.gov/grants/urban-greening/>

Local Partnership Program

California Transportation Commission

This program provides local and regional agencies that have passed sales tax measures, developer fees, or other transportation-imposed fees to fund road maintenance and rehabilitation, sound walls, and other transportation improvement projects using SB1 funds. Jurisdictions with these taxes or fees are then eligible for a formulaic annual distribution of no less than \$100,000.

These jurisdictions are also eligible for a competitive grant program. Local Partnership Program funds can be used for a wide variety of transportation purposes including roadway rehabilitation and construction, transit capital and infrastructure, bicycle and pedestrian improvements, and green infrastructure. Funds are programmed by the CTC.

Road Maintenance And Rehabilitation Program

California Transportation Commission

Senate Bill 1 created the Road Maintenance and Rehabilitation Program (RMRP) to address deferred maintenance on state highways and local road systems. Program funds can be spent on both design and construction efforts. On-street active transportation related maintenance projects are eligible if program maintenance and other thresholds are met. Funds are allocated to eligible jurisdictions. Funds are programmed by the State Controller's Office with guidance from the CTC.

Minimum/Maximum Grant Amounts: Varied depending on project

Website: <https://catc.ca.gov/programs/sb1>

Regional Funding

Bicycle Facilities Grant Program

Bay Area Air Quality Management District

The goal of this program is to reduce air emissions from on-road motor vehicles and to improve air quality by helping residents and commuters mode-shift to bicycling and walking as alternatives to driving for short distances and first-and last-mile trips. During the FYE 2017 Cycle up to \$5 million in funds were available for this competitive program.

Minimum/Maximum Grant Amounts: \$10,000 per project/\$1,500,000 per agency

Required Local Match: 10 percent

Website: <http://www.baaqmd.gov/grant-funding/public-agencies/bikeways-roads-lanes-paths>

The Air District has additional grant programs that can provide funding for bicycle parking facilities.

FUNDING SOURCES

One Bay Area Grant Program 2

Metropolitan Transportation Commission (MTC)

One Bay Area integrated the region's federal transportation program with California's climate laws and the Sustainable Communities Strategy, by targeting funding to Priority Development Areas (PDAs), Priority Conservation Areas (PCAs), and Climate Initiatives while maintaining commitments to existing transportation priorities. Known as OBAG 2 for short, the second round of OBAG funding is projected to total roughly \$916 million to fund projects from 2017-18 through 2021-22. The OBAG 2 program is divided into a Regional Program, managed by MTC, and County Program, managed by the nine Bay Area Congestion Management Agencies (CMAs).

The County Program will provide over \$386 million over five years. Cities and counties can invest in: local street and road maintenance, streetscape enhancements, bicycle and pedestrian improvements, safe routes to school projects, priority conservation areas, and transportation planning. These funds are targeted to projects in PDAs to support efforts for focused growth. In the case of San Carlos, these funds will be managed by San Mateo County Transportation Authority.

Minimum/Maximum Grant Amounts: TBD

Required Local Match: TBD

Website: http://www.alamedactc.org/app_pages/view/8495

Federal dollars are often times distributed through state and regional agencies/programs. Federal policies may still apply.

Measure A Pedestrian and Bicycle Program

San Mateo County Manager's Office

The goal of the Measure A Pedestrian and Bicycle Program is to fund specific projects that improve bicycling and walking accessibility and safety in San Mateo County, helping to encourage more residents to participate in active transportation.

Minimum/Maximum Grant Amounts:

N/A/\$1,000,000

Required Local Match: 10 percent

Website: http://www.smcta.com/Projects/Call_for_Projects/2017_Measure_A_Pedestrian_and_Bicycle_Program_Call_for_Projects.html

Transportation Funds for Clean Air

Bay Area Air Quality Management District

Money in the Transportation Funds for Clean Air program, established by Assembly Bill 434, is generated by a \$4 vehicle registration surcharge in the nine Bay Area counties. The funds may be used on projects that reduce vehicle emissions, including bicycle and pedestrian projects, and can also be used as a match for competitive state or federal programs. Funds are programmed by the Bay Area Air Quality Management District (BAAQMD) and San Mateo County Transportation Authority.

Minimum/Maximum Grant Amounts: \$10,000/\$1,500,000

Required Local Match: 10 percent

Website: <http://www.baaqmd.gov/funding-and-incentives/funding-sources/regional-fund>

Local Funding

Assessment Districts

Different types of assessment districts can be used to fund the construction and maintenance of bikeway facilities. Examples include Mello-Roos Community Facility Districts, Infrastructure Financing Districts (SB 308), Open Space Districts, or Lighting and Landscape Districts. These types of districts have specific requirements relating to the establishment and use of funds.

Impact Fees

Another potential local source of funding are developer impact fees, typically tied to trip generation and traffic impacts as a result of proposed projects. In San Carlos, this fee is called a Traffic Impact Fee. A developer may be required to help mitigate the overall impact of vehicular trips by paying this fee.

Open Space District

Local Open Space Districts may float bonds that go to acquiring land or open space easements, which may also provide for some improvements to the local trail and bikeway system.

Non-Traditional and Private Funding

In the search for funding sources, it becomes increasingly necessary to ‘think outside the box’. With the environmental and health benefits afforded by walking and bicycling, there is an even greater opportunity to build partnerships with organizations and non-profits that have a similar interest in improving conditions for pedestrians and bicyclists. Teaming ventures with non-profit organizations will open up sources of private grant and foundation funding that is not open to a public agency.

California Conservation Corps (CCC)

The program provides emergency assistance and public service conservation work for government agencies and non-profit organizations. Both urban and rural projects are eligible and selected on the basis of environmental and natural resource benefits and on-the-job training opportunities.

Rails to Trails Conservancy (RTC)

The Conservancy assists rails-to-trails conversions through technical assistance, public education, advocacy, negotiations, legislation and regulatory action.

Grant and Foundation Opportunities

Private foundations provide excellent opportunities for funding specific capital projects or single event programs. Generally to qualify for these types of

funds, a Bicycle Advisory Committee or established non-profit group acting on its behalf must exist. In general, private foundations are initially established for specific purposes, e.g. children and youth need, promotion of certain professional objectives, educational opportunities, the arts, and community development. An excellent source of information about foundations and their funding potential can be found in the Foundation Directory, available at many public libraries or on-line at www.fconline.fdncenter.org/. Several foundations to consider are:

- Compton Foundation, Inc.
- Nathan Cummings Foundation
- Ottinger Foundation
- REI Corporate Contribution Programs
- Surdna Foundation, Inc.
- Robert Wood Johnson Foundation
- Bikes Belong Coalition

Adopt-A-Trail/Path Programs

Modeled upon the Southern California program of highway maintenance contributions, this program would post signs to indicate which individual or group has contributed to the development, installation or maintenance of a particular bike facility. Trail construction can also be considered by school or civic groups as a year-long project. The City has a local adopt-a-park program that could be expanded to include trails and paths.

Memorial Funds

These programs are advertised as potential donor projects to be funded via ongoing charitable contributions or funds left to a particular project through a will. Most memorial projects include the location of a memorial plaque at a location specific to the improvement or at a scenic vista point.

Revenue-Producing Operations

As part of the development of a trail or bike path, plans can specifically include the location of a revenue-producing operation adjacent to the proposed improvement. For example, bicycle rental/repair facilities, food and drink establishments, and bike storage facilities would be appropriate uses. The on-going lease revenues from these operations could then be used for trail/path maintenance.

FUNDING SOURCES

APPENDIX C

Community Engagement

Pop-Up Event Comments

Pop-Up Event Comments		
Comment	Event	Location
Remove barrier at border.	Art & Wine Faire	Crestview at Hallmark
101 Freeway is dangerous for runners	Art & Wine Faire	101 Freeway
Awful	Art & Wine Faire	Holly at Industrial
Cars don't stop. Unsafe.	Art & Wine Faire	Holly at Laurel
Create a flat nature path along a creek for elderly and disabled	Art & Wine Faire	No exact location provided
Drivers roll through right turns, don't stop	Art & Wine Faire	Holly at El Camino Real
Safer crossing	Art & Wine Faire	Howard at Alameda
I love Springfield Drive	Art & Wine Faire	Springfield Drive
Brighter streetlights on Holly	Art & Wine Faire	Holly
Street shrinking for businesses	Art & Wine Faire	E. San Carlos
Install ped scramble intersection on	Art & Wine Faire	Laurel at San Carlos
Light slow for bikes	Art & Wine Faire	Commercial at El Camino Real
Need better lighting.	Art & Wine Faire	Walnut near Magnolia Avenue
Make one-way	Art & Wine Faire	Walnut at Cherry
Safer walking from Tierra Linde to DT	Art & Wine Faire	
Needs to have Eucalyptus and restore sidewalks and bike lanes for kids to get to school at Arundel	Art & Wine Faire	San Carlos Ave from Arundel to residential area
Better ADA enforcement on Laurel	Art & Wine Faire	Laurel
Walking down Laurel in CBD hard because of outdoor seating	Art & Wine Faire	Laurel
Howard Ave: Mainly bikes and car and the parking on the street	Art & Wine Faire	Howard Avenue
Hillcrest Rd Circle Park: Unsafe walking speeders	Art & Wine Faire	Hillcrest Road Circle Park
San Carlos Ave: Unsafe walking	Art & Wine Faire	San Carlos Avenue
Brittan Ave/Tramanto: Crossing is dangerous	Art & Wine Faire	Brittan at Tramanto
Sidewalks on Carmelita	Art & Wine Faire	Carmelita
Blind curve. Cars too fast. Need stop sign or crosswalk	Art & Wine Faire	Manzanita at Vine
Need safe walking path to Arundel from southside SC Ave	Art & Wine Faire	Arundel

Re-open gate on Crestview at Los Vienzoz Way in order to access open space	Art & Wine Faire	Crestview at Los Vienzoz Way
Need safe way to bike or walk from town to upper S.C. (e.g. to Cañada Rd)	Art & Wine Faire	Upper San Carlos
Need better enforcement of speeding	Art & Wine Faire	No exact location provided
San Carlos, Laurel, ECR: Dangerous for runners, peds, cyclists	Art & Wine Faire	San Carlos, Laurel, El Camino Real
Need ped crossings on Alameda. Currently no crosswalks	Art & Wine Faire	Alameda De Las Pulgas
Crestview is horrible for bicyclists and peds.	Art & Wine Faire	Crestview
Speeding on Eaton	Art & Wine Faire	Eaton
Colton Ave/Park Ave: People run this stop sign	Art & Wine Faire	Colton Avenue at Park Avenue
Near Central Ave: No one stops for pedestrians	Art & Wine Faire	Near Central Avenue
El Camino Real: Not safe feeling	Art & Wine Faire	El Camino Real
Dangerous crossing	Art & Wine Faire	Near Crestview Park
Favorite	Art & Wine Faire	Eaton Park
SCARY.	Art & Wine Faire	Brittan at Brook
Big hill for kids to bike. Fun but cars on Orange need to be careful	Art & Wine Faire	Orange at Arroyo
Needs flashing lights	Art & Wine Faire	Woodland at Brittan
Drivers beating ECR light	Art & Wine Faire	Howard at Laurel
Class IV needed - separate bike lane <u>and</u> light	Art & Wine Faire	No exact location provided
Improved bike lanes.	Art & Wine Faire	Alameda De Las Pulgas
Unsafe for bikes/peds.	Art & Wine Faire	Alameda De Las Pulgas
Traffic calming.	Art & Wine Faire	Alameda De Las Pulgas
Need N-S Bike boulevard like Bryant in Mountain View	Art & Wine Faire	No exact location provided
Ralston is a great bike/ped overpass	Art & Wine Faire	Ralston
Bike parking and bike plans in Industrial area (Devil's Canyon/wineries) so people ride bike and don't drink and drive.	Art & Wine Faire	Devil's Canyon

Cyclists don't stop at stop sign on Brittan Ave	Art & Wine Faire	Brittan Avenue
Separated bikeways on ECR/Industrial	Art & Wine Faire	El Camino Real and Industrial
Protected and separated bikeways along 101	Art & Wine Faire	Highway 101
Better crossing/bridge across 101	Art & Wine Faire	Highway 101
Dangerous cycling.	Art & Wine Faire	Industrial Road
Needs more bike/ped improvements.	Art & Wine Faire	Old County Road
No bike lanes to cross	Art & Wine Faire	Brittan at El Camino Real
Love it! Make this happen.	Art & Wine Faire	No exact location provided
Dangerous biking/driving interactions.	Art & Wine Faire	Holly at Highway 101
Cedar St: Cars can't see bikes/pedestrians at Crook. No sidewalks, so bike lane would be great	Art & Wine Faire	Cedar Street
More bike parking downtown; since can't park a car on Laurel anymore	Art & Wine Faire	Downtown - Laurel
Cars parked on wide sidewalk.	Art & Wine Faire	Elm at Olive
Too dangerous for kids and families to ride bikes	Art & Wine Faire	Alameda De Las Pulgas
Need safe bike parking	Art & Wine Faire	
Elm/Cherry: Drive to Bay Trail with bike. Make it safer	Art & Wine Faire	Elm at Cherry
Carmelita Ave: Narrow street for drivers, makes it to where it's unsafe for drivers/cyclists	Art & Wine Faire	Carmelita
Laurel near Arroyo: ADA improvements needed	Art & Wine Faire	Laurel near Arroyo
Hull Dr: Less cars parked, creates blindspots	Art & Wine Faire	Hull Drive

Old County is very unsafe currently for bikers with parked cars and 2 lanes of traffic. Not enough space.	Art & Wine Faire	Old County Road
Safe to cross over 101 to Bay Trail. Super unsafe now	Art & Wine Faire	Highway 101
Debris and glass on Old County Road	Art & Wine Faire	Old County Road
Industrial Rd - N of Redwood City	Art & Wine Faire	Industrial Road
Safe across San Carlos Ave to school	Art & Wine Faire	San Carlos Avenue
Downtown-Laurel generally unsafe on bike	Art & Wine Faire	Downtown - Laurel
Make it more walkable by Arundel	Art & Wine Faire	Arundel
Drive to La Cañada. Won't bike on Ralston	Art & Wine Faire	Ralston
Need bike paths and walkways <u>not</u> on streets	Art & Wine Faire	No exact location provided
Going to T.L. from <u>any</u> direction (I live on Palm Ave) is <u>scary</u> . Also, with parking San Carlos Ave needs <u>bike lane</u> , not just a painted symbol	Art & Wine Faire	No exact location provided
No sidewalk or lane is here	Art & Wine Faire	Alameda De Las Pulgas
Connect biking to Sawyer Camp and Cañada Rd!	Art & Wine Faire	No exact location provided
Dangerous for bike/ped and kids	Art & Wine Faire	San Carlos Ave near Arundel Elementary
Need separated bike lanes	Art & Wine Faire	Belmont Station
Educate drivers on "Dutch reach"	Art & Wine Faire	No exact location provided
Crossing El Camino is hard	Art & Wine Faire	El Camino Real
Would bike to Menlo Park if better crossing over 101	Art & Wine Faire	No exact location provided
Airport--safer and wider--especially on bridge and sidewalk all the way to Edgewood Park.	Art & Wine Faire	No exact location provided
More bike trails! Separate from cars... I won't ride with cars.	Art & Wine Faire	No exact location provided
Would love to see more trails and places to feel safe without worry and heavy traffic.	Art & Wine Faire	No exact location provided
Driver upset at peds jaywalking and cyclists biking out of bike lane. We need education for drivers, bikes and peds.	Art & Wine Faire	No exact location provided
Repaint bike lanes that are faded.	Art & Wine Faire	No exact location provided
Better traffic enforcement at major San Carlos Ave intersections.	Art & Wine Faire	No exact location provided

Video cameras if better traffic enforcement at intersections isn't feasible. Subsidize cameras for homeowners to help add greater awareness for those homes who are subjected to constant speeders.	Art & Wine Faire	No exact location provided
Raised sidewalks.	Art & Wine Faire	No exact location provided
Traffic awareness campaigns quarterly.	Art & Wine Faire	No exact location provided
High school outreach specifically.	Art & Wine Faire	No exact location provided
Please reopen Postman's Path for bicycles.	Art & Wine Faire	Postman's Path
Protected bike lanes and more bike lanes.	Art & Wine Faire	No exact location provided
We need separated bikeways--especially on major arteries like Brittan, San Carlos Avenue, and El Camino	Art & Wine Faire	No exact location provided
General transformation of Downtown and White Oaks to bike-first community. Please!	Art & Wine Faire	Downtown and White Oaks
Kids bike paths are not safe. We need more safe paths.	Art & Wine Faire	No exact location provided
Children need better bike safety education.	Art & Wine Faire	No exact location provided
Would like to see more bike parking.	Art & Wine Faire	No exact location provided
Please make sure Uber and Lyft drivers stay out of the bike lane.	Art & Wine Faire	No exact location provided
No light wait past 40 seconds anywhere in the city.	Art & Wine Faire	No exact location provided
Create <u>safe</u> connections among bikes/peds/cars. I am also concerned about blind spots for drivers at driveways.	Art & Wine Faire	No exact location provided
Adding 4-way stop because currently cut-through	ACC	White Oak Way next to Elm
New construction. Red curb. Improve. Trucks parked and visibility is bad.	ACC	Chestnut and Cherry
Trucks parked too close to exist of their apartment. City does not pave red.	ACC	No exact location provided
Improve visibility around.	ACC	White Oaks Elementary
Green paint on bike lanes.	ACC	Alameda
Blinking... Could it aso be at Brit Ave.	ACC	No exact location provided
Scooters - Not on sidewalk, on bike lanes.	ACC	No exact location provided
PD - Get on bikes, as good model, see what kids are going through.	ACC	No exact location provided

Green paint.	ACC	No exact location provided
Sand Hill as example	ACC	No exact location provided
RWC Brittan Acres	ACC	Howard
Neighborhood Watch - Connect with these groups... Next Door.	ACC	No exact location provided
More school signage.	ACC	No exact location provided
People double-park	ACC	Heather School
Roundabout has helped slow people down on Howard.	ACC	Howard
Marshalls!	ACC	Marshall's
crosswalk at Cherry and Chestnut. Walking Group meets every Friday morning.	ACC	Chestnut and Cherry
Some people have been injured because of falling; cracks and uneven sidewalk.	ACC	No exact location provided
Driver Education - Watch on right-hand turns; polite behaviors.	ACC	No exact location provided
Fast cars on San Carlos... Posted speed limit...	ACC	San Carlos
Better signage around center.	ACC	No exact location provided
Bike pathway to the schools.	Farmer's Market	No exact location provided
Brittan crossing is difficult for pedestrians where the sidewalks end and you have to cross the street. Medkin is difficult to cross...	Farmer's Market	Brittan Avenue
El Camino/Holly: Unsafe crossing because cars northbound turning right on Holly don't look for pedestrians (and sometimes don't stop at all).	Farmer's Market	El Camino Real at Holly
San Carlos Avenue: No sidewalk. Can't walk from hotel to downtown.	Farmer's Market	San Carlos Avenue

Getting across from west side to IA/Breweries/wineries. No way to get through.	Farmer's Market	No exact location provided
Park Ave./St. Francis: Cross walk not aligned with kids leaving W.O. school.	Farmer's Market	Park Avenue at St. Francis
Dayton/Brittan: Sidewalk repair. Separated tripping hazard.	Farmer's Market	Dayton at Brittan
Brittan crossing (marked) at Tramanto	Farmer's Market	Brittan
Need crossing area between Big Canyon Park and Eaton Park Trail. Prefer something with motion-activated lights. Traffic is <u>fast</u> here and crossing is a little dangerous.	Farmer's Market	Big Canyon Park and Eaton Park Trail
Big Canyon Park: Pedestrian bridge doesn't have handrails.	Farmer's Market	Big Canyon Park
Speed and traffic calming around Heather Elementary. <u>Many</u> children walk to school, especially on Melendy and Hewitt.	Farmer's Market	Melendy/Hewitt
Arroyo/Woodland/Cedar... Drop-off/kids/cyclists conflicts.	Farmer's Market	Arroyo/Woodland/Cedar
Skyway Rd: Connection to path...	Farmer's Market	Skyway Road
Nighttime lighting, pedestrians.	Farmer's Market	No exact location provided
Brittan Avenue, near Big Canyon Park: No cross walk... Trail head.	Farmer's Market	Brittan Avenue near Big Canyon Park
Carmelita Ave/Alameda De Las Pulgas: Needs a crosswalk.	Farmer's Market	Carmelita Avenue at Alameda De Las Pulgas
Prospect St.: Missing sidewalk!	Farmer's Market	Prospect Street
El Camino Real near San Carlos Student Services Preschool: Missing sidewalk. Hard to get to breweries, etc.	Farmer's Market	El Camino Real near San Carlos Student Services Preschool
Skyway Rd: Pedestrian overpass :-)	Farmer's Market	Skyway Road
Cherry/Laurel: Potential new parking garage	Farmer's Market	Cherry at Laurel
US Highway 101: R&D	Farmer's Market	Highway 101
Old County Road: New R&D workers will want to use this connection	Farmer's Market	Old County Road

Between Industrial Rd and Highway 101: Vanishing bike lane	Farmer's Market	Between Industrial Road and Highway 101
Danger zone with freeway on/off-ramps at Holly and 101	Farmer's Market	Holly at Highway 101
Arroyo/El Camino is an obstacle course for bikes and peds. Too many opportunities to crash into poles.	Farmer's Market	Arroyo at El Camino Real
Old County - Wall barrier: Maybe a 2-lane bikway or one side of street?	Farmer's Market	Old County Road
Southbound bike lane near Highway 101 vanishes at light. VERY dangerous.	Farmer's Market	Highway 101
Connection to Belmont bike lanes near Holiday Inn. No existing connection to San Carlos.	Farmer's Market	No exact location provided
Old County bike lane needs improvements. VERY hard on bike tires. No longer want to use it.	Farmer's Market	Old County Road
Bike parking at Burton Park, Trader Joe's.	Farmer's Market	Burton Park
Connect to Peninsula Bikeway in Redwood City	Farmer's Market	Peninsula Bikeway
OCR Road south of Caltrain - bike lane is not continued.	Farmer's Market	Old County Road south of Caltrain
Along Edgewood: Safer, more identifiable bike lane.	Farmer's Market	Edgewood
San Carlos Avenue near Palm Ave: Increase safety for cyclists.	Farmer's Market	San Carlos Avenue near Palm Avenue
Alameda De Las Pulgas near Lupin Avenue: Too steep.	Farmer's Market	Alameda De Las Pulgas near Lupin Avenue
Skyway Rd: Pedestrian overpass	Farmer's Market	Skyway Road
Cedar Street: Lots of kids biking to school	Farmer's Market	Cedar Street
Phelps Rd/Palm Ave: Bottleneck. No space for walking and biking.	Farmer's Market	Phelps Road at Palm Avenue
Club Dr: No bike lane - to middle school, very steep.	Farmer's Market	Club Drive

Club Dr: Kids ride their bikes down - cars are moving too fast. Speed bump	Farmer's Market	Club Drive
Club Dr: Walking path to school	Farmer's Market	Club Drive
Magnolia Ave near Spring St: Hole in wall by CVS	Farmer's Market	Magnolia Avenue near Spring Street
<p>Peninsula Bikeway Suggested Route(s)</p> <p>my suggestion is that our part of the Peninsula Bikeway that starts at Warwick go west on Eaton, north on Cedar to Arroyo, and then forks to two options: The Downtown/Train Station option goes east on Arroyo, crosses El Camino, goes through the nice wide tunnel under the tracks, and drops right onto Old County Road, where it goes past the train station and north to Belmont. The North San Carlos option continues on Cedar St. all the way to Magnolia, heads east through the roundabout, to Laurel Street, follows Laurel north all the way to Hull, then turns east on Hull to cross El Camino there. From there I'd suggest bikes and pedestrians share the sidewalk on the east side of El Camino for 1 block (along the plaza between My Gym and CVS), to hang a right just past CVS where there is another tunnel underneath the Caltrain, which then also drops a biker back on to Old County Road. Either option takes a biker roughly equidistantly to Old County Road, however the North San Carlos is probably better for more experienced bikers because it has a stretch on Laurel that combines a narrowing of the street on a hill with poor visibility and densely parked cars on both sides of street. (Traffic Commission probably should flag that street for future safety consideration regardless.)</p> <p>Proposed Maps of Peninsula Bikeway—the North San Carlos option: (https://goo.gl/maps/FP4aCGs8RKy), Downtown/Train Station option: (https://goo.gl/maps/6JJkNxxhEKS12)</p>	Other	Peninsula Bikeway
<p>East-West adjoining Arterials</p> <p>East-west Arterials to improve access to downtown and train station for north San Carlans, (and in particular to keep as many bikers as possible off of San Carlos Ave since a right-side bike lane and the right car turn-lane onto El Camino are in pretty significant conflict), we suggest include: Cherry Street, between Cedar and El Camino Real</p> <p>Arroyo and/or Belle between Cedar and Tamarack (for Brittan Acres and St. Charles access) Other suggestions??</p>	Other	No exact location provided

<p>Improved Intersection Safety</p> <p>Tentatively identified key intersections that would benefit from the following video examples of Dutch awesomeness, (as shared by local biker extraordinaire with great ideas Kate Wormington), are:</p> <p>Arroyo @ El Camino Real</p> <p>Cherry @ El Camino Real</p> <p>Hull @ El Camino Real</p> <p>Video 1) Bikes stop ahead of cars for better visibility and separated crossing lights stagger traffic (https://vimeo.com/177962114), and</p> <p>Video 2) Intersection medians carve out paths for safer right AND left turns (https://www.youtube.com/watch?v=FIApbxLz6pA)</p>	Other	No exact location provided
<p>Pedestrian Zone Downtown</p> <p>I also spoke with _____, yet another biker extraordinaire in San Carlos with excellent ideas for sensible bicycle infrastructure in our fair town. He spoke about the idea of a 'Superblock', a downtown Pedestrian Zone that is blocked off so a) either only pedestrians and non-motorized wheels are allowed, or b) cars move one-way, racetrack-style up Laurel and down Walnut, with plenty of space for bike & human-powered wheels lanes. Personally I would like to see those two blocks between Arroyo and Cherry totally closed to non-human powered traffic, meaning the Superblock would be flanked on both sides by streets with safe biking infrastructure.</p>	Other	No exact location provided

Workshop #1

Comments

Workshop Comments

Workshop Comments			
Comment	Event	Board	Location
Please reopen "Postman's Path". (One additional commenter agreed with this comment.)	March 5, 2019 Workshop	Comment card	Postman's Path
Provide an educational course for safety, especially for kids riding to and from school. Partner with schools.	March 5, 2019 Workshop	Comment card	No exact location provided
Please address the safety of skateboards and e-scooters on the sidewalks of Laurel Street. Pedestrians should not have to walk in the street in order to avoid these modes of transportation.	March 5, 2019 Workshop	Comment card	Laurel Street
Meeting with Caltrain about proposed bike lane along El Camino Real where parking lots are.	March 5, 2019 Workshop	Comment card	El Camino Real
Need more bike parking.	March 5, 2019 Workshop	Existing Bikeways	Skyway Road
East to west connections to Bay Trail.	March 5, 2019 Workshop	Existing Bikeways	US Highway 101
Kids riding to Sequoia High School all follow the same path.	March 5, 2019 Workshop	Existing Bikeways	Sequoia High School
Narrow!	March 5, 2019 Workshop	Existing Bikeways	Belmont Avenue near Geneva Avenue
Priority route for schools.	March 5, 2019 Workshop	Existing Bikeways	Cedar Street at Belmont Avenue
Missing gap.	March 5, 2019 Workshop	Existing Bikeways	San Carlos Station near San Carlos Avenue at Walnut Street
Needs bike parking.	March 5, 2019 Workshop	Existing Bikeways	Laurel Street
Central Arroyo students cross dangerously around the school.	March 5, 2019 Workshop	Existing Bikeways	Central Arroyo School

Workshop Comments

Going north even on nice days against prevailing wind.	March 5, 2019 Workshop	Existing Bikeways	Old County Road at Taylor Way
Consider walkway/bikeway into this cul de sac.	March 5, 2019 Workshop	Existing Bikeways	Brook Street at Alameda De Las Pulgas
Light at Brittan at Alameda De Las Pulgas will <u>not</u> change for bike (going on 27 years).	March 5, 2019 Workshop	Existing Bikeways	Brittan at Alameda De Las Pulgas
Reduce vehicle speed to 15 MPH on all bike routes. (One additional commenter agreed with this comment.)	March 5, 2019 Workshop	Existing Bikeways	No exact location provided
Need bike rodeos (education) at all schools, both public and private.	March 5, 2019 Workshop	Existing Bikeways	No exact location provided
Turning left onto St. Francis from Cedar is crazy.	March 5, 2019 Workshop	Existing Bikeways	St. Francis at Cedar Street
Most people drive in bike lane.	March 5, 2019 Workshop	Existing Bikeways	Near Arundel Elementary
Although showing actual collision sites <u>is</u> noteworthy, it does not fully encompass pedestrian safety on our streets.	March 5, 2019 Workshop	Pedestrian Collisions	No exact location provided
Streetlight needs to prioritize pedestrians over cars. (One additional commenter agreed with this comment.)	March 5, 2019 Workshop	Pedestrian Collisions	San Carlos Station
No light needed.	March 5, 2019 Workshop	Collisions	Holly Street near Magnolia Avenue
Diagonal pedestrian crossing.	March 5, 2019 Workshop	Collisions	Walnut Street at Cherry Street
Needs 4-way stop.	March 5, 2019 Workshop	Collisions	Olive Street at Elm Street
Child crossing sign.	March 5, 2019 Workshop	Collisions	Avenue and Woodland Avenue
Students need to cross Brittan Avenue.	March 5, 2019 Workshop	Collisions	Brittan Avenue at Cedar Street

Workshop Comments

Make Holly Street one lane in each direction. Again. (One additional commenter agreed with this comment.)	March 5, 2019 Workshop	Pedestrian Collisions	Holly Street
Child crossing sign.	March 5, 2019 Workshop	Collisions	Manzanita Avenue at Sycamore Street
Difficult to cross.	March 5, 2019 Workshop	Collisions	Alameda De La Pulgas
Going to be very dangerous with the new high-density development on Black Mountain Water property.	March 5, 2019 Workshop	Bicycle Collisions	Tamarack Avenue between Alma Street and Arroyo Avenue
Divert auto traffic from downtown.	March 5, 2019 Workshop	Bicycle Collisions	Downtown San Carlos
Pinch point no matter what.	March 5, 2019 Workshop	Bicycle Collisions	Holly Street at El Camino Real
Redirect vehicle traffic?	March 5, 2019 Workshop	Bicycle Collisions	Holly Street
Route to school challenging for bicycles and cars.	March 5, 2019 Workshop	Bicycle Collisions	Cedar Street between White Oak Way and Central Middle School
Bike lane to Brittan Acres School.	March 5, 2019 Workshop	Bicycle Collisions	Brittan Acres School
Need a park	March 5, 2019 Workshop	Sidewalk Presence	Old County Road at Commercial Street
No sidewalk and we would like a crosswalk at 3 places: Alma Street, Carmelita and San Carlos Avenue.	March 5, 2019 Workshop	Sidewalk Presence	Cordilleras Avenue between San Carlos Avenue and Elizabeth
No sidewalk here.	March 5, 2019 Workshop	Sidewalk Presence	Howard Avenue at Bayport Avenue
Now used as cutthrough.	March 5, 2019 Workshop	Sidewalk Presence	El m Street between Magnolia Avenue and Eaton Avenue
Make sure trails connect.	March 5, 2019 Workshop	Sidewalk Presence	Near Highlands Park
Need a park.	March 5, 2019 Workshop	Sidewalk Presence	Timothy Drive near De Anza Avenue
More traffic calming.	March 5, 2019 Workshop	Sidewalk Presence	Alberta Avenue near White Oaks Elementary
Connect to 280.	March 5, 2019 Workshop	Sidewalk Presence	Crestview Drive

Workshop Comments

Direct car traffic.	March 5, 2019 Workshop	Sidewalk Presence	Holly Street near Magnolia Avenue
Very narrow, no sidewalks, no lighting, parking old cars.	March 5, 2019 Workshop	Sidewalk Presence	Manor Drive
Hiking/biking trails.	March 5, 2019 Workshop	Sidewalk Presence	Near Highlands Park
Would like to walk here more.	March 5, 2019 Workshop	Sidewalk Presence	Between Orange Avenue and Cordilleras Avenue
Crossing guard.	March 5, 2019 Workshop	Sidewalk Presence	Brittan Avenue at Orange Avenue
Crossing guard.	March 5, 2019 Workshop	Sidewalk Presence	Belle Avenue at Cordilleras Avenue
Tricky intersection.	March 5, 2019 Workshop	Sidewalk Presence	Arroyo Avenue at Cedar Street
Along Alameda.	March 5, 2019 Workshop	Types - Sidewalk +	Alameda De Las Pulgas
Shade trees: East side residential streets.	March 5, 2019 Workshop	Types - Sidewalk +	No exact location provided
Every parking lot in San Carlos needs to be at least 50%-70% shaded.	March 5, 2019 Workshop	Pedestrian Facility Types - Sidewalk + Path Improvements	No exact location provided
Pedestrian refuge islands: Our refuges don't have poles.	March 5, 2019 Workshop	Types - Crossing Improvements	No exact location provided
Pedestrian refuge islands.	March 5, 2019 Workshop	Types - Crossing	Holly at Old County Road
Pedestrian refuge islands.	March 5, 2019 Workshop	Types - Crossing	Holly at El Camino
Curb extensions: Everywhere people walk.	March 5, 2019 Workshop	Types - Crossing	No exact location provided
This is a signal to drivers that people walk and bike here.	March 5, 2019 Workshop	Types - Crossing Improvements	No exact location provided
distance.	March 5, 2019 Workshop	Types - Crossing	No exact location provided
Curb ramps: Corners of all residential streets.	March 5, 2019 Workshop	Types - ADA	No exact location provided
Curb ramps: Isn't there a better design?	March 5, 2019 Workshop	Types - ADA	No exact location provided
Connectedness is key! Align with Redwood City, Belmont, and beyond!	March 5, 2019 Workshop	Vision for an Active San Carlos	
San Carlos participates in bike/ped infrastructure like the cities around us.	March 5, 2019 Workshop		
Start an electric shuttle service, like e-scooters.	March 5, 2019 Workshop		

Workshop Comments

Find a more inclusive name that covers e-scooters, skateboards, etc., beyond bikes and peds.	March 5, 2019 Workshop		
Actually solve access to Bay Trail this year.	March 5, 2019 Workshop		
In NYC's Central Park, they broke it up into particular areas and people deeply cared about their <u>one</u> small flowerbed. That's what we need here. Arundel Park volunteers, etc.	March 5, 2019 Workshop		Arundel Park
San Carlos needs an opportunity for community involvement; a pedestrian committee, bike committee, Arundel Park, Highland Park, etc.	March 5, 2019 Workshop		
Make sidewalks (corners) handicap accessible.	March 5, 2019 Workshop		
Too many parents driving kids to school!	March 5, 2019 Workshop		
Ovrepass over 101!	March 5, 2019 Workshop		
Protecting the bike lane to get to Redwood Shore.	March 5, 2019 Workshop		Industrial at Brittan
Needs safety improvements, such as a scramble system.	March 5, 2019 Workshop		Laurel at San Carlos
There is no way to cross as a pedestrian. Slow down cars with a stop sign.	March 5, 2019 Workshop		Melendy Drive at Crestview
Emphasize bike safety. Teach riders that stop signs and traffic signals must be obeyed.	March 5, 2019 Workshop		
Driver education could provide huge benefits. Especially for kids!	March 5, 2019 Workshop		

Workshop Comments

Being able to take my dog with me... And ditch the car! Groceries, errands, made more safe and accessible for dog walking.	March 5, 2019 Workshop		
Make biking and walking a lifestyle, not just for commute. Paradise as we replace car trips with e-bikes and strolls!	March 5, 2019 Workshop		
City o' good living equals bikes and walkers love it... Not there yet!	March 5, 2019 Workshop		
Bring back Scoot. Why pay Uber or Lyft? (Two additional commenters agreed with this comment.)	March 5, 2019 Workshop		
Consider low-tech solutions like reverse 45 degree parking and planters to enable bikes and pedestrians but reroute cars.	March 5, 2019 Workshop		
Make Holly Street and El Camino Real safe. Fast, illegal right-hand turns and out of control intersection.	March 5, 2019 Workshop		Holly Street at El Camino Real
North to south connections to neighboring cities.	March 5, 2019 Workshop		
Move connected bike routes, as they are very disassociated right now. (One additional commenter agreed with this comment.)	March 5, 2019 Workshop		
Connect San Carlos to the Bay Trail for recreational biking.	March 5, 2019 Workshop		

Workshop Comments

Overpass over 101: Unsafe crossing.	March 5, 2019 Workshop		
More connected long distance bike route.	March 5, 2019 Workshop		Belm to San Carlos to RWC
Create a culture that is HUMAN-centered.	March 5, 2019 Workshop		
Work with NACTO on bike/ped design.	March 5, 2019 Workshop		
Bike access to work, shop, home, and trails.	March 5, 2019 Workshop		
Include e-bikes! Safety, parking is expensive, easy routes, etc.	March 5, 2019 Workshop		
A bicycle utopia! Where people love to ride bikes around neighborhoods and downtown.	March 5, 2019 Workshop		
Don't make Holly Street a thoroughfare only. Improve residential quality of life.	March 5, 2019 Workshop		
Share the parking in alleys by Laurel. Replace street parking with protected lanes.	March 5, 2019 Workshop		Laurel Street
Make Laurel Street a one-way. (One additional commenter agreed with this comment.)	March 5, 2019 Workshop		
Put scrubbers at wood-fired restaurants. Smoke equals allergen, particulates, stinky.	March 5, 2019 Workshop		
Outdoor escalators in BMS property to bring pedestrians off the hill.	March 5, 2019 Workshop		
Caltrain needs to make more stops in San Carlos so we can get from place to place more often.	March 5, 2019 Workshop		

Workshop Comments

More pedestrian connections between neighborhoods, e.g. Hillcrest and Sycamore Street.	March 5, 2019 Workshop		
Please be mindful of <u>double standards</u> . Private automobiles little the public right-of-way for <u>free</u> . The ROW should be used to <u>MOVE</u> people. Therefore: Do not hesitate to replace parking on the street with bike lanes.	March 5, 2019 Workshop		
Make it so I can do all my shopping, eating, and errands by bike to replace my car.	March 5, 2019 Workshop		
Look at Pearl Street in Boulder, CO as an example of pedestrian-only streets.	March 5, 2019 Workshop		
Pedestrians and cyclists <u>must</u> feel safer at intersections.	March 5, 2019 Workshop		
No parking cars on sidewalks, it's illegal.	March 5, 2019 Workshop		
Give more access to <u>senior citizens</u> around the downtown.	March 5, 2019 Workshop		Downtown San Carlos
Give aggressive options for comprehensive bike/walk lifestyle.	March 5, 2019 Workshop		
Exercise stations for adults at Laureola Park.	March 5, 2019 Workshop		Laureola Park
Create more access to creeks.	March 5, 2019 Workshop		
Set up exercise stations all over San Carlos.	March 5, 2019 Workshop		

Workshop Comments

Coordinate with (and develop) a much better transit system.	March 5, 2019 Workshop		
San Carlos has to get bike- and pedestrian-friendly soon or we'll be moving away.	March 5, 2019 Workshop		
Secure bike parking.	March 5, 2019 Workshop		
Set up 50-100 safe bike racks downtown, length of Laurel.	March 5, 2019 Workshop		Downtown San Carlos
Close portions of Laurel Street to traffic. The restaurants are taking over the sidewalks. I frequently walk downtown with my dog. If I pass a restaurant with outdoor seating and someone has a dog, a stroller, a wheelchair, etc. and it's challenging to get through.	March 5, 2019 Workshop		Laurel Street
Fewer cars, bike lanes, larger sidewalks.	March 5, 2019 Workshop		Laurel Street
Close first block of Laurel on weekends for trial period. (Two additional commenters agreed with this comment.)	March 5, 2019 Workshop		Laurel Street
Close off a couple of streets on Laurel Street to pedestrians only.	March 5, 2019 Workshop		Laurel Street
No scooters on sidewalks, they are going too fast.	March 5, 2019 Workshop		
Enforce no u-turns on Laurel.	March 5, 2019 Workshop		Laurel Street
Pedestrian- and bike-friendly city.	March 5, 2019 Workshop		

Workshop Comments

Separated 2-way bike path along Caltrain on east side of El Camino Real.	March 5, 2019 Workshop		El Camino Real near Caltrain Station
Make El Camino bike-friendly! (Seven additional commenters agreed with this comment.)	March 5, 2019 Workshop		El Camino Real
Connect bike lanes from Belmont to San Carlos to Redwood City, etc. (Two additional commenters agreed with this comment.)	March 5, 2019 Workshop		No exact location provided
Connect eastside to westside for pedestrians and bikes. (One additional commenter agreed with this comment.)	March 5, 2019 Workshop		No exact location provided
East side except for residential streets are generally not suitable for pedestrians.	March 5, 2019 Workshop		No exact location provided
Bury through traffic on Holly Street in tunnel or trench. Save residential quality of life.	March 5, 2019 Workshop		Holly Street
Adult play equipment at Laureola Park, such as pull-up bars, swings, etc.	March 5, 2019 Workshop		Laureola Park
Bike/ped overpass over 101 from Holly to Redwood Shore - when? My coworker was almost killed at that crossing, resulting in a concussion and two broken wrists while biking.		Comment card	Over Highway 101 from Holly to Redwood Shore
I was almost run down with four of us in the crosswalk during broad daylight.		Comment card	Laurel/San Carlos Intersection

Workshop Comments

No parking one side, protected bike lane.		Comment card	Elm - one-way north
No parking one side, protected bike lane.		Comment card	Cedar - one-way south
Protected bike lane under train and bike parking at train station.		Comment card	Brittain East/West to Industrial through Old County one-way north
<p>As children, bicycles were our primary mode of transportation: going to school, after school travel, etc. Our travel was confined to residential streets on which volumes and speeds were low and slow. As we grew older and got a driver's license, we switched from bicycles to automobiles. Designs of streets reflected this use. Protected bike lanes that physically separated bicycle from auto traffic were not part of the design. As the gas tax, VLF, etc. paid for highway and street improvements, omitting bikes—which paid no taxes, made no financial contributions and were used principally by children—from the design was uncontroversial. Over the past several years, there's been a change of attitude toward bike use; bikes are seen as promoting a healthy lifestyle, good for the environment, and so forth. This change in attitude has spawned a belief by some that provision of bike lanes and other bicycle activity is now a charge on the public purse. Since our streets and principal transit arteries were designed without consideration for bicycles, bike use and safety are an afterthought, a bolt-on. For example, bike lanes are stripes painted on streets. These substitute for physically removing bike traffic from motor vehicle exposure. Additionally, motorists are exposed to greater liability in the event of auto/bicycle encounters due to proximity of increased</p>		Letter	

Workshop Comments

Other areas that need improvement: Bicyclists are required to observe all traffic laws. Exceptions for bicyclists are few. Bicyclists do not seem to believe this. I regularly observe bicyclists failing to stop at stop signs and proceeding through intersections when the traffic signal is red. I see them riding on the sidewalk and not at the speed of a pedestrian.		Letter	
Bike rental units, e.g. Lime Bike, are parked inappropriately after use. There must be some better place than the middle of the sidewalk to leave a rental unit.		Letter	
Add ADA access to curb connectivity.		Existing Bikeways Board	Highlands Park
Switch back path to Highlands Park at Coronado Avenue up to park.		Existing Bikeways Board	Highlands Park
Cars moving fast around this sweeping curve often drift into bike lane. Is there a possibility for speed bumps or soft hit posts?		Existing Bikeways Board	Near Brittan Acres Elementary
No streets in San Carlos are low to moderate auto travel. Pretty much none feel safe to people in the 50-60 age group.		Existing Bikeways Board	No exact location provided

Workshop Comments

Upgrade Cedar to Class II. There is plenty of right-of-way, lots of student bike traffic to/from Sequoia High School.		Existing Bikeways Board	Cedar Street near Sequoia High School
Bike lane should go all the way to El Camino.		Existing Bikeways Board	El Camino near San Carlos Avenue
Separate bike lane on San Carlos Avenue.		Existing Bikeways Board	San Carlos Avenue
Road condition is bad in places on OCR.		Existing Bikeways Board	Old County Road
Bike parking at Caltrain and downtown needs locked rooms accessible only by card to easily and securely park expensive e-bikes, etc.		Existing Bikeways Board	Caltrain Station and downtown San Carlos
Downtown Laurel Street is dangerous for bicyclists. (Four additional commenters agreed.)		Existing Bikeways Board	Laurel Street
Overpass North for cars and non-cars at Brittan		Existing Bikeways Board	Brittan Avenue
Bike share near train station would be great.		Existing Bikeways Board	Caltrain Station
Class IV lanes wherever feasible.		Existing Bikeways Board	No exact location provided
Plan for e-bikes and scooters as well as bikes... They're here, like it or not.		Existing Bikeways Board	No exact location provided
You don't have pedestrian collisions because parents won't let their kids walk or bike to school.		Pedestrian Collisions Board	No exact location provided
Elm is unsafe between Arroyo and San Carlos Avenue for bikes, cars and pedestrians.		Pedestrian Collisions Board	Elm, between Arroyo and San Carlos Avenue

Workshop Comments

Add sidewalk to east side of Alameda De Las Pulgas next to Tierra Linda School.		Pedestrian Collisions Board	Near Tierra Linda Middle School
Need sidewalk on south side of club between San Carlos Avenue and Exeter		Pedestrian Collisions Board	Between San Carlos Avenue and Exeter
Need no left turn onto Dartmouth from San Carlos Avenue, as it is dangerous.		Pedestrian Collisions Board	Dartmouth at San Carlos Avenue
Finish sidewalks and bike/scooter/etc. "pathways" on San Carlos Avenue		Pedestrian Collisions Board	San Carlos Avenue
Exit Crestview Park to Crestview Drive is very dangerous! Cannot see southbound cars on existing path!		Pedestrian Collisions Board	Crestview Park at Crestview Drive
Need sidewalks along Cordilleras.		Pedestrian Collisions Board	Cordilleras
Especially need a crosswalk at Alma and Cordilleras.		Pedestrian Collisions Board	Cordilleras at Alma
Cars block both crosswalks and the intersection. Enforcement is needed.		Pedestrian Collisions Board	Northbound El Camino Real to eastbound Holly
Pedestrian crossing across El Camino and Oak is a death trap. Please install HAWK.		Pedestrian Collisions Board	El Camino at Oak
300 block of Elm is a blind curve on a narrow street. Needs speed bumps to slow down cars. Used to go to Belmont by many commuters.		Pedestrian Collisions Board	Elm at Oak
Widen current sidewalks.		Pedestrian Collisions Board	San Carlos Avenue at Cedar Street
Put through traffic on Holly in trench or tunnel.		Pedestrian Collisions Board	Holly Street

Workshop Comments

Dangerous crossing overpass?		Pedestrian Collisions Board	El Camino Real at Holly Street
TOP pedestrian danger zone.		Pedestrian Collisions Board	Near Belmon Station
Move both transit village pedestrians and GE SC residents to station and downtown.		Pedestrian Collisions Board	No exact location provided
Sidewalk space and room for people to walk even with outdoor restaurant tables.		Pedestrian Collisions Board	Near San Carlos Airport
Enforcement needed as well as planning and design.		Pedestrian Collisions Board	No exact location provided
Connect Carmelita all the way downtown for walking and biking using existing right-of-way!		Pedestrian Collisions Board	Carmelita
Intersection of Cedar and Cherry needs all-way stop. Cars on Cedar go way too fast approaching crosswalk. Turning cars from Cherry are too focused on poorly visible road to pay attentiton to pedestrians in crosswalk.		Pedestrian Collisions Board	Cedar Street at Cherry
People jaywalk across Old County Road <u>all</u> the time at the Caltrain Station because the pedestrian light doesn't react when the button is pushed (except on weekend).		Pedestrian Collisions Board	Old County Road near Caltrain Station
Pedestrian signal doesn't work.		Pedestrian Collisions Board	Arroyo at El Camino Road
I've been almost t-boned on Rosewood because Morse is a "yield".		Pedestrian Collisions Board	Rosewood at Morse

Workshop Comments

No stop.		Pedestrian Collisions Board	Baytree Road at Woodland Avenue
This curve is <u>very</u> dangerous.		Pedestrian Collisions Board	Chestnut near Mustang Hall
RRFB at Brittan and Woodland Avenue. Kid was hit by car in October 2018 at 2 p.m. People avoid this route to Burton and schools because cars don't stop for pedestrians crossing crosswalk.		Pedestrian Collisions Board	Brittan Avenue at Woodland Avenue
Should be one-way street. It's way too narrow for parking and two-way traffic.		Pedestrian Collisions Board	Greenwood Avenue between Elm and Laurel
Can we close Fairmont during school start and end times?		Pedestrian Collisions Board	Fairmont
Need school buses!		Board	Wellington Drive
Can't read street signs because of trees and bushes.		Sidewalk Presence Board	No exact location provided
Flashing lights at crosswalk or something to get drivers' attention.		Sidewalk Presence Board	No exact location provided
Sidewalk partial and no good crossing between gaps in sidewalk.		Sidewalk Presence Board	No exact location provided
Crosswalk (school route)		Board	Cordilleras at Alma
Need cut-through and path improvements.		Sidewalk Presence Board	Arquello Park
Should be walkable. Collector of NBD streets.		Sidewalk Presence Board	San Carlos Avenue
More trail connections to uphill streets.		Sidewalk Presence Board	Near Big Canyon Park
No left turn. Dangerous.		Sidewalk Presence Board	San Carlos Avenue at Dartmouth
Cars cut through bike lane.		Board	San Carlos Avenue

Workshop Comments

ADA access for those with a wheelchair.		Sidewalk Presence Board	No exact location provided
Kids are walking in street uphill after school with cars whizzing by and lots of cars parked in street.		Sidewalk Presence Board	Dartmouth near Tierra Lind Middle School
Crossing is dangerous for walking and biking. Needs flashing crosswalk.		Sidewalk Presence Board	Alameda at Carmelita
Curb ramps.		Board	Cedar Street at Manzanita
Sidewalks.		Sidewalk Presence Board	Cedar Street at Prospect Island
Speeding.		Board	Cedar Street
Path between Sycamore and Hillcrest Park		Sidewalk Presence Board	Between Sycamore and Hillcrest Park
Crosswalk is not safe. Vehicles speed on Cedar and vehicles turning from Cherry onto Cedar have terrible visibility and don't pay enough attention to pedestrians. Needs all-way stop.		Sidewalk Presence Board	Cherry at Cedar Street
Cars park on sidewalks and create a danger for pedestrians.		Sidewalk Presence Board	Upper and Lower Lupin
Needs stop sign.		Sidewalk Presence Board	Arroyo at Woodland Avenue
More sidewalks, as crossing on Brittan, Rosewood and Woodland is challenging. Cars don't stop for pedestrians.		Sidewalk Presence Board	Brittan, Rosewood, and Woodland Avenues
Pedestrian augmentation at busy intersection, e.g. post office		Sidewalk Presence Board	No exact location provided
Pedestrian augmentation at Laurel and Cherry at Peet's Coffee.		Sidewalk Presence Board	Laurel Street at Cherry

Workshop Comments

Put cars on sam level as bikes and walking.		Sidewalk Presence Board	No exact location provided
Install RRFBs.		Sidewalk Presence Board	Brittan Avenue at Woodland Avenue
Install RRFBs.		Board	Near Burton Park
Install RRFBs.		Board	Central School
Install RRFBs.		Board	Arroyo School
Commercial driveways are blind to pedestrians.		Sidewalk Presence Board	Holly Street
Automatic pedestrian signals.		Sidewalk Presence Board	Downtown San Carlos
Exits are dangerous to pedstrians.		Sidewalk Presence Board	Old County Road
Focus on crossing from train station to downtown		Sidewalk Presence Board	Caltrain Station
Drivers don't look for pedestrians on the sidewalk.		Sidewalk Presence Board	Old County Road
Dangerous.		Sidewalk Presence Board	Holly Street at Old County Road
Dangerous.		Sidewalk Presence Board	Holly Street at El Camino Real
Crossings!		Board	Holly Street
Overcrossing.		Sidewalk Presence Board	Holly Street at Old County Road
Overcrossing.		Sidewalk Presence Board	Holly Street at El Camino Real
Bike lanes <u>end</u> .		Board	Old County Road
Better light signals.		Sidewalk Presence Board	El Camino Real at Holly Street
Wider bike lanes. When school lets out, the kids take over the street, riding three to four kids across.		Sidewalk Presence Board	Cedar Street
Partial sidewalk.		Board	Alameda De Las Pulgas

Workshop Comments

Sidewalk on Brittan, west of Alameda De Las Pulgas, so that pedestrians don't have to cross Brittan to use sidewalk.		Sidewalk Presence Board	Brittan Avenue at Alameda De Las Pulgas
Cars park on sidewalk by the dozens, especially at night.		Sidewalk Presence Board	Eaton Avenue
Stop signs needed.		Board	Elm at Olive
Apps such as Waze redirect traffic onto smaller streets, transforming the streets into very busy areas.		Sidewalk Presence Board	No exact location provided
Crossing Crestview from Crestview Park is an issue. The park is heavily used all day, especially on weekends, but children also cross to school bus stop daily. This area needs a <u>pedestrian-activated</u> flashing light north on Crestview.		Sidewalk Presence Board	Crestview Park

Workshop Comments

Dangerous for <u>all</u> traffic. Could be improved by putting in a crosswalk across Crestview with pedestrian-activate flashing lights--lights which flash at least 100 yards north of Melendy on Crestview south. See newly installed light on Maple for Kaiser traffic crossing from Kaiser Cypress to parking lot. This would not unduly restrict southbound traffic on Crestview, the main reason for inaction in the past. Even cyclists travel 50 MPH down this piece of road!		Sidewalk Presence Board	Melendy at Crestview
<u>High</u> visibility crosswalks.		Board	No exact location provided
Consistent sidewalks.		Sidewalk Presence Board	From Wellington to San Carlos
Crossing visibility... All-way stop?		Sidewalk Presence Board	Cherry at Cedar Street
San Carlos Avenue is a main thoroughfare from Laurel or train station all the way to Arundel, yet no consistent sidewalks, no way to cross to sidewalk, and dangerous side streets as drivers pay no attention.		Sidewalk Presence Board	San Carlos Avenue from Laurel to Arundel
San Carlos "super block" ped only space downtown.		Bicycle Collisions Board	San Carlos Avenue
Add secure bike parking.		Board	Wheeler Plaza
Grocery store connections and add secure parking.		Bicycle Collisions Board	No exact location provided
Crossings: school children.		Board	San Carlos Avenue

Workshop Comments

Improve east-west connections.		Bicycle Collisions Board	San Carlos Avenue
Old County Road to get to RWC: fast and easy.		Bicycle Collisions Board	Old County Road
Blind curve when making right turn because housing blocks view. Flashing lights would provide warning.		Bicycle Collisions Board	Near Crestview Park
Bike detection at all intersections.		Bicycle Collisions Board	No exact location provided
No bike parking downtown.		Board	Downtown San Carlos
No designated left turn, so cars don't pay attention.		Bicycle Collisions Board	Brittan Avenue at Howard
One-way couplet.		Board	Downtown San Carlos
Add separated bike path wherever possible.		Bicycle Collisions Board	No exact location provided
Need increased awareness around your safety and the safety of others.		Bicycle Collisions Board	No exact location provided
Enforcement for speeding cars to prevent speeding.		Bicycle Collisions Board	No exact location provided
Cars and trucks parked up on sidewalks.		Bicycle Collisions Board	Near San Carlos Airport
Cars parked on street more than before.		Bicycle Collisions Board	Near San Carlos Airport
Who's at fault and looking at bicycle behavior at stop signs and lights.		Bicycle Collisions Board	No exact location provided
Bike lanes on UC end at Belmont and become exceedingly hazardous to Belmont Caltrain.		Bicycle Collisions Board	Belmont Avenue
Shared bicycle lanes are too dependent on rides of highest ability. No room for children, tired adults or adults who don't have access to showers at work.		Bicycle Facility Types Board	No exact location provided (Class I Bikeways section)

Workshop Comments

Connect Carmelita to Prospect for walking and biking only! Right-of-way is there.		Bicycle Facility Types Board	Carmelita (Class I Bikeways section)
A well-lit path between El Camino and train tracks would be ideal.		Bicycle Facility Types Board	El Camino Real (Class I Bikeways section)
Add ADA access to the curb connections the existing switchback multi-use path up to Highlands Park from Coronado Avenue.		Bicycle Facility Types Board	Highlands Park from Coronado Avenue (Class I Bikeways section)
Anything that a car can drive through is not safe for a bike.		Bicycle Facility Types Board	No exact location provided (Class II Bikeways section)
Stop showing "bike lanes" outside of parked cars.		Bicycle Facility Types Board	No exact location provided (Class II Bikeways section)
Green stripe?		Bicycle Facility Types Board	Alameda De Las Pulgas (Class III Bikeways section, bike route)
Route from San Carlos to RWC via Elm to RWC		Bicycle Facility Types Board	San Carlos to RWC via Elm to RWC (Class III Bikeways section, bike route)
Fewer of these, unsafe on Old County Road		Bicycle Facility Types Board	Old County Road (Class III Bikeways section, bike route)
Bike boulevards north/south through residential areas.		Bicycle Facility Types Board	No exact location provided (Class III Bikeways section, bike boulevard)
Bike boulevards Laurel Street through downtown.		Bicycle Facility Types Board	Laurel Street through downtown San Carlos (Class III Bikeways section, bike boulevard)

Workshop Comments

Create over cul de sac streets by creating barricades where bikes and emergency vehicles, if needed, can move through neighborhoods. See Palo Alto.		Bicycle Facility Types Board	No exact location provided (Class III Bikeways section, bike boulevard)
Remove parking on Old County Road and add a separated bike lane. (Five additional commenters agreed.)		Bicycle Facility Types Board	Old County Road (Class IV Bikeways section)
Separated bike lane on San Carlos Avenue. (Four additional commenters agreed.)		Bicycle Facility Types Board	San Carlos Avenue (Class IV Bikeways section)
Separated bike lane on Alameda De Las Pulga. (Three additional commenter agreed.)		Bicycle Facility Types Board	Alameda De Las Pulga (Class IV Bikeways section)
Separated bike lane on Holly. (One additional commenter agreed.)		Bicycle Facility Types Board	Holly (Class IV Bikeways section)
Separated bike lane on Brittan Avenue. (Two additional commenters agreed.)		Bicycle Facility Types Board	Brittan Avenue (Class IV Bikeways section)
Separated bike lane on Howard. (One additional commenter agreed.)		Bicycle Facility Types Board	Howard (Class IV Bikeways section)
Separated bike lane on Howard at Belmont.		Bicycle Facility Types Board	Howard at Belmont (Class IV Bikeways section)
Bicycle-friendly intersection needed.		Bicycle Facility Types Board	Howard at El Camino Real (Support facilities, bicycle-friendly intersections section)
Bicycle-friendly intersection needed. (One additional commenter agreed.)		Bicycle Facility Types Board	Holly at Old County Road (Support facilities, bicycle-friendly intersections section)

Workshop Comments

Bicycle-friendly intersection needed.		Bicycle Facility Types Board	Holly at El Camino Real (Support facilities, bicycle-friendly intersections section)
Bicycle-friendly intersection needed.		Bicycle Facility Types Board	Elm at San Carlos Avenue (Support facilities, bicycle-friendly intersections section)
Bike parking needed.		Bicycle Facility Types Board	Anywhere near restaurants and shops (Support facilities, bike parking section)
Bike parking needed.		Bicycle Facility Types Board	Library? Garage? (Support facilities, bike parking section)
Locked rooms accessible only by card for e-bikes, expensive bikes, etc.		Bicycle Facility Types Board	Downtown San Carlos (Support facilities, bike parking section)
Bike parking needed. (One additional commenter agreed.)		Bicycle Facility Types Board	Caltrain Station (Support facilities, bike parking section)
Bike parking needed.		Bicycle Facility Types Board	Laurel (Support facilities, bike parking section)
Bike parking needed.		Bicycle Facility Types Board	Burton Park (Support facilities, bike parking section)
Bike repair station needed.		Bicycle Facility Types Board	Library? (Support facilities, bike repair stations section)
Bike repair station needed.		Bicycle Facility Types Board	Near dog park? (Support facilities, bike repair stations section)
HAWK beacon needed.		Pedestrian Facility Types Board	Carmelita at Alameda De Las Pulgas
Ramps on Holly are <u>wrong</u> .		Pedestrian Facility Types Board	Holly Street
Why is there no protected bike lane shown?		Pedestrian Facility Types Board	No exact location provided
Need to light parking lot.		Pedestrian Facility Types Board	Wheeler Plaza

Workshop Comments

Curb extensions/bulb-outs.		Pedestrian Facility Types Board	Phelps at San Carlos Avenue
Please actively include a "super block" (e.g. Barcelona) concept for Laurel Street		Pedestrian Facility Types Board	Laurel Street

Workshop #2

Comments

SAN CARLOS BPMP WORKSHOP 2

October 23, 2019

Top Themes:

- Workshop participants were interested in a protected bike facility on either Old County Road OR El Camino Real that connects seamlessly between Belmont and Redwood City.
- Industrial Avenue should not be the only north-south bicycle route, but could provide an important connection to employment areas on Redwood Shores
- Pedestrian recommendations need to consider safe routes to schools within the city
- Cordilleras should have dedicated bicycle facilities that support students getting to school (Britton Acres, St. Charles, Central, Arroyo, Sequoia High School)
- Community support for reopening Postman's path was mixed

Bike Recommendations to consider:

- Bicycle Boulevard on Cordilleras from Brittan Ave to San Carlos Ave
- Cedar Street north of San Carlos Ave should be one way with bike lanes
- Leading Bicycle Interval (LBI) at Holly/El Camino Real, Cherry St/ECR, and Cherry St/Old County Road
- Shared use path from Brittan Ave to Crestview Drive through Big Canyon Park
- Bicycle boulevard on Edgewood from Warwick Street to Elm Street
- Undercrossing to connect St Francis Way and Old County Road
- Secure Bike Parking in Downtown San Carlos
- Left Turn bike box or alternative for Westbound San Carlos onto Alameda de las Pulgas
- Future undercrossing connecting Hull Drive to Taylor Way and bicycle boulevard on Taylor Way
- Extend Laurel Street Bike Boulevard to Belmont
- Extend buffered bike lanes for the length of Alameda de las Pulgas (School District)

Pedestrian Recommendations to consider:

- New pedestrian routes:
 - Add Melendy Drive and Crestview Drive as pedestrian route for school kids
 - Add Carmelita as alternative ped route to San Carlos Ave
 - Continuous sidewalks on Dartmouth to access Tierra Linda School
- New spot improvements
 - Alameda De Las Pulgas and Upper Lupine Way - Reopen switchback
 - Arroyo and Cordilleras – stop sign warrant
 - Cherry/ Cedar – 3-way stop
 - Cherry/ Chestnut – all way stop, crossing improvements
 - Cedar/Arroyo – all way stop, crossing improvements
 - Alameda de las Pulgas/ Howard - RRFB
 - Dartmouth/ San Carlos Ave - No right on red
 - Additional pedestrian underpass near Edgewood (School District)

- Traffic restrictions on Cedar (Cedar/Arroyo intersection) (School District)
- Lighting improvement on San Carlos Ave east of the tracks (Caltrain/SamTrans)
- Crossing improvements at San Carlos and Laurel (Caltrain/SamTrans)
- Club, Beverly, Devonshire, Crestview hill areas (School District)

COMMENTS

What is your priority north-south bike route?

- Doesn't matter which route! Any route that provides a safe, buffered bikeway where cyclists/drivers don't come into conflict and have to fight for right of way (lots of agreement on this)
- Backstreets unless route is super safe (pylons?). If safe, then probably El Camino N-S, maybe Alameda.
- Separate cars from bikes (reroute cars to other arteries)
- ECR or OCR with CalTrain assistance
- Pick one, do it well – full separation. Connectivity north and south and to Bay Trail
- Make Elm and Cedar one-way streets. One north, one south with good bike lanes. Elm is too narrow already – even for cars!
- Either ECR or Old County must seamlessly connect with Belmont bike route and Redwood City
- Yes, seamless connection to Belmont and RWC on whoever route is chosen
- Old County – RWC to Belmont and San Carlos Ave from CalTrain station up to Ralston. Most commuters and kids (San Carlos Ave feeds biggest elementary, biggest middle school and high school and charter school)
- Cordilleras between San Carlos Ave and Britton
- Make driving less attractive – toll during rush hours on El Camino and Alameda
- Cedar
- Old County Road
- Industrial, Britton up to Belmont
- Alameda or Cordilleras
- Alameda and San Carlos Ave, El Camino
- Pick one – Old County, Industrial, El Camino or Laurel and make it a dedicated excellent bike route – segregated
- Cedar – tons of kids biking in AM/PM and not safe. Get rid of parking on one side
- Cordilleras – lots of kids
- El Camino – local businesses, easy to get to Laurel, Sequoia High School, stop lights for nights straightaways
- I use Cedar
- Old County and El Camino
- I encourage peninsula bike path signs running north to south – helpful to drivers and bicycles
- Elm and Laurel
- I use Elm Street north/south
- Do more space!!!

BICYCLE BOARDS

Bicycle Recommendations – Board #1

- Please consider how to make Cordilleras safe for bikers. It isn't now and kids use it to commute
- Please consider Safe Routes to School when identifying places to protect bikers
- Kids use Cordilleras to bike to Britton Acres, St. Charles, Central, Arroyo, Sequoia High School
- The curve along San Carlos Avenue toward Tierra Linda Middle is very dangerous as cars parked in bike lane create a choke point
- Extend bike lanes near Alameda de Las Pulgas to Britton Acres Elementary from Arroyo Avenue
- Cedar Street needs bike lanes; get rid of parking on half of the street
- Connect Warwick Street and Elm Street (not just Cedar)
- A two-directional and safe bike lane on Old Country Road north to south would be magical! And much needed
- Future bike/ped/scooter, etc. under crossing of CalTrain, St. Francis, and Bing
- Commit to one two-way cycle track on either El Camino, Old Country Road, or Industrial. Coordinate with other cities to link
- One-way Elm and Cedar with full side lane/area
- Please add bicycle parking at Downtown Laurel
- Make North Cedar a one-way street and add a bike lane
- Taylor Way bike route – Hull to Industrial
- Future when shopping center redeveloped undercrossing tracks at Hull and Taylor
- The bike/ped path over Holly would be a great way to connect to the Bay Trail to stop along on a long ride
- Participants love the shared use paths
 - Connecting Skyway Road and Industrial
 - Connecting Old Country Road to Industrial

Bicycle Recommendations – Board #2

- Opening the gate to Belmont will reduce traffic on San Carlos Avenue
- Priorities
 - San Carlos Avenue train to Carlmont – buffered lanes
 - Britton-Alameda to Downtown – buffered lane
 - Alameda-Edgewood-San Carlos – buffered lanes
 - Laurel – pedestrian/bikes
- Add shared use path from Britton to Crestview through Big Canyon Park
- St. Francis, Cedar, and Elm proposed routes are very helpful for students
- Add wayfinding signage at Edgewood Road and Warwick Street
- Add bike button or LBI at Old Country Road/Britton/Holly
- Undercrossing at St. Francis to Old Country Road
- Add bike button or LBI to El Camino and Cherry
- Why not buffered Class IIB at San Carlos Ave and Alameda de Las Pulgas?
- Add safe crossing for Carmelita crossing Alameda de las Pulgas and extend Carmelita bike/ped path downtown to Cherry

- Add bumps to protect bike lanes near schools or high traffic areas
- Laurel Street bike boulevard – continue north to Belmont – road is wide enough
- Marked bike turning from westbound San Carlos Avenue to southbound Alameda

Bike Facility Types

- Class III Bikeway
 - Bike Route – pointless unless quiet street and slow traffic (Cedar okay, San Carlos Avenue no way)
- Class IV Bikeways
 - Two-way cycle track – Yes (x2!)
- Citywide Recommendations
 - Bike Parking
 - Locked
 - Weather-proof room too?
 - Surveillance cameras to deter

Bicycle Level of Traffic Street

- Level 1 All Ages and Abilities – Move toward 80% being like this (except hills)
- Shoreway Road – not a green BLTS score yet

PEDESTRIAN BOARDS

Pedestrian Recommendations – Board #1

- Reduce speed limit on Britton?
- Melendy – Pedestrian route for school kids
- Crestview – Pedestrian route for school kids
- Incorporate switchbacks through Big Canyon Park
- Sidewalks or bike lanes on Windsor leading to Devonshire
- Add sidewalk to Tierra Linda Middle School – missing, just gravel for now
- Need improved ped access on hillside roads around schools on Dartmouth!! Refer to Safe Routes to Schools parent survey – this is a major pain point!
- Revive old switchback downhill route next to St. Charles
- Near P26 & P27 recommend four-way stop
- Recommend four-way stop at Arroyo and Cordilleras
- There are utility poles in the middle of sidewalks. Can we move these?
- Pedestrian improvements on Orange
- Pedestrian bridge – Cedar/Arroyo
- Please for the safety of kids and family crossing to go to Burton Park, mark the crossing!
- Stairways connecting to sidewalks so that people who live in Hillcrest area can connect to downtown
- Stop sign at Cherry/Chestnut/Cedar
- Improve public right of way with pedestrian stairway between Sycamore and Chestnut?

- Add sidewalk on eastside between Oak/Hull – missing piece on Laurel Street
- Restrict on-street parking to allow two-way traffic and pedestrians?
- Crosswalks needed at Cherry/Chestnut intersection please!
- Traffic calming beacon crosswalks at Leslie/Melendy and on Leslie between Crestview Park and Big Canyon Park
- Continue Bay Trail north toward Belmont?

Pedestrian Recommendations – Board #2

- Carmelita is alternative to San Carlos
- Please add a trail connection from Big Canyon up to Portofino/Melendy Drive to open up longer hill hikes, connection without having to walk on busy Britton/Alameda de las Pulgas
- Carmelita Ave traffic calming – SR2S
- Safe ped route through the canyon? Sidewalks? Windor? At Torino Drive
- Beverly needs to be addressed as far as pedestrians from Club to Exeter
- Yes! Continuity of sidewalk on at least one side of Britton would be great
- Difficult for peds walking on Britton just east of Alameda de las Pulgas – no sidewalk or one side then other – dangerous to cross with hills and fast cars – couldn't cross from Big Canyon to flat section of San Carlos Ave
- Alameda de las Pulgas/Howard intersection needs pedestrian flashing light. Important ped crossing from east of Alameda de las Pulgas and Eaton open space trails to White Oaks.
- Rapid flash (RRFB) at Arroyo/Cordilleras crosswalk (lots of kids walking and car traffic at school start time). Kids walking at all times – need due to hill and rushing that occurs in area
- Lighting needed at Eucalyptus and Cordillera
- Four-way stop needed at Arroyo and Cordillera
- Please add crosswalk on Phelps at San Carlos Avenue where kids are entering around Arundel Elementary School
- No sidewalk on Cordilleras between San Carlos Ave and Belle Ave
- Please add improvements to Arroyo and Cordilleros – Yes!!!
- We need bicycle and pedestrian improvements at Dartmouth/Club and San Carlos Ave/Alameda
- No right turn on red light from Dartmouth to Club – many misses observed just by me.
- Crosswalk at Orange and Arroyo needed
- Traffic calming on Arroyo hill (people speed down it and kids walking)
- Please make the Safe Routes to Schools the priority pedestrian route
- Please add three-way stop at Cherry and Cedar
- Please add crosswalks at Cherry and Chestnut intersection – high priority!
- Lighting is an issue as pedestrian in winter on San Carlos Ave between Alameda de las Pulgas and Sycamore
- We need four-way stop at Cherry and Walnut – fast traffic on Cherry
- Please add crosswalk across Alameda de las Pulgas at Cordilleros
- Visibility issue at Cherry and Cedar crosswalk on south approach – remove parking space on SE corner
- Cyclists can't make left onto Cedar from Cherry and see oncoming cars – pedestrian risk is high at crosswalk

- Please add crosswalk at Carmelita and Cordilleras
- More streetlights on Chestnut Street

Pedestrian Improvements

- Stop signs – really? Just one at P28?

Pedestrian Collisions

- Recent fatality 2016-2017 at San Carlos Avenue and Carmelita
- Fatality Halloween night 1991 (11-year-old) at San Carlos and Carmelita
- Beverly Drive is one-way street – no sidewalk acts a choke point
- No right turn on red needed at Tierra Linda Middle drop-off point (San Carlos and Dartmouth)

Pedestrian Level of Traffic Street

- Red zone (Level 4)!! Heavily used - Carmelita and Alameda de Las Pulgas
- Eaton Ave and El Camino Level 4 PLTS – Disagree, there are parts of El Camino that are fine and comfortable
- Car businesses need more mirrors and caution – Bransten and Industrial
- Red zone (Level 4) between E San Carlos and Holly Street intersections at Industrial Road

VISION AND GOALS BOARD

Vision and Goals

- Need better east connections
- Need better east-west connections
- Add secure bike parking downtown please!
- Need secure bike parking at City Hall for employees
- Create a network of safe, accessible ped/bike ways between each residential zone and town center (multiple “yes-es” to this!)
- Actively acknowledge the carbon pollution climate crisis as a motivator of this more aggressive plan!
- People who live in the San Carlos hills (especially if they have kids) will still need reliable parking near downtown
- Eliminate street parking downtown but make other provision for displaced parking
- Educate drivers to yield to pedestrians...especially when there’s no clear crosswalk markings
- Adopt the European principle (in the master plan): if cars are going >10mph faster or heavy car traffic, then bicycles must be physically separated (bike path or barrier or behind parked cars) – e.g. Old Country Road, San Carlos Ave, El Camino
- Liberally adopt LBI (leading bike indicator lights) at busy intersections, e.g. Holly/El Camino or Cherry crossing El Camino or Old Country
- Advantaged (closer, safer) parking downtown for bicycles (versus cars)
- Add more bike/ped under crossings below CalTrain tracks

- Safer, more enjoyable, fun, attractive, enticing, cheaper, scenic
- What will be fundamentally new/different in 5 years when implemented?
- Define where – e.g. from neighborhood feeder streets to school, businesses, and transit center?

CROSS SECTIONS BOARDS

Study Corridor: Industrial Road – 50' Width

- Existing Conditions – no
- Concept A
 - Yes! Absolutely puts people first and beautifies (lots of agreement about this)
 - Yes, but is this a heavy bike route? Lane removal creates more traffic?
 - Yes, either Industrial or El Camino or Old County – commit to one dedicated bike route and invest heavily there.
 - This would be great – more buildings going up on Industrial – would be great to encourage safe biking (especially from CalTrain)

Study Corridor: Laurel Street – 50' Width

- Existing Conditions
 - Please take three parking spots and add bike racks. Can be done ASAP at little cost!
 - Worst option – never bike or you will die
 - Horrible
- Concept A
 - Doesn't improve bike or pedestrian safety
 - Honking 10am to 4pm
 - Not ideal, bad
 - Sharrows are terrible
 - How many parking spaces would be impacted?
- Concept B
 - Maybe eliminate parking?? Rush hour, tolls on El Camino/Alameda
 - Better than current but not great – people agree with this
- Concept C
 - Yes please, this is amazing. Let's be trail blazers!
 - Best, brilliant! Amazing – this is my dream. Been waiting for this!
 - Lots of yes-es and positive reactions (+1000)
 - Yes, will increase local business (restaurants especially)
 - This is a city of good living
 - Laurel as pedestrian walkway
 - Community garden
 - Find a place for bikes to travel too (different speeds)
 - No – need bike lane
 - How many parking spots will be impacted?
 - Less need for street parking on Laurel now that parking garage is open. Would love to see car-free pedestrian zone on Laurel

- I work on the 600 block of Laurel and watch near-accidents constantly – concept C would be ideal. Either B or C would be better than what we have. Kids are skateboarding down the middle of the street.

Study Corridor: Old County Road – 45’ Width

- Concept A
 - This is great!
 - Where does parking move to?
 - People first. Let’s prioritize kids and all people before cars
- Concept B
 - Yes, great idea! Sounds safe.
 - Lots of yes-es, fabulous, awesome and hearts for this one!
 - More and safer bike lanes would encourage people to commute via bike and reduce car traffic. Win, win!

Study Corridor: Old County Road – 35’ Width

- It would be nice to have road bumps in patches along bike path where bikes are trapped against wall and it is narrow
- Unhappy with existing conditions
- Concept A
 - Lots of yes-es and stars!
 - There would be a parking impact (board edited to say YES vs. NO)
 - Commit to Old County as a multi-city bike throughway with two-way cycle track. Connect San Mateo, Belmont, San Carlos and Redwood City. Easier and less impact than El Camino; better continuity than Industrial.

Study Corridor: Old County Road – North of Holly – 35’ Width

- Please no sharrows!
- Concept B
 - Bikes constrained by wall – less safety
 - Yes, but protect bike lane
 - Bulb out – compromise on this for bike lane both ways?
- None of the above (unhappy with existing conditions, Concept A and B)
 - Must have dedicated bike lane north and south for safety
 - Pick one of Old County or Industrial or El Camino or Laurel to make local traffic/shopper only and ensure dedicated bicycle lanes both ways
 - Two-way cycle track on whole length of Old County
 - A lot of agreement about the three points above
- Will CalTrain cede land for bike paths? If not, stop using bikes in marketing materials for those “Transit Village” condos

Study Corridor: El Camino Real – South of San Carlos Avenue – 78’ Width

- Existing Conditions – die on bike scenario

- Concept A
 - Several yes-es – not as enthusiastic as other cross section locations
 - Yes, however, what is traffic impact when going from 3x3 to 2x2?
 - Does paint and soft hit posts cost this much?
 - No, if this major change, include bus lanes so that buses can travel faster and more people ride. I would ride “express” buses

Study Corridor: San Carlos Avenue

- Schools in area
 - Arundel
 - Carlmont
 - Armstrong
 - Mariposa
- Concept A
 - Unhappy about this one (due to cars sharing lanes with cars, and unprotected bike lanes/doors opening and hitting cyclists)
- Concept B
 - Great solution for school access
 - Kid friendly along corridor to schools
 - Priority for getting kids to school safely (and out of cars)
 - With safe left turn hatching (San Carlos Ave – Alameda)
 - Absolutely needed – yes by all schools
 - Lots of yes-es, fabulous, stars, happy faces
 - Could you have a hybrid – one two-way bike path separated to road with parking on one side?
 - Yes! I’m a resident and am 100% willing to lose parking to increase safety. This is a safe route to school area and currently very unsafe

COMMENT CARDS

Comments

- See letter from [REDACTED] in files along with photos they attached
- We need to make El Camino and Alameda less attractive as an alternate commuter route by implementing tolls during rush hour using the Fastrak system and having revenue pay for bike/ped improvements [REDACTED]
- If you haven’t done so, please have every member of your team get out to bicycle and walk the routes that you are making recommendations about. Especially areas where you are considering shared car/bike lanes [REDACTED]
- Crosswalk at Cordilleras and Alameda; fix Laurel Street – concept B or C [REDACTED]
[REDACTED]
- I hope that the city will include in this process a look at the 30mph speed limits on Brittan and Crestview. My experience is that this faster speed in residential areas is not safe for pedestrians

and bikers with many commuters using Brittan, in particular, it feels that their urgency to get to 280 detracts from a safe environment for our community [REDACTED]

- I have several friends and acquaintances who live in the San Carlos hills and (because of mobility, kids, pets, cargo) will likely never be bicyclists or pedestrians in a consistent way. They already feel traffic is clogged (San Carlos Ave, El Camino) and are freaked out by the idea of adding bikes and losing parking spaces, which are already at a premium. Question on their behalf: how many parking spaces do we currently have? Will they be relocated? Eliminated? Will there be any new parking structures? [REDACTED]
- Four-way stop at Arroyo and Cordilleras. I've been walking that intersection for Brittan Acres and drop-off/pick-up for 3 years. Cars have hit the yellow ped sign in the crosswalk multiple times. My daughter and I almost got hit by a car on Monday. [REDACTED]
- Sharrows do not provide safety – they may actually encourage a false sense of safety. Please don't recommend, recommend at least Class II.
- Master Plan concepts look great. What are quick win items we can prioritize today? I would like to see more bicycle parking on downtown Laurel Street. Simply remove a few parking spots. [REDACTED]
- Holly over ramp needs protection – bike and walk both directions
- Other countries have a slight grade change between roadway, bike path and walking. Less than 1" – very small but effective [REDACTED]
- Plan must include provision for 1) intense sidewalk maintenance to eliminate tripping hazards 2) Keeping sidewalks free from low hanging tree limbs and encroaching hedges [REDACTED]
[REDACTED] Also see attached photos in files

San Carlos ACC Walking Tour Notes

San Carlos ACC Walking Tour Notes

Friday, March 29, 2019

9-10:30 AM

In Attendance: [REDACTED]

Route: Started at San Carlos Adult Community Center (ACC), heading South on Chestnut, continued on Woodland past Burton Park and across Britton Ave. Took right on Greenwood Avenue and looped back on Cedar, going through Burton Park and back to ACC on Baytree Road.

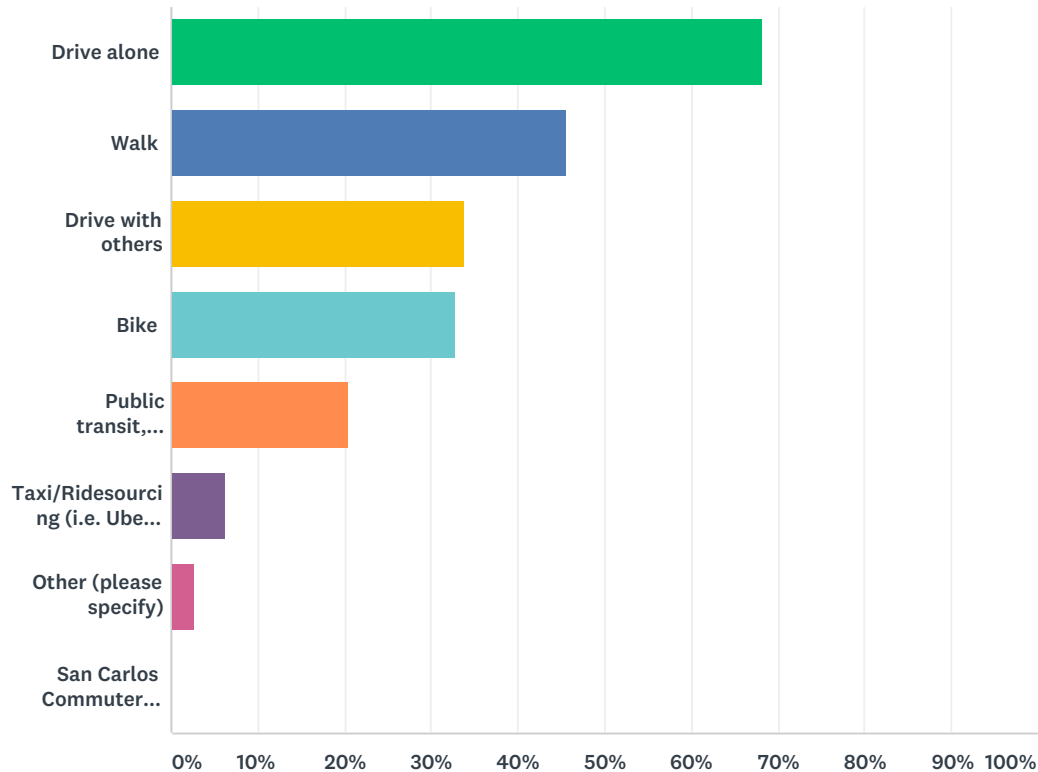
NOTES:

- Chestnut and Cherry – challenging intersection to cross, cars on Cherry Street do not stop. [REDACTED] has gone to City Council to ask for crosswalks at this intersection.
- Baytree and Woodland Avenue is also challenging and wide intersection that is striped strangely, missing curb ramps
- No sidewalks on Woodland and Morse
- Crossing Brittan Ave has higher speed cars, it is unsignalized but cars sometimes stop. There is room for curb extensions even without impeding the bike lanes
- Britton Park has become a dog on leash only park because of issues of off-leash dog park
- No curb cut on Woodland and Greenwood
- Unlevel sidewalk (tripping hazard) on Orange Ave near Brittan Ave
- Bocce Ball is a good community asset, well loved by walking tour attendees
- Walking down San Carlos, there is also a lot of fumes that can make it unpleasant

Online Survey Summary

Q1 In general, how do you typically get to work, school, or other places that you travel to regularly?

Answered: 377 Skipped: 3



ANSWER CHOICES	RESPONSES	
Drive alone	68.17%	257
Walk	45.62%	172
Drive with others	33.95%	128
Bike	32.89%	124
Public transit, including buses, trains, and ferries	20.42%	77
Taxi/Ridesourcing (i.e. Uber, Lyft, etc.)	6.37%	24
Other (please specify)	2.65%	10
San Carlos Commuter Shuttle	0.27%	1
Total Respondents: 377		

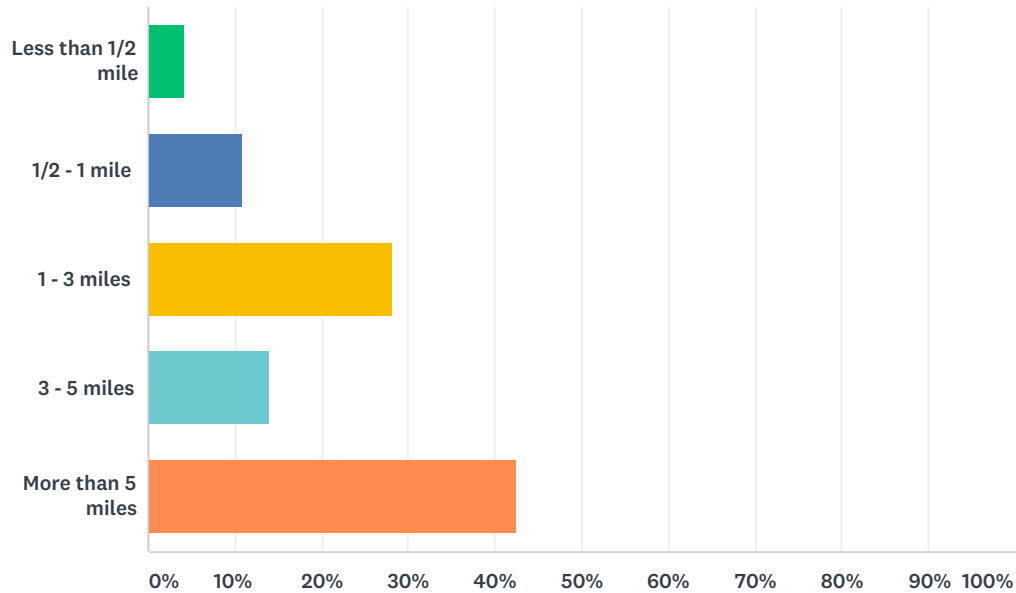
#	OTHER (PLEASE SPECIFY)	DATE
1	Drop off kids at before school care and drive to work	2/27/2019 6:56 AM
2	company shuttle	2/24/2019 3:58 PM
3	I hope to get back to a bike but health has limited this.	2/4/2019 6:55 PM
4	Corporate Shuttle (leaves from CalTrain station)	2/4/2019 10:35 AM

San Carlos BPMP

5	bike or carpool to CalTrain	1/20/2019 4:06 PM
6	Kick scooter (non-electric)	1/18/2019 4:06 PM
7	Kick Scooter	1/16/2019 4:09 PM
8	work from home	1/16/2019 3:59 PM
9	Private Commuter Shuttle, Carpool	1/12/2019 8:12 PM
10	scooter	1/9/2019 10:45 PM

Q2 In general, how far away is your place of employment, school, or places you travel to regularly from your residence?

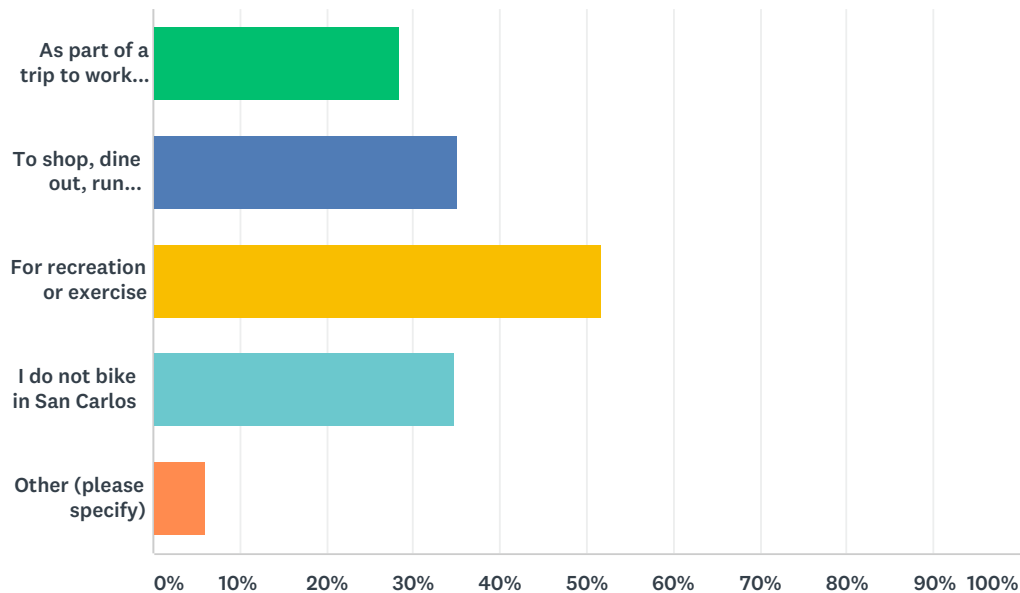
Answered: 376 Skipped: 4



ANSWER CHOICES	RESPONSES	
Less than 1/2 mile	4.26%	16
1/2 - 1 mile	10.90%	41
1 - 3 miles	28.19%	106
3 - 5 miles	14.10%	53
More than 5 miles	42.55%	160
TOTAL		376

Q3 What are the reasons you bike in San Carlos? Please select all that apply.

Answered: 335 Skipped: 45



ANSWER CHOICES	RESPONSES	
As part of a trip to work or school	28.36%	95
To shop, dine out, run errands, visit people, go to an event, or similar activity	35.22%	118
For recreation or exercise	51.64%	173
I do not bike in San Carlos	34.63%	116
Other (please specify)	5.97%	20
Total Respondents: 335		

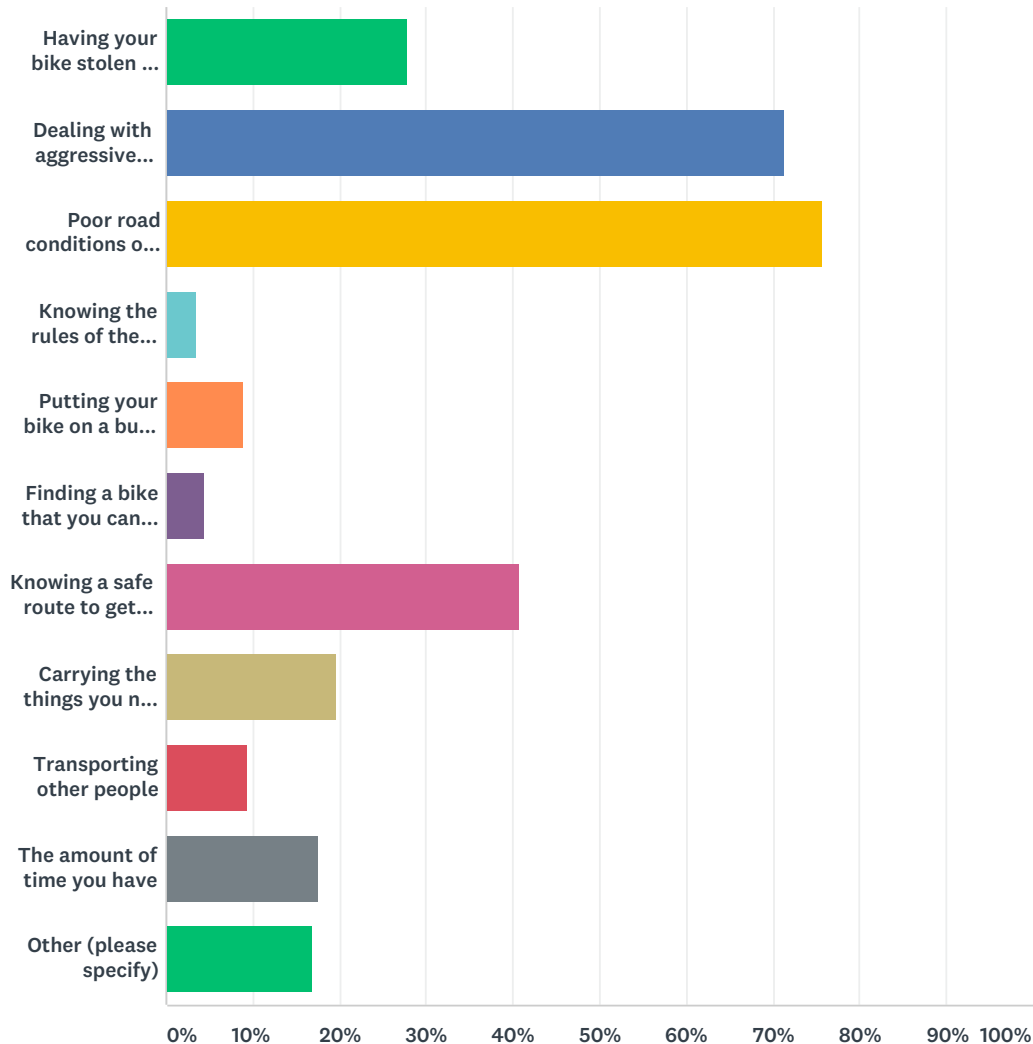
#	OTHER (PLEASE SPECIFY)	DATE
1	Kids enjoy riding so we ride bikes	2/27/2019 6:57 AM
2	We bike with our kids for recreation	2/17/2019 10:59 AM
3	maybe after knee replacement surgery	2/16/2019 8:40 PM
4	too dangerous	2/10/2019 2:19 PM
5	At least I hope to get back to this!	2/4/2019 6:55 PM
6	Kids sports events/lessons	2/4/2019 5:16 PM
7	I would love to bike more, but do not feel it is safe.	2/4/2019 12:28 PM
8	I would like to set up an old bike with a basket or carrier to run errands within San Carlos.	1/26/2019 9:41 PM
9	There is not enough room to bike here. As a pedestrian, I am often infringed on by riders on sidewalks. Adults and children n	1/18/2019 9:16 AM
10	Have curtailed bike riding. Too scary in San Carlos.	1/17/2019 12:43 PM
11	It's faster than driving in horrible traffic and I can take my bike on Caltrain	1/16/2019 5:42 PM

San Carlos BPMP

12	i would if it were safer especially the 101/holly overpass	1/16/2019 9:49 AM
13	Do not bike-walk only	1/15/2019 11:20 AM
14	My children ride for fun and transportation	1/13/2019 10:32 AM
15	Go to the gym in Belmont	1/12/2019 8:13 PM
16	To reach public transit ie train	1/11/2019 5:05 PM
17	errands, avoid trying to find parking for car downtown	1/10/2019 6:28 PM
18	After being hit by a car at El Camino & Greenwood in I will never again bicycle in California.	1/9/2019 7:42 PM
19	If you morons continue to restrict automobile thoroughfares in San Carlos, you will not have service industry businesses to accommodate San Carlos businesses or homeowners.	1/9/2019 2:16 PM
20	Some errands, some recreation.	1/8/2019 2:33 PM

Q4 Which concerns do you have when biking around San Carlos? Please select all that apply.

Answered: 255 Skipped: 125



ANSWER CHOICES	RESPONSES	
Having your bike stolen or vandalized	27.84%	71
Dealing with aggressive drivers	71.37%	182
Poor road conditions or lack of bicycle facilities (i.e. bike lanes, shared lane markings, multi- use trails)	75.69%	193
Knowing the rules of the road for biking	3.53%	9
Putting your bike on a bus or a train	9.02%	23
Finding a bike that you can ride comfortably	4.31%	11
Knowing a safe route to get where you are going	40.78%	104
Carrying the things you need with you	19.61%	50
Transporting other people	9.41%	24

San Carlos BPMP

The amount of time you have	17.65%	45
Other (please specify)	16.86%	43
Total Respondents: 255		

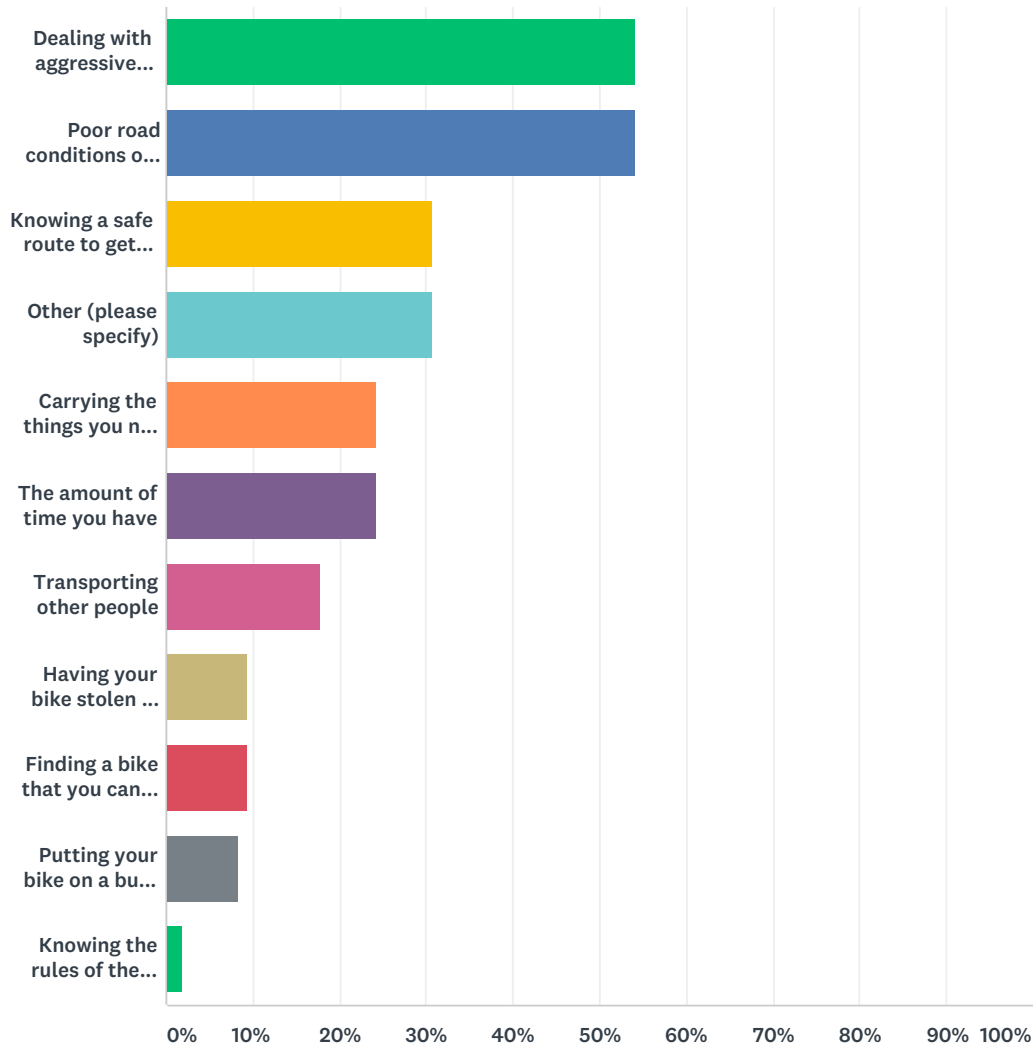
#	OTHER (PLEASE SPECIFY)	DATE
1	No major concerns (typically, I bike from home (Brittan and Alameda) to Laurel St)	3/11/2019 9:32 AM
2	Bike paths are under defined...we need to employ green lanes	3/5/2019 5:23 PM
3	Sheriff told me to move to the right when I rode my bike where shallow directed me in front of Central Middle School. Pavement condition was poor near the edge of street and I didn't want to be doored by parked cars. There was no traffic behind me other than the sheriff and he didn't have his siren on. Sharrows have minimal value to bicyclists, especially on arterial streets where autos travel at 35 mph.	3/3/2019 9:41 PM
4	Navigating the narrow streets when there are cars parked on both sides of the streets, e.g., Cedar Street north of San Carlos Ave.	3/2/2019 12:40 PM
5	Not necessarily "aggressive" drivers, but traffic and inattentive drivers in general.	3/1/2019 5:33 PM
6	Safety of kids as they ride on sidewalks as still younger and I worry about cars but sidewalks narrow	2/27/2019 6:58 AM
7	Opening of parked car doors into bike lanes.	2/23/2019 9:08 PM
8	My main concern is when my children bike, I want bike facilities designed with elementary aged kids in mind.	2/21/2019 8:11 PM
9	Need more options for locking up our bikes while shopping on Laurel	2/21/2019 6:04 PM
10	Knowing where closest (and most secure) bike racks are. Knowing what best bike route is when it is "twilight" - eg. sun/glare issues.	2/21/2019 3:24 PM
11	Drivers are impatient and don't wait or watch for kids walking to school before and after school, esp. at Brittan Ave. and Cordilleras.	2/12/2019 11:02 PM
12	Aggressive drivers that don't stop at STOP signs	2/6/2019 12:09 AM
13	Concern that the car doesn't see you or know the rules.	2/4/2019 6:56 PM
14	Safely biking with my kids	2/4/2019 5:18 PM
15	Safety is the number one concern.	2/4/2019 12:28 PM
16	Bike-safe ways of getting across El Camino, and Caltrain tracks.	2/2/2019 6:16 PM
17	Street lighting is poor	1/31/2019 12:50 AM
18	Lack of bike racks	1/30/2019 6:53 PM
19	Speeding vehicles, and those not respecting crosswalks	1/30/2019 5:04 PM
20	A convenient place to lock up my bike	1/26/2019 9:42 PM
21	Dealing with drivers who don't see me, distracted drivers, some roads not appropriate for bikes especially el Camino. I have to weave my way around to avoid it and get where I want to go. San Carlos also messed up old county road. Lack of through roads to easily ride along the flatter parts of the peninsula, especially connections through to Redwood City and Belmont. This prevents me from using my bike to get to other locations. I know ways but they are not convenient and decent for riding.	1/26/2019 2:48 PM
22	Not enough places to lock my bike	1/21/2019 2:59 PM
23	It's not aggressive drives but clueless drivers.	1/20/2019 4:07 PM
24	the drivers don't know the laws surrounding bikes	1/17/2019 12:43 PM
25	Old County road needs repair	1/17/2019 11:38 AM
26	Cars parked at junctions obscuring view of oncoming vehicles	1/16/2019 5:49 PM
27	having the kids with us and making sure they are okay/safe on their bikes.	1/16/2019 2:56 PM

San Carlos BPMP

28	traffic congestion in downtown San Carlos	1/16/2019 8:22 AM
29	none	1/15/2019 11:20 AM
30	Insufficient bicycle parking downtown	1/15/2019 9:50 AM
31	Left turn lanes can't detect my bike so I don't get green turn and have to go on red	1/14/2019 12:59 AM
32	No convenient places to lock a bike	1/13/2019 11:03 AM
33	Crossing Holly Street at Walnut	1/12/2019 8:15 PM
34	Weather	1/11/2019 6:36 PM
35	Lack of connectivity to through routes into Belmont, Redwood City, Woodside etc	1/11/2019 5:06 PM
36	Dealing with risky routes, i.e. no good safe convenient route to ride from Jack in the Box to REI. So I rarely ride my bike to REI, BestBuy, etc. Also no good plentiful bike racks downtown.	1/10/2019 6:36 PM
37	Convenient and decent bike racks	1/10/2019 4:40 PM
38	No bike share to allow for one way trips	1/10/2019 8:10 AM
39	Finding bike racks to lock our bikes to. Can use street signs but would rather not block pedestrians.	1/10/2019 12:36 AM
40	Traffic lights don't recognize bicycles	1/9/2019 10:04 PM
41	Steep hills from downtown to Crestview	1/9/2019 9:07 AM
42	some streets in the hills are very narrow and have cars parked making it very tight for cyclists (Shelford Ave between Dartmouth Ave and Wellington Dr is specially dangerous since the space is very tight and there is no visibility due to the street being curved)	1/8/2019 10:54 PM
43	getting back home up the hill i live on	12/7/2018 11:47 AM

Q5 Which concerns prevent you from biking around San Carlos? Please select all that apply.

Answered: 107 Skipped: 273



ANSWER CHOICES	RESPONSES	
Dealing with aggressive drivers	54.21%	58
Poor road conditions or lack of bicycle facilities (i.e. bike lanes, shared lane markings, multi- use trails)	54.21%	58
Knowing a safe route to get where you are going	30.84%	33
Other (please specify)	30.84%	33
Carrying the things you need with you	24.30%	26
The amount of time you have	24.30%	26
Transporting other people	17.76%	19
Having your bike stolen or vandalized	9.35%	10
Finding a bike that you can ride comfortably	9.35%	10

San Carlos BPMP

Putting your bike on a bus or a train	8.41%	9
Knowing the rules of the road for biking	1.87%	2
Total Respondents: 107		

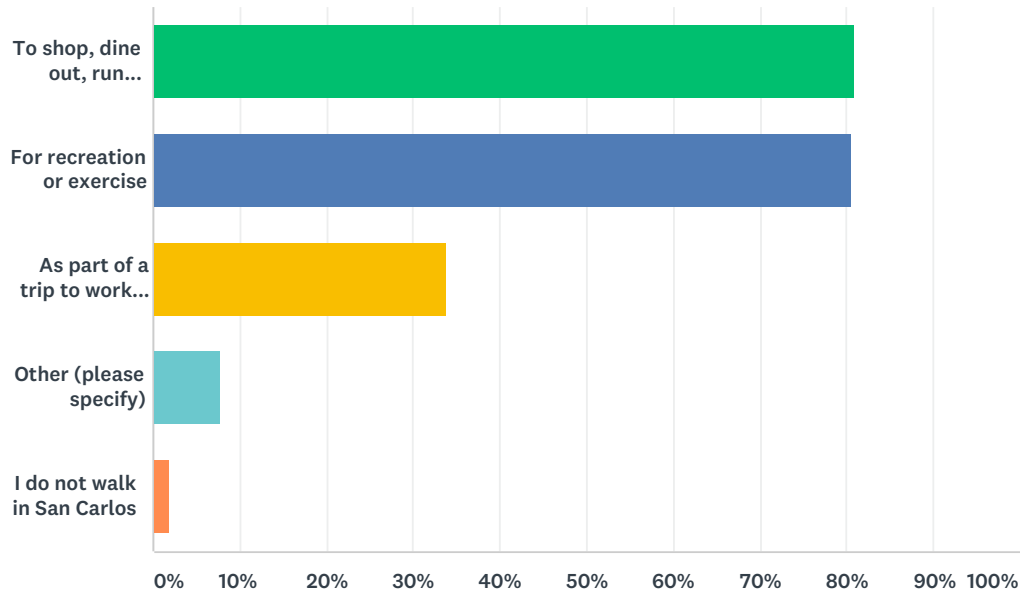
#	OTHER (PLEASE SPECIFY)	DATE
1	comfort on a bike	3/4/2019 5:46 PM
2	I don't like to bike, I like to walk	2/25/2019 6:53 PM
3	I'm just not comfortable enough on a bike to use as a mode of transportation	2/22/2019 9:43 PM
4	I just need to bring my bike to work.	2/20/2019 5:33 PM
5	Prefer to walk	2/19/2019 11:44 PM
6	Live in the hills, not feasible to get downtown and back via bike or walking	2/19/2019 6:58 PM
7	hills	2/19/2019 2:29 PM
8	I live on a steep road.	2/18/2019 7:32 PM
9	I can't get groceries and bike home up a hill in a timely manner. I also don't want to stress about being next to cars going 20-40mph	2/18/2019 8:20 AM
10	home in hills of San Carlos	2/14/2019 3:00 PM
11	san carlos affluenza distracted drivers	2/10/2019 2:20 PM
12	Live on hill, so hard to get back home	2/2/2019 1:34 PM
13	Would rather walk with my young kids	2/1/2019 10:57 PM
14	I'm a senior and choose not to bike	1/25/2019 1:34 PM
15	I live up in the hills, it is not practical.	1/24/2019 6:18 PM
16	it is just too dangerous with speeding and distracted drivers these days	1/18/2019 4:03 PM
17	We do not have enough room here for bike lanes to make a difference. Cars all over town are being forced to go single file down our streets. Bike lanes would just be for looks. The traffic here is also lawless. Bike lanes would be a dangerous joke.	1/18/2019 9:19 AM
18	Living on a hill and having to go down/up a busy street	1/17/2019 9:44 PM
19	I live in the hills along with many other residents. Biking up Brittan or Melendez or crestview isn't feasible.	1/16/2019 3:42 PM
20	Health issues	1/16/2019 2:53 PM
21	No concerns	1/12/2019 6:48 PM
22	Live in the hills	1/12/2019 5:47 PM
23	Distance and hills.	1/12/2019 11:38 AM
24	I do not feel comfortable riding a bike in an urban area. For me there are better ways of transportation than riding a bike.	1/11/2019 8:19 PM
25	Large hill up Brittan pas Alameda	1/11/2019 1:36 AM
26	Recreational biking at other places	1/10/2019 10:16 PM
27	my husband decided to ride his bike to caltrain for his commute. on the second day of his new habit, his bike was stolen (it was locked with a good lock.) you need to use bait bikes and catch people doing this.	1/10/2019 6:18 PM
28	Biking is too dangerous in ANY urban area.	1/10/2019 11:34 AM
29	Lots of very steep hills in our neighborhood	1/9/2019 7:36 PM
30	Take your bike to Canada Road and get your exercise there.	1/9/2019 2:17 PM
31	Don't own one. I walk	1/9/2019 12:52 PM

San Carlos BPMP

32	N/A since I don't bike	1/9/2019 12:06 PM
33	Having a protected route that is safe enough for biking with kids on bikes to downtown	1/8/2019 9:36 PM

Q6 What are the reasons you walk in San Carlos? Please select all that apply.

Answered: 372 Skipped: 8



ANSWER CHOICES	RESPONSES	
To shop, dine out, run errands, visit people, go to an event, or similar activity	80.91%	301
For recreation or exercise	80.65%	300
As part of a trip to work or school	33.87%	126
Other (please specify)	7.80%	29
I do not walk in San Carlos	1.88%	7
Total Respondents: 372		

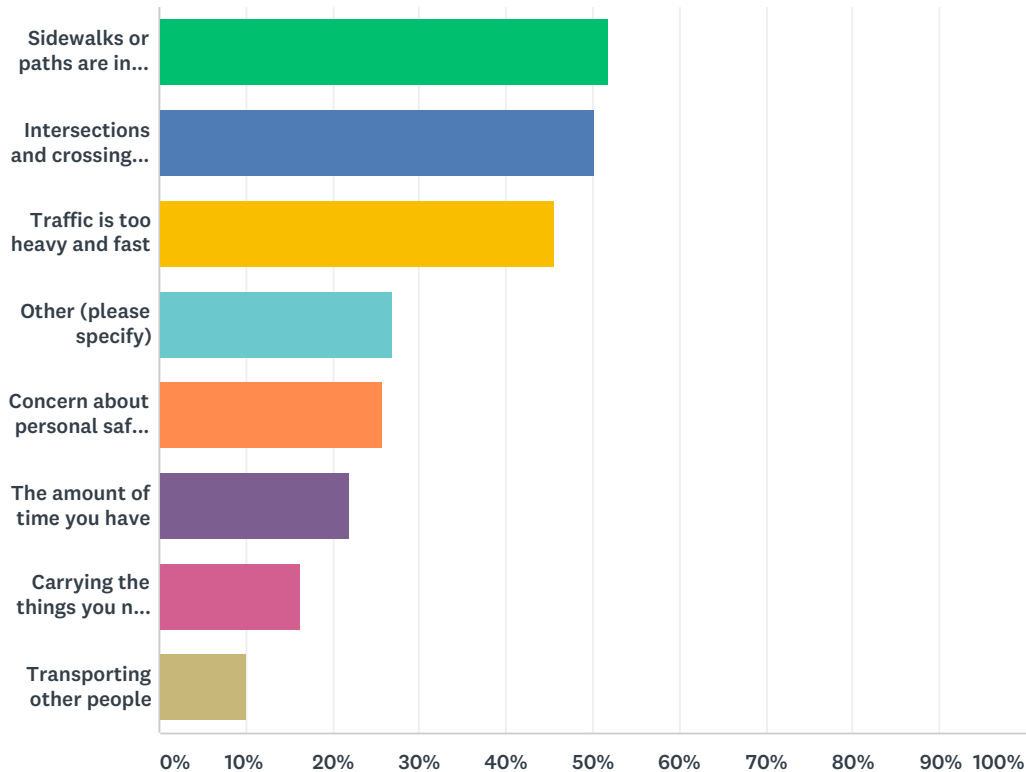
#	OTHER (PLEASE SPECIFY)	DATE
1	Walk the dog	3/11/2019 9:32 AM
2	Farmers market	3/5/2019 5:24 PM
3	jog for exercise	3/3/2019 9:42 PM
4	Walk our dogs	3/1/2019 2:56 PM
5	Because the car parking is frustratingly limited.	2/21/2019 3:24 PM
6	walk only after driving to get from hills to shopping and dining areas	2/19/2019 6:59 PM
7	Walking the dogs.	2/18/2019 8:21 AM
8	Walking my dog	2/17/2019 11:01 AM
9	Walk my dog	2/16/2019 1:16 PM
10	Walking the dog	2/12/2019 9:06 PM
11	canine companion excercise. walking = healthy	2/10/2019 2:21 PM
12	dog walking	2/6/2019 10:40 AM

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13	Pick up my kid from school	2/1/2019 10:58 PM
14	Walk the dog.... I appreciated burton park as a dog park but that was taken away, why? I love pulgas ridge and trail improvements in Eaton, can this be off leash?	1/26/2019 2:50 PM
15	I would LOVE to walk in SC but I live on upper Brittan Ave; there is no continuous sidewalk to the Alameda. It ends abruptly around Sunset Drive. Building a continuous sidewalk on Brittan would greatly improve the walkability of San Carlos and reduce traffic. People could walk to Burton Park, church, bus stops etc. Please consider this in your planning; the houses west of Alameda are forced to drive short distances due to poor/no sidewalks. It is short piece of sidewalk to install.	1/24/2019 6:24 PM
16	Especially have concerns about my nearby crosswalk at Brittan and Woodland. It needs a flashing beacon in order to make it a safe place to cross for our residents.	1/24/2019 11:38 AM
17	We walk our daughter to Arundel Elementary at least one or two days a week. Faster, easier, better.	1/16/2019 5:50 PM
18	mental health	1/16/2019 4:01 PM
19	to avoid the parking nightmare in the downtown district	1/15/2019 11:38 PM
20	To get to public transportation (Caltrain & Samtrans)	1/13/2019 11:01 AM
21	To walk the dogs!	1/12/2019 1:08 PM
22	Walk to shop to pick up car.	1/12/2019 11:38 AM
23	Safer than cycling	1/10/2019 11:35 AM
24	To get to the train station.	1/10/2019 11:26 AM
25	Kids walk to school as well	1/10/2019 12:37 AM
26	Walk to school	1/9/2019 10:46 PM
27	Also to avoid parking and very poor street engineering.	1/9/2019 7:45 PM
28	I only walk to Laurel to support other businesses and services, because driving and parking has become impossible.	1/9/2019 2:19 PM
29	to avoid parking hassles	1/9/2019 12:18 PM

Q7 Which concerns do you have when walking around San Carlos? Please select all that apply.

Answered: 349 Skipped: 31



ANSWER CHOICES	RESPONSES	
Sidewalks or paths are in poor condition	51.86%	181
Intersections and crossings are challenging	50.14%	175
Traffic is too heavy and fast	45.56%	159
Other (please specify)	26.93%	94
Concern about personal safety or security	25.79%	90
The amount of time you have	22.06%	77
Carrying the things you need with you	16.33%	57
Transporting other people	10.03%	35
Total Respondents: 349		

#	OTHER (PLEASE SPECIFY)	DATE
1	when pushing a stroller, the curbs are not cut	3/17/2019 9:36 AM
2	poor lighting	3/15/2019 6:48 AM
3	lack of sidewalks in some neighborhoods	3/14/2019 1:33 PM
4	Aggressive drivers, failure to yield	3/6/2019 8:17 PM

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5	too many cars	3/5/2019 5:24 PM
6	Crossing San Carlos Ave at Laurel can be dangerous and accidents have happened. Cars speed up on Laurel to make it through light on San Carlos Ave before it turns red.	3/3/2019 9:45 PM
7	Distracted drivers (many on cellphones) not paying attention to pedestrians	3/1/2019 2:57 PM
8	Cleanliness of Laurel Street	2/25/2019 7:13 PM
9	Sidewalks don't exist	2/25/2019 1:25 PM
10	Cars parked on sidewalks on Eaton Ave, no sidewalk between Eaton and Edgewood	2/24/2019 10:18 PM
11	No serious concerns	2/24/2019 11:42 AM
12	We live up in the hills, so while I walk recreationally in them, I always take a car to get down to 'the flats' to run errands and such.	2/22/2019 9:45 PM
13	When there is no tree canopy to create shade it becomes, especially in the warm months for kids, the elderly, or those walking strollers.	2/21/2019 8:13 PM
14	Drivers not paying attention at crosswalks	2/21/2019 5:00 PM
15	Sidewalks are not clean due to dog excrement.	2/21/2019 3:26 PM
16	There is a lack of shortcuts and many streets are winding, i would walk much more if we would leverage more right of ways to cut across blocks	2/21/2019 1:09 PM
17	No issues.	2/20/2019 5:33 PM
18	too dark at night and stair that go up hill are not lit	2/19/2019 2:30 PM
19	My street (Clifton Ave.) has a lot of blind turns, and no sidewalks. Since the advent of traffic apps like Waze, traffic on Clifton Ave. has increased significantly as drivers reroute to avoid congestion on San Carlos Avenue. This creates a very hazardous condition for those of us who walk our children to school!	2/18/2019 10:37 AM
20	Walking around San Carlos is fine; walking ON San Carlos Ave should be improved as you head away from downtown towards Alameda...sidewalks, maybe a railing to separate you from the freeway of cars that use that one street	2/17/2019 11:03 AM
21	Parking on sidewalks	2/16/2019 7:40 PM
22	Lack of stop signs at some busy intersections	2/16/2019 6:48 PM
23	None, safe town, cars yield to peds	2/16/2019 1:22 PM
24	No lights in some areas after dark	2/16/2019 1:12 PM
25	Walking with kids who are too small to be seen by cars is scary when drivers do a rolling stop and want you to move fast so they can skip a full stop at the stop sign.	2/12/2019 11:03 PM
26	affluenze/inattentive/distracted drivers	2/10/2019 2:22 PM
27	Aggressive drivers that don't stop at marked STOP signs or use caution at intersections.	2/6/2019 12:12 AM
28	Impatient drivers who even fail to wait at stop signs and crosswalks	2/5/2019 3:06 PM
29	sidewalks that are blocked by vehicles parked on them and landscaping that overhangs them. Current codes don't require them to be wide enough for two people to walk without one constantly having to be careful of driveway ramps.	2/4/2019 6:59 PM
30	It's extremely dark on most streets after 5pm in the winter. Even on well traveled paths (I walk home from CalTrain on San Carlos Ave and then down Cordilleras. Cordilleras is very dark and could benefit by some more street lights.	2/4/2019 10:37 AM
31	sidewalks don't exist	2/3/2019 5:15 PM
32	Lack of sidewalks and lights.	2/2/2019 7:16 PM
33	Live on hill, so need transportation to and from downtown areas	2/2/2019 1:36 PM
34	Street lighting is poor	1/31/2019 12:50 AM
35	9 year old walks (mostly) to Heather, but cannot cross Crestview safely near Melendy due to speeding cars at rush hour	1/30/2019 5:05 PM

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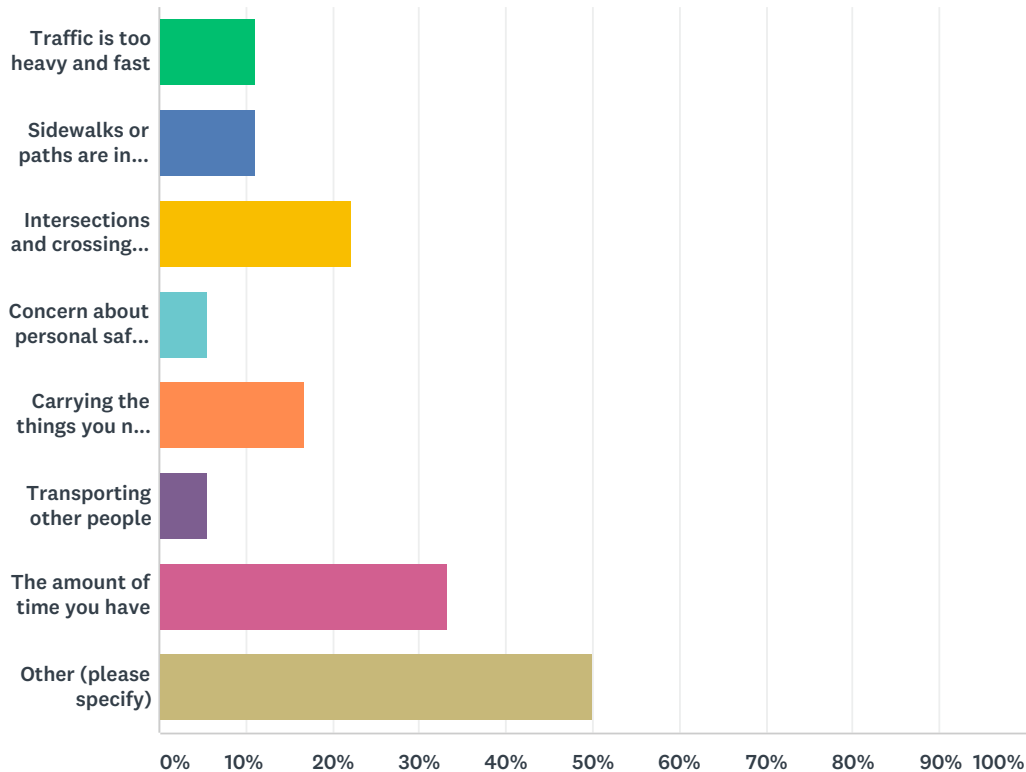
36	El Camino road is a big barrier, perceived and actual, to get across to businesses on the other side and very unpleasant to walk along.	1/26/2019 2:50 PM
37	no concerns	1/25/2019 7:32 PM
38	Greatest concern is the unsafe crosswalk at Brittan and Woodland	1/24/2019 11:38 AM
39	Specifically corner of Brittan and Woodland	1/23/2019 7:02 PM
40	Certain sidewalks (San Carlos Ave and Alameda between San Carlos and Brittan) are uneven and difficult for walking/running, especially in low light. Overall, there needs to be better lighting of the sidewalks for pedestrians to be able to walk through the neighborhoods. Also, too many corners do not have ramps so they are challenging for individuals with wheelchairs or strollers	1/22/2019 6:04 PM
41	Laurel street sidewalks are obstructed with tables and chairs in multiple locations limiting getting by and unpassable if in a wheelchair	1/21/2019 6:23 PM
42	strongly sloped sidewalks (e.g., Cordilleras between Emerald and Belmont)	1/19/2019 4:28 PM
43	Traffic safety is a concern - mostly due to bad/distracted driving	1/19/2019 2:51 PM
44	Drivers who don't know the rules at intersections and crosswalks. Location of bus stops (the one across from Walgreens is suicide trying to cross El Camino because cars DO NOT stop, even when you press the flashing pedestrian light)	1/18/2019 4:09 PM
45	Lack of ramps for strollers at cross streets	1/18/2019 8:24 AM
46	I walk my dog about 45 minutes every day and take various routes. Most of the time drivers are very courteous but occasionally see a driver that won't stop for pedestrians. I walk defensively.	1/17/2019 8:20 PM
47	Specifically the crosswalk at Woodland & Brittan Ave.	1/17/2019 2:25 PM
48	Specifically, Woodland/ Brittan crosswalk	1/17/2019 12:51 PM
49	Pedestrian walk lights at intersections are not long enough	1/17/2019 11:40 AM
50	People SPEED and there aren't enough sidewalks.	1/16/2019 5:51 PM
51	Cars parked on the sidewalk	1/16/2019 5:50 PM
52	Not enough enforcement of unsafe drivers in downtown area; certain areas need crosswalks (i.e. blind curve at intersection of Elm and Olive) and on Cordilleras between Belle and Eucalyptus where cars race down the hill as it curves	1/15/2019 10:58 PM
53	Cut through traffic is priority over safety needs of community	1/15/2019 8:33 PM
54	Some sidewalks are obstructed by trees and dog droppings	1/15/2019 11:24 AM
55	I have no problems walking in San Carlos.	1/14/2019 4:22 PM
56	Need more business south of arroyo in laurel	1/14/2019 12:59 AM
57	Congested sidewalks, too many autos	1/13/2019 5:13 PM
58	The walking routes into Arundel school often don't have sidewalks	1/13/2019 2:15 PM
59	Lack of sidewalks in some areas	1/13/2019 1:06 PM
60	It's a bit far for us, and walking Brittan Ave into town is not a pleasant experience	1/13/2019 11:05 AM
61	After dark, the lack of street lamps everywhere but on Laurel. Makes it easy to trip and also kind of creepy.	1/13/2019 11:04 AM
62	Aggressive drivers underlie ~all~ of my concerns. I wouldn't worry about the lack of sidewalks if I trusted drivers. I wouldn't worry about the intersections if I wasn't worried about the drivers. It's all about the agro drivers using San Carlos as their personal highway between 101 and 280 -- and I'm tired of it.	1/12/2019 10:10 PM
63	Near misses with cars on intersection of Holly and Walnut	1/12/2019 8:15 PM
64	No concerns	1/12/2019 6:48 PM
65	I have no concerns	1/12/2019 2:21 PM
66	Hills.	1/12/2019 11:38 AM
67	No sidewalks. Walking in gutter or street.	1/11/2019 8:53 PM

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68	Walking "downtown" is a challenge due to the number of restaurant tables that encroach the sidewalk.	1/11/2019 8:21 PM
69	lack of sidewalks in some areas	1/11/2019 8:11 PM
70	Lack of scenic/uncongested routes	1/11/2019 6:03 PM
71	No sidewalk on key routes like Lower Carmelita Ave	1/11/2019 5:07 PM
72	Hill up Brittan past Alameda	1/11/2019 1:36 AM
73	walking is mainly a problem when crossing large streets like San Carlos Ave.	1/10/2019 11:38 PM
74	lack of sidewalks our neighborhood	1/10/2019 11:17 PM
75	Sometimes, no sidewalks--for example, part Brittan Ave just west of Alameda, also Belle Ave/Elizabeth St/Knoll Dr/Valley Rd.	1/10/2019 5:25 PM
76	People don't pay attention to the driving laws. Why doesn't the city do something about the illegal u-turns on Laurel St.?	1/10/2019 11:36 AM
77	Bikes that do not stop for stop signs or red lights	1/10/2019 10:34 AM
78	The hills up San Carlos avenue has no lights at night	1/10/2019 9:52 AM
79	No concerns	1/10/2019 12:37 AM
80	Crossing El Camino and Old County is a pain and takes too long.	1/9/2019 10:16 PM
81	bringing my child in a stroller	1/9/2019 8:43 PM
82	Sidewalks are non-existing along many streets eg San Carlos Ave north side west of Sycamore.	1/9/2019 7:50 PM
83	Upper Eaton seems to get less attention on the sidewalk maintenance than other parts of San Carlos	1/9/2019 6:39 PM
84	Missing crosswalks	1/9/2019 6:22 PM
85	The sidewalks are dirtier than San Francisco. Open electrical box connections on city trees, dog feces and spilled milkshakes to name a few.	1/9/2019 2:20 PM
86	I had several close calls in the intersection of Laurel and San Carlos Ave. I find it too dangerous both for motorists and pedestrians.	1/9/2019 12:57 PM
87	have been nearly hit by vehicle thrice @ Laurel/San Carlos Ave.	1/9/2019 12:25 PM
88	When walking on San Carlos Ave. (eastbound), one has to be extremely careful not only of cars making rt. turns, but also cars traveling west on San Carlos Ave. making Left turns.	1/9/2019 12:08 PM
89	some streets lack sidewalks	1/8/2019 10:56 PM
90	Dog crap. Corner curbs that are not stroller friendly.	1/8/2019 9:36 PM
91	Lack of sidewalks. Literally walking in the road amongst the traffic.	1/8/2019 3:32 PM
92	No sidewalks on many busy streets!!	12/11/2018 5:24 PM
93	The Holly and El Camino Real intersection is particularly bad for pedestrians.	12/7/2018 1:09 PM
94	people don't stop at stop signs or drive into crosswalks before stopping	12/7/2018 11:49 AM

Q8 Which concerns prevent you from walking around San Carlos? Please select all that apply.

Answered: 18 Skipped: 362



ANSWER CHOICES	RESPONSES	
Traffic is too heavy and fast	11.11%	2
Sidewalks or paths are in poor condition	11.11%	2
Intersections and crossings are challenging	22.22%	4
Concern about personal safety or security	5.56%	1
Carrying the things you need with you	16.67%	3
Transporting other people	5.56%	1
The amount of time you have	33.33%	6
Other (please specify)	50.00%	9
Total Respondents: 18		

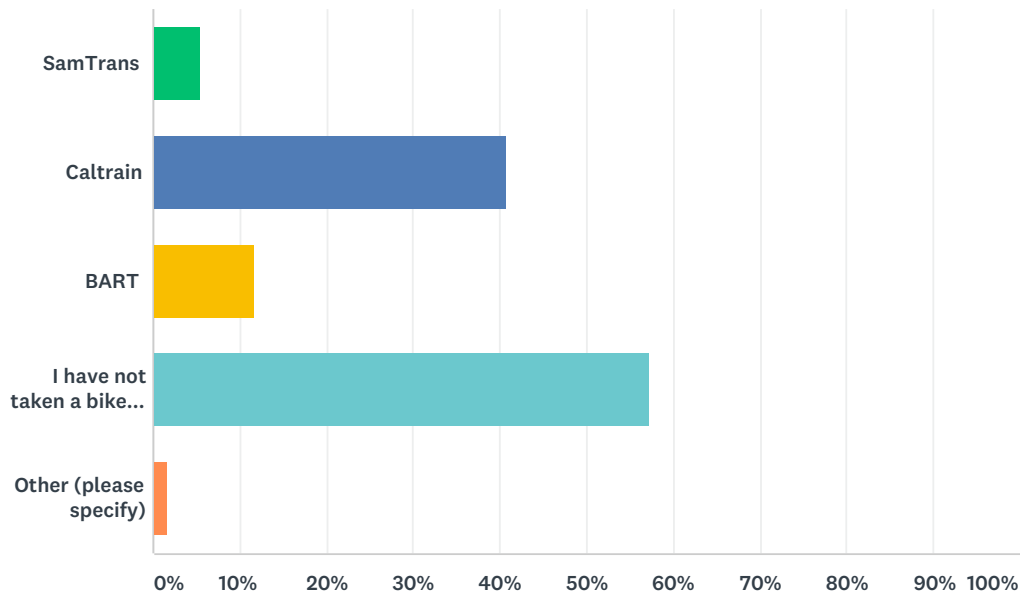
#	OTHER (PLEASE SPECIFY)	DATE
1	none in the downtown areas where i walk	2/19/2019 7:00 PM
2	I live at the top of San Carlos.	2/18/2019 7:33 PM
3	No concerns for myself but some for my kids	2/4/2019 1:35 PM
4	Continuous sidewalks on major thoroughfares such as upper Brittan Ave and San Carlos Ave would make tremendous difference to many people in San Carlos.	1/24/2019 6:26 PM

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5	The connection to my home street is fenced off. More details later.	1/19/2019 5:03 PM
6	I live in the hills, far from schools, banks and groceries. Please stop assuming everybody in San Carlos can walk.	1/16/2019 3:44 PM
7	None	1/14/2019 10:34 AM
8	None	1/13/2019 9:36 AM
9	I rarely walk in San Carlos.	1/8/2019 2:35 PM

Q9 Have you ever taken a bike on any of the following public transit systems? Please select all that apply.

Answered: 356 Skipped: 24

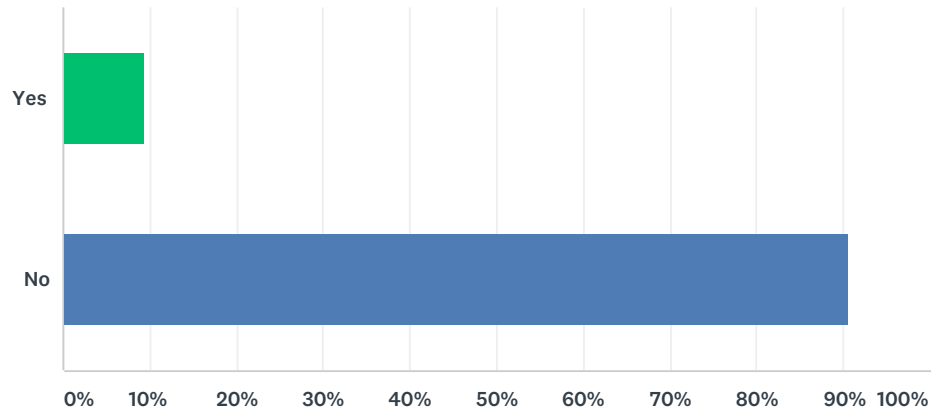


ANSWER CHOICES	RESPONSES	
SamTrans	5.34%	19
Caltrain	40.73%	145
BART	11.80%	42
I have not taken a bike on transit	57.30%	204
Other (please specify)	1.69%	6
Total Respondents: 356		

#	OTHER (PLEASE SPECIFY)	DATE
1	As part of my trips from San Carlos, I connect to Stanford's Margueritte Shuttle, VTA (from MV Caltrain) and SF Muni (from 4th and King)	2/16/2019 9:09 PM
2	I am disabled, I love RediWheels	2/10/2019 2:23 PM
3	Work bus	1/21/2019 3:01 PM
4	Caltrain system is terrible - tried a few times, never again	1/19/2019 2:51 PM
5	I wouldn't be able to lift my bike up without help	1/17/2019 11:42 AM
6	Dumbarton Express	1/8/2019 2:36 PM

Q10 Do you use SamTrans buses to get around San Carlos?

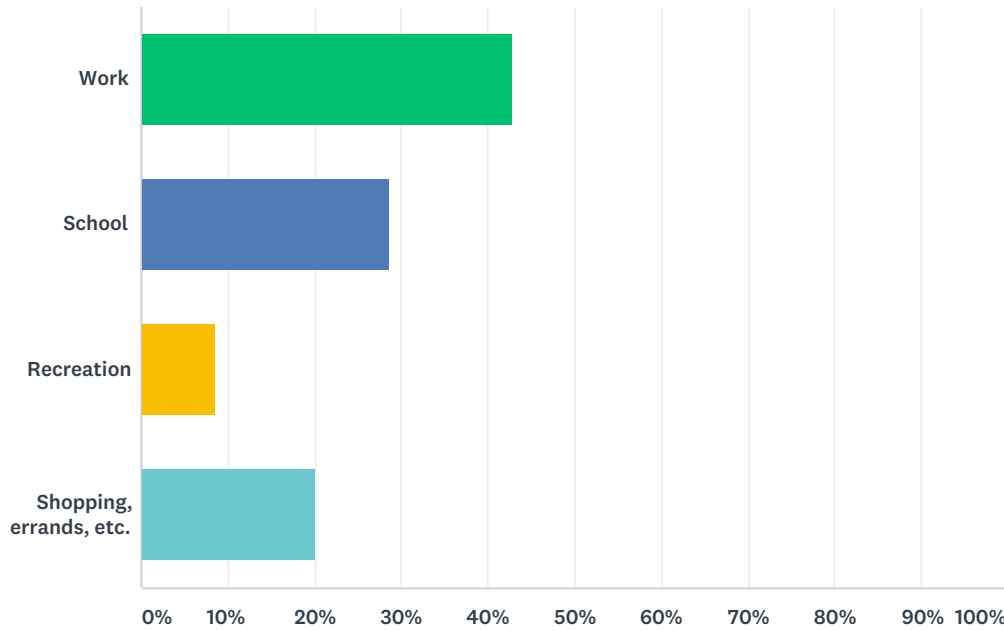
Answered: 368 Skipped: 12



ANSWER CHOICES		RESPONSES	
Yes		9.51%	35
No		90.49%	333
TOTAL			368

Q11 What are the reasons you use SamTrans buses? Select all that apply.

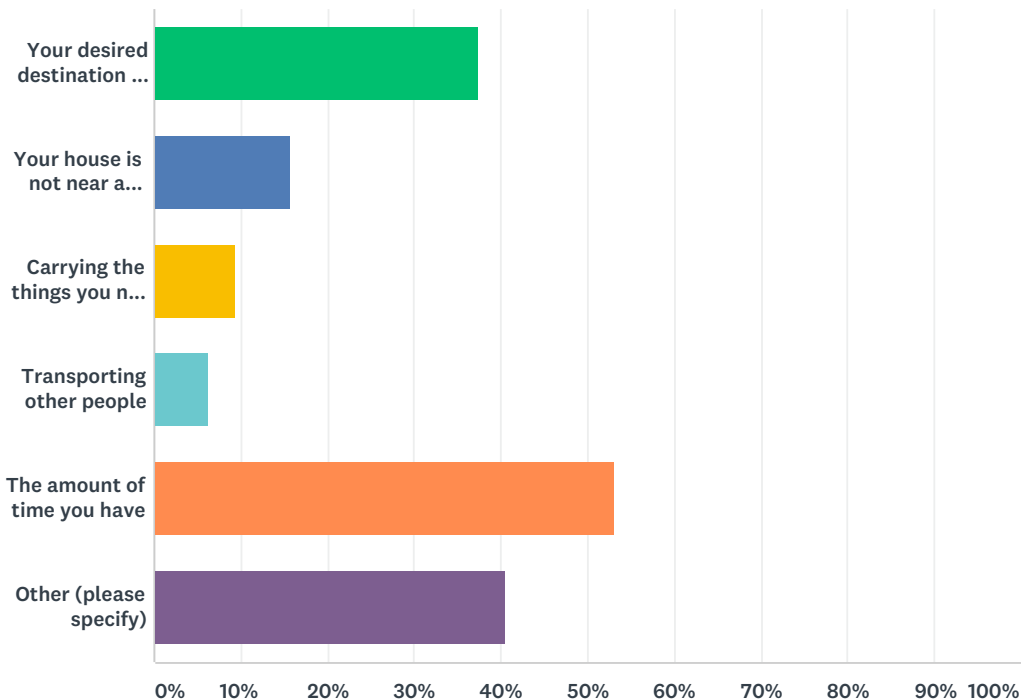
Answered: 35 Skipped: 345



ANSWER CHOICES	RESPONSES	
Work	42.86%	15
School	28.57%	10
Recreation	8.57%	3
Shopping, errands, etc.	20.00%	7
TOTAL		35

Q12 Which of the following are concerns as you think about whether you should SamTrans buses to get around San Carlos?

Answered: 32 Skipped: 348



ANSWER CHOICES	RESPONSES	
Your desired destination is not near a transit stop	37.50%	12
Your house is not near a transit stop	15.63%	5
Carrying the things you need with you	9.38%	3
Transporting other people	6.25%	2
The amount of time you have	53.13%	17
Other (please specify)	40.63%	13
Total Respondents: 32		

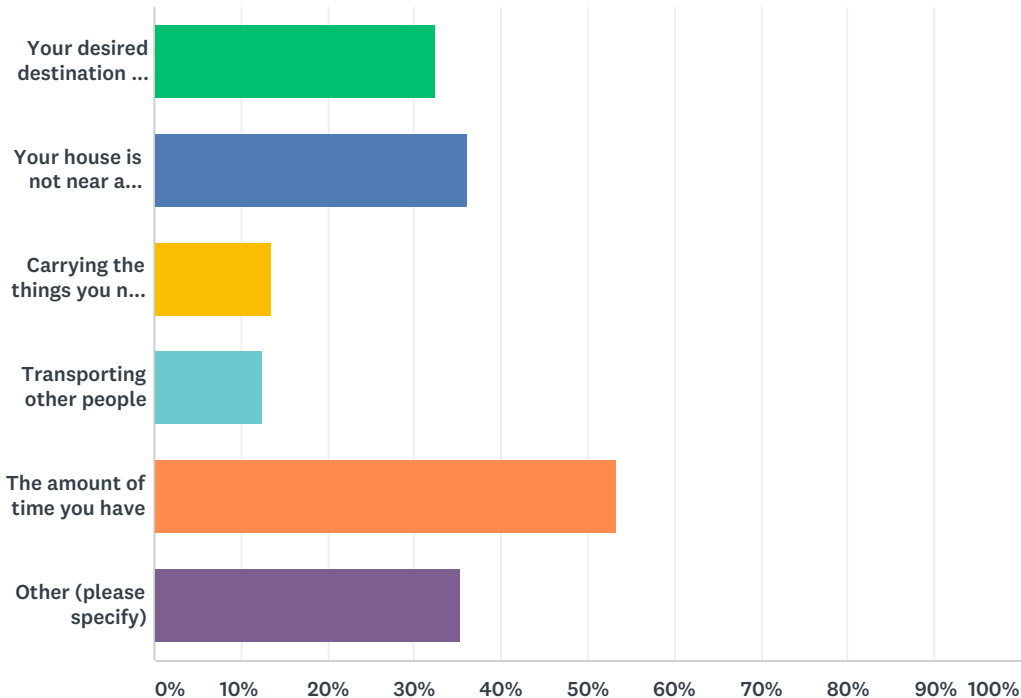
#	OTHER (PLEASE SPECIFY)	DATE
1	there aren't any buses to pick the kids up from school (they go to charter and the times don't work)	3/4/2019 1:49 PM
2	frequency of busses	2/24/2019 7:30 PM
3	Samtrans schedules and routes	2/17/2019 5:26 PM
4	The filth and stench of the buses	2/16/2019 7:41 PM
5	Schedules and frequency of busses are not ideal.	2/16/2019 12:20 PM
6	Schedule and safety of bus stops on San Carlos Ave.	2/9/2019 9:59 AM
7	schedule and frequency of buses	2/3/2019 5:15 PM
8	Not sure if you mean ONLY w/in SC but: irregular slow service, not synchronised w/ Caltrain schedule	1/31/2019 12:52 AM

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9	The amount of time it takes and how often the buses run. Also #9 is not multi-select and should be	1/21/2019 8:49 PM
10	SamTrans doesn't have Real Time arrival information, location of bus stops is sometimes extremely dangerous. Crossing El Camino where there isn't a traffic light is suicide because cars just don't stop. They don't care.	1/18/2019 4:11 PM
11	Weather conditions - there's no cover at bus stops	1/12/2019 8:17 PM
12	Not enough frequent times no 295 on weekends	1/10/2019 9:54 AM
13	timetables	1/10/2019 8:47 AM

Q13 Which concerns prevent you from using SamTrans buses to get around San Carlos?

Answered: 309 Skipped: 71



ANSWER CHOICES	RESPONSES	
Your desired destination is not near a transit stop	32.36%	100
Your house is not near a transit stop	36.25%	112
Carrying the things you need with you	13.59%	42
Transporting other people	12.62%	39
The amount of time you have	53.40%	165
Other (please specify)	35.28%	109
Total Respondents: 309		

#	OTHER (PLEASE SPECIFY)	DATE
1	bus schedule, unpredictability=need lots extra time to be on time to deal with bus schedule	3/6/2019 10:55 PM
2	unreliability of Sam Trans--buses may be late or full at peak times	3/5/2019 6:16 PM
3	I drive or walk in San Carlos.	3/4/2019 8:07 PM
4	The bus runs infrequently, isn't near me and it is more convenient to drive	3/3/2019 9:46 PM
5	No interest.	3/2/2019 12:42 PM
6	It's easier to walk than wait for a bus	3/1/2019 2:58 PM
7	There is not a need as I have a car and kids	2/27/2019 7:00 AM
8	bus schedule	2/25/2019 6:12 PM

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9	no buses run to big canyon park on weekends	2/24/2019 4:00 PM
10	Other options are more convenient for short distances.	2/23/2019 9:09 PM
11	Frequency of samtrans service.	2/21/2019 8:14 PM
12	The bus stop is 1/4-1/2 mile away. I live up Wellington, might as well walk downtown instead of taking a bus.	2/18/2019 8:22 AM
13	schedule of the bus too infrequent	2/17/2019 9:11 PM
14	I can walk or drive just as easily as deal with a bus	2/17/2019 8:33 PM
15	its easier to walk directly there and back	2/16/2019 8:43 PM
16	San Carlos is too small for buses. I take buses to other cities.	2/16/2019 2:46 PM
17	I live close to downtown, I have all I need to get around	2/16/2019 1:24 PM
18	I prefer to bike or walk	2/15/2019 8:48 PM
19	not safe to travel on bus at night as a woman	2/14/2019 3:01 PM
20	I'd prefer to walk or ride my bike	2/14/2019 10:45 AM
21	driving is easier	2/11/2019 3:31 PM
22	Don't know where or how it runs	2/11/2019 2:48 PM
23	I am disabled, rediWheels or Lyft saves the day	2/10/2019 2:25 PM
24	Prefer to walk or scooter	2/10/2019 12:29 PM
25	i'm in walkable/cyclable distance from downtown	2/6/2019 10:44 AM
26	If i use a vehicle, it will be my own car	2/5/2019 3:07 PM
27	I'm sure that they are better than I think. Some awareness publicity would help. I honestly haven't looked to see what routes are available and the timing on them. Timing is a big issue. If you miss the bus how long will you have to wait for the next one?	2/4/2019 7:01 PM
28	Not sure how long I'll wait for a bus.	2/4/2019 10:37 AM
29	Desired San Carlos destination is usually walkable	2/1/2019 11:00 PM
30	No need	1/28/2019 6:17 PM
31	I've never been in a position in which I've had to get used to following a transit schedule instead of my own.	1/26/2019 9:44 PM
32	Seems slow and very unappealing, easier just to drive	1/26/2019 2:52 PM
33	There is not a safe sidewalk from the Sam Trans drop off to my house.	1/24/2019 6:27 PM
34	Would prefer to drive but might be a good resource for my children when they are old enough to ride bus on their own	1/24/2019 11:39 AM
35	I have a car	1/23/2019 9:09 PM
36	Concern about carrying bike on the bus	1/23/2019 9:02 PM
37	Dont need a bus-live and work in SC	1/23/2019 7:03 PM
38	Prefer to bike/walk	1/23/2019 5:14 PM
39	I prefer to ride my bike	1/23/2019 1:27 PM
40	The train does not pick up or drop off at the stops I want at the times I want	1/22/2019 9:11 PM
41	Routes are not well publicized and do not seem to be frequent	1/22/2019 6:05 PM
42	Unfamiliar with directions and destinations	1/22/2019 8:58 AM
43	quicker to walk or bike	1/21/2019 11:56 PM
44	Very expensive for one stop. 4 dollars is more than a tank of gas and could make 10 times that trip.	1/21/2019 3:12 PM
45	schedules don't match up	1/20/2019 4:08 PM

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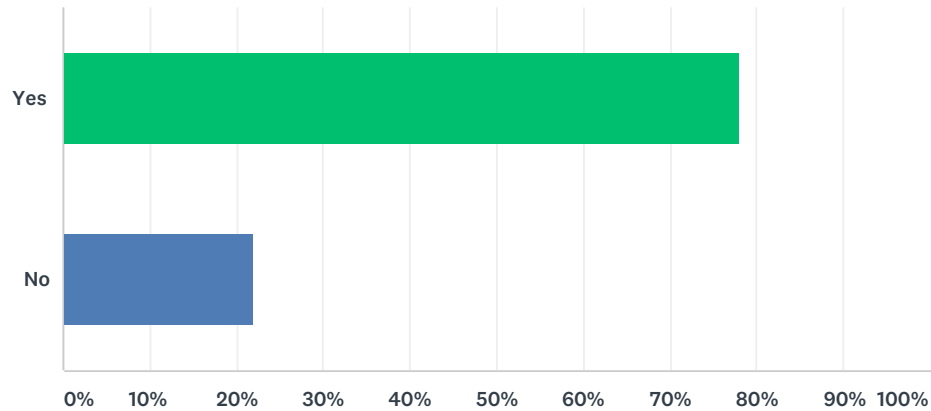
46	In general public transportation is a terrible experience	1/19/2019 2:53 PM
47	I would love to see the local shuttle to downtown reinstated	1/18/2019 4:05 PM
48	I don't know anything about it.	1/18/2019 9:35 AM
49	Added cost.	1/18/2019 8:26 AM
50	Does not operate when I need to travel	1/17/2019 7:32 PM
51	Would rather walk, drive, or bike (more personal control)	1/17/2019 3:08 PM
52	Don't know where stop is located	1/17/2019 2:13 AM
53	convenience, go multiple places faster via car	1/16/2019 10:54 PM
54	Safe covered stops for young children to go to school	1/16/2019 10:51 PM
55	dirty, very odd people	1/16/2019 5:59 PM
56	I bike or drive	1/16/2019 5:50 PM
57	Safety	1/16/2019 4:31 PM
58	Not familiar with SamTrans routes, or other basic info + seems inconvenient	1/16/2019 4:12 PM
59	schedule not frequent enough	1/16/2019 4:03 PM
60	Dont need to	1/16/2019 2:41 PM
61	not enough buses	1/16/2019 9:51 AM
62	I prefer to drive	1/15/2019 11:02 PM
63	Busses are a pain in the neck to get where we need to be	1/15/2019 6:09 PM
64	Biking or walking is faster	1/15/2019 3:04 PM
65	need exercise for health reasons	1/15/2019 11:28 AM
66	Quicker to bike or walk	1/15/2019 11:00 AM
67	Infrequent service, lack of transit hub	1/15/2019 9:55 AM
68	Strange people on the buses	1/14/2019 10:35 AM
69	I don't know where the routes go, what the price is, or when they come.	1/13/2019 2:28 PM
70	Would rather walk	1/13/2019 10:52 AM
71	Public transit other than CalTrain = yuk	1/13/2019 10:44 AM
72	I don't feel the need to use SamTrans	1/13/2019 10:41 AM
73	I have never looked into it	1/13/2019 10:36 AM
74	Lots of weird people on the bus	1/13/2019 9:37 AM
75	I like to walk and bike.	1/12/2019 10:10 PM
76	I can walk or bike to most of my destinations	1/12/2019 9:05 PM
77	I don't like buses and have no need to use them	1/12/2019 8:06 PM
78	It is more convenient to drive	1/12/2019 8:05 PM
79	No concerns	1/12/2019 6:48 PM
80	Would prefer to walk or bicycle as it does not tie me to a schedule.	1/12/2019 1:10 PM
81	Transit runs too infrequently.	1/12/2019 11:39 AM
82	San Carlos is too small for buses. I take buses to other cities.	1/12/2019 1:31 AM
83	Don't want to pay fares, I just want to get around for free.	1/11/2019 7:22 PM
84	Cost and lack of convenient routes and availability.	1/11/2019 5:08 PM
85	The busses are un reliable and don't keep to a schedule	1/11/2019 11:24 AM
86	where does it go?	1/11/2019 9:50 AM

San Carlos BPMP

87	Usually walk	1/10/2019 10:18 PM
88	I drive or walk.	1/10/2019 8:57 PM
89	Buses have bad image. No one I know rides the buses.	1/10/2019 6:40 PM
90	I can walk most of the place I need to go in SCarlos; I live near an El Camino SanTrans stop for other destinations.	1/10/2019 5:33 PM
91	Within San Carlos, a bike is quicker and more convenient	1/10/2019 4:42 PM
92	Buses are not convenient way to run errands and takes too long.	1/10/2019 3:19 PM
93	Quite honestly, it is just not convenient.	1/10/2019 11:37 AM
94	Frequency of service	1/10/2019 8:11 AM
95	Never crossed my mind. My perception is driving, biking, and possibly even walking would be faster than waiting for a bus.	1/10/2019 12:38 AM
96	I can walk to most places in San Carlos	1/9/2019 10:18 PM
97	Buses don't go around San Carlos. They go up and down El Camino. Would rather walk anyway.	1/9/2019 7:58 PM
98	Don't know the routes	1/9/2019 7:51 PM
99	Service not frequent enough to get trips in.	1/9/2019 7:22 PM
100	bus schedule is not spontaneous!	1/9/2019 6:52 PM
101	I would rather ride, walk or drive. I feel the bus system is inefficient and wasteful for the amount of riders	1/9/2019 6:41 PM
102	My business depends on the use of an automobile. Riding a bike is not even a valid consideration.	1/9/2019 2:21 PM
103	just not as convenient as own vehicle	1/9/2019 12:20 PM
104	I would rather walk	1/9/2019 12:08 PM
105	Bus frequency is lacking	1/9/2019 9:08 AM
106	I don't use Samtrans to get around San Carlos.	1/8/2019 2:37 PM
107	Timely schedules	1/8/2019 1:49 PM
108	cost for a very short bus ride	12/11/2018 5:24 PM
109	uneducated on routes and stops and times	12/7/2018 11:50 AM

Q14 Do you use Caltrain to travel to or from San Carlos?

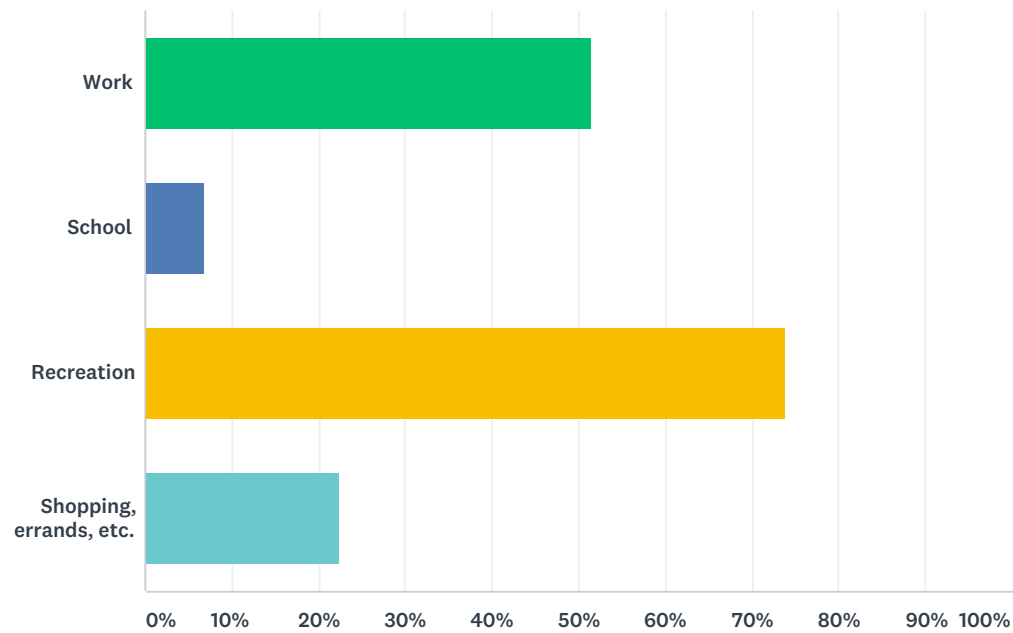
Answered: 370 Skipped: 10



ANSWER CHOICES		RESPONSES	
Yes		78.11%	289
No		21.89%	81
TOTAL			370

Q15 What are the reasons you use Caltrain? Select all that apply.

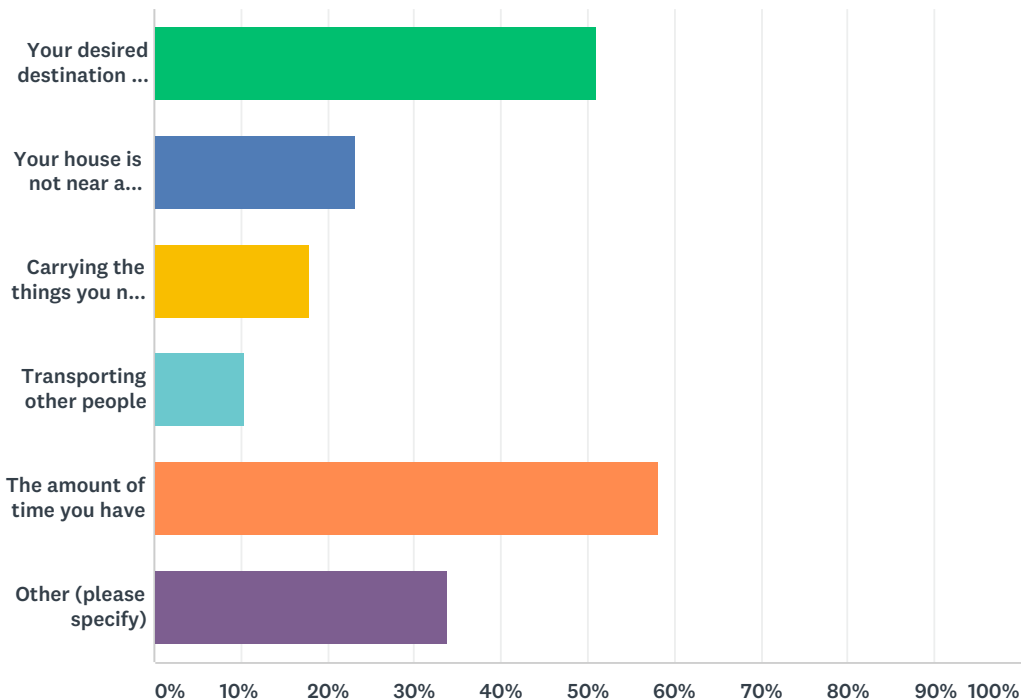
Answered: 287 Skipped: 93



ANSWER CHOICES		RESPONSES	
Work		51.57%	148
School		6.97%	20
Recreation		73.87%	212
Shopping, errands, etc.		22.30%	64
Total Respondents: 287			

Q16 Which of the following are concerns as you think about whether you should use Caltrain?

Answered: 268 Skipped: 112



ANSWER CHOICES	RESPONSES	
Your desired destination is not near a Caltrain station	51.12%	137
Your house is not near a Caltrain station	23.13%	62
Carrying the things you need with you	17.91%	48
Transporting other people	10.45%	28
The amount of time you have	58.21%	156
Other (please specify)	33.96%	91
Total Respondents: 268		

#	OTHER (PLEASE SPECIFY)	DATE
1	Weekend schedule outside of baseball season	3/14/2019 2:48 PM
2	Whether there is room on the Caltrain for my bike.	3/11/2019 9:34 AM
3	Caltrain fare structure (i.e., zones) creates large \$\$\$ (x2 for roundtrips) changes if cross zone boundaries (e.g., Millbrae vs San Bruno).	3/6/2019 10:57 PM
4	My girls feel uncomfortable on Caltrain--there are often sketchy characters riding on it.	3/5/2019 6:17 PM
5	Caltrain would be ideal except it is a huge pain to take my bike on the train (getting bumped on the way home). If I had a safe place to leave my bike at the San Carlos train station (eg, a bike locker), I could commute by bike/train every day instead of car.	3/4/2019 11:42 PM
6	Available parking at the station.	3/2/2019 12:43 PM
7	timing, and stop is not near downtown sf	2/27/2019 9:26 PM

San Carlos BPMP

8	It's easy	2/27/2019 7:01 AM
9	Cleanliness of train	2/25/2019 7:14 PM
10	schedule of caltrains	2/25/2019 2:22 PM
11	parking is expensive + train ticket = doesn't make financial sense	2/25/2019 2:03 PM
12	Frequency of departures/arrivals	2/24/2019 11:43 AM
13	frequency of service during non commute hours	2/21/2019 8:15 PM
14	frequency of trains	2/21/2019 6:07 PM
15	Lack of timely schedule options. Caltrain schedule is too infrequent during non rush hours.	2/21/2019 3:27 PM
16	Parking at the train station is too far or difficult to find	2/19/2019 11:48 PM
17	can not get a seat on the train, can not take me elderly father on train because too crowded and no where for him to sit	2/19/2019 2:31 PM
18	When the weather is rainy or windy, the 20-minute walk to CalTrain is a lot less appealing than driving...	2/18/2019 10:39 AM
19	Cost	2/17/2019 5:27 PM
20	Parking at Caltrain	2/17/2019 1:58 PM
21	getting on and off the train is difficult (knee problems)	2/16/2019 8:44 PM
22	Parking at train station	2/16/2019 6:48 PM
23	parking at Caltrain station, and long walk to station with the new post-construction configuration	2/16/2019 1:18 PM
24	I go to the Hillsdale station if I need to travel north as there are more frequent trains.	2/16/2019 12:22 PM
25	I don't have any concerns, Caltrain is very good	2/14/2019 10:46 AM
26	Can't bring the dog on Caltrain	2/12/2019 9:09 PM
27	If I can get a seat on the train. Commuter trains are packed by the time they get to San Carlos or Redwood City.	2/11/2019 6:42 PM
28	you [REDACTED] the station to build [REDACTED] to make san carlos suck	2/10/2019 2:27 PM
29	Cost with 4 people driving is less expensive	2/10/2019 12:31 PM
30	Cost of a round trip ticket vs. driving and location of stops	2/6/2019 12:16 AM
31	Too expensive for my family of 5 to take the Caltrain.	2/4/2019 11:58 PM
32	The schedule is horrible when you consider frequency and how crowded it can be to try and take a bike on it. Of course timed linking with SamTrans would help. Hopefully electrifying will help this a lot.	2/4/2019 7:04 PM
33	I used to bike commute daily for years but now I have 2 kids	2/4/2019 5:21 PM
34	The safety of the bike route while in San Carlos	2/4/2019 12:30 PM
35	caltrain does not stop in San Carlos frequently enough, especially in evening rush hour	2/3/2019 5:16 PM
36	Re time: infrequency of services & no bus exchange often makes caltrain/+bus a poor alternative to Lyft etc for local routes, much slower than driving for long routes. Can be quicker to walk 45 minutes than use public transit!	1/31/2019 12:54 AM
37	Making sure the schedule works for my needs	1/30/2019 9:14 PM
38	All the constructions	1/28/2019 6:17 PM
39	The parking fee since I usually go to SF and spend the night at a friend's.	1/26/2019 9:45 PM
40	I used cal train to go to work before but now have a job closer to home. The issues I had were that my place of employment was 3 miles from the train station. Dressing for work including shoes, makeup, hair dryer, showers, time and rainy weather, breakfast, were the biggest barriers, plus decent bike storage at work. It takes some investment and planning. It was also difficult to stay late at work because of the dark and waiting for a train in the cold.	1/26/2019 2:57 PM

San Carlos BPMP

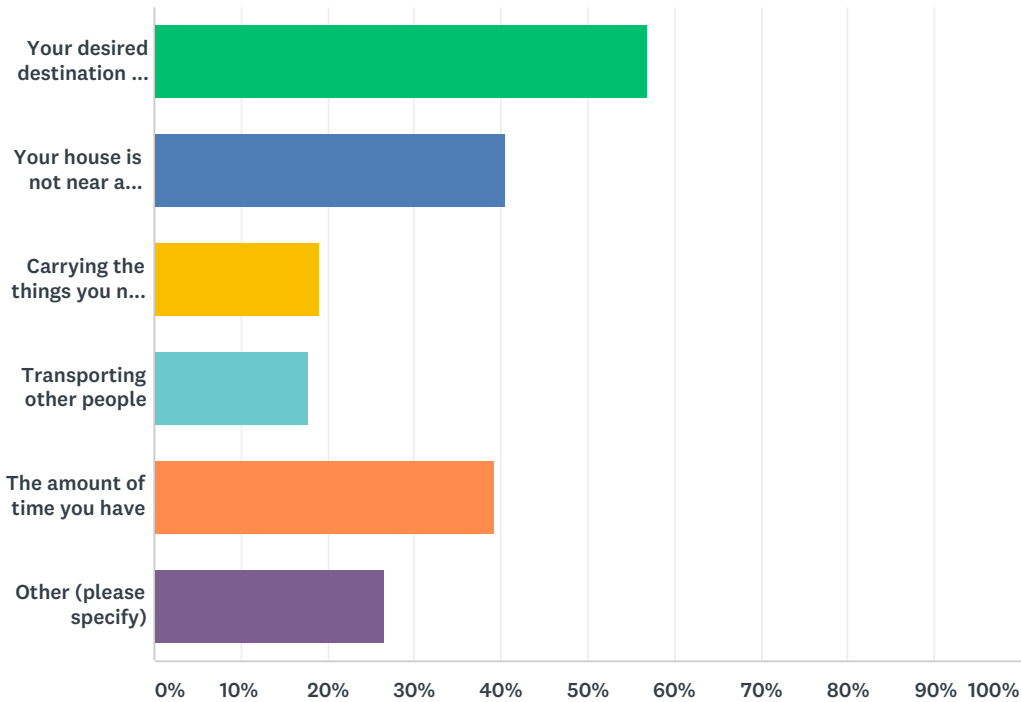
41	too expensive	1/25/2019 7:34 PM
42	Needs WiFi	1/23/2019 9:09 PM
43	Would ride more if Caltrain schedule was more frequent	1/23/2019 9:02 PM
44	Timing of when the trains come	1/22/2019 9:12 PM
45	The train doesn't run often enough - every 90 minutes on weekends and during the week not often enough. Also crowded.	1/21/2019 8:50 PM
46	Also fairly pricey.	1/21/2019 3:12 PM
47	cost	1/20/2019 3:48 PM
48	Costly and convince - Caltrain is last resort, worst case driving is always (30 mins) faster then Caltrain/VTA trip to work	1/19/2019 2:55 PM
49	Caltrain does not run frequently enough, especially on the weekends. They should also make the parking lots free on the weekends.	1/18/2019 4:11 PM
50	Wish San Carlos had a bullet train	1/18/2019 12:39 AM
51	Work off schedule and trains do not run late or overnight	1/17/2019 7:33 PM
52	Parking for the train	1/17/2019 2:26 PM
53	Can't lift my bike up onto the train	1/17/2019 11:44 AM
54	San Carlos parking near Caltrain and congestion with the building going on. Easier to go to Belmont station.	1/16/2019 10:56 PM
55	Safety, they no longer have sheriff deputies riding on the trains.	1/16/2019 4:33 PM
56	Too expensive	1/16/2019 4:25 PM
57	Weekend times have been limited	1/16/2019 4:12 PM
58	getting to the station easily	1/16/2019 4:03 PM
59	The parking in San Carlos Caltrain is now AWFUL. The parking is way too far from the platforms...especially on a rainy day and when you are constantly rushing with kids.	1/16/2019 2:58 PM
60	The stupid construction they built around the train station	1/16/2019 2:42 PM
61	Limited schedule during non-peak hours	1/15/2019 11:00 PM
62	Slow and costly	1/15/2019 8:34 PM
63	None of the above, I love Caltrain.	1/15/2019 2:43 PM
64	Cost is high	1/15/2019 11:02 AM
65	Caltrain fares	1/15/2019 9:55 AM
66	I have no problems taking CalTrain.	1/14/2019 4:23 PM
67	Day use storage lockers at either end of commute	1/13/2019 5:14 PM
68	San Carlos has limited express trains to San Carlos	1/13/2019 2:16 PM
69	Cost to park; could ride a bike to station, but bike doesn't feel secure, too few lockers	1/13/2019 11:07 AM
70	combining a bicycle and CalTrain makes it so easy to get everywhere I want to go.	1/12/2019 9:06 PM
71	Frequency/timing of the services	1/12/2019 8:17 PM
72	Caltrain takes a lot of effort and planning	1/12/2019 8:07 PM
73	No concerns	1/12/2019 6:49 PM
74	Few trains stop at both San Carlos and Mountain View during my normal commute hours.	1/12/2019 1:11 PM
75	Will there be room on the train	1/12/2019 10:08 AM
76	Parking near the train and the cost of parking.	1/11/2019 8:23 PM
77	I like to get around for free	1/11/2019 7:22 PM

San Carlos BPMP

78	Safe routes to Caltrain. The postman's path at the end of Carmelita is shut down so I have to walk/bike on the heavily traffic'ed San Carlos Avenue which doesn't feel safe	1/11/2019 6:05 PM
79	Parking, cost, availability of bike parking and bike capacity	1/11/2019 5:09 PM
80	Caltrain only stops at San Carlos once an hour during the day and we need it more	1/11/2019 11:26 AM
81	Would like more express+ trains to/from the SC station	1/11/2019 9:51 AM
82	Caltrain schedule	1/10/2019 8:57 PM
83	Parking costs \$5 a day, and when I biked to Caltrain, my bike was stolen	1/10/2019 8:52 PM
84	Excessive time between trains. They don't run late to SF. Risk of not being able to squeeze my bike in and having to wait a long time before next train.	1/10/2019 6:42 PM
85	I live a mile+ from a CalTrain station (SC or RWC); CalTrain does not adequately serve SFO or SJC.	1/10/2019 5:34 PM
86	The weekend schedule usually makes driving the better choice.	1/10/2019 4:43 PM
87	No bike share at the station	1/10/2019 8:12 AM
88	Making sure there is a seat for me and room for my bike	1/9/2019 11:09 PM
89	cost	1/9/2019 7:21 PM
90	Cost	1/9/2019 7:16 PM
91	only a few trains stop in San Carlos during rush hour	1/8/2019 10:58 PM

Q17 Which concerns prevent you from using Caltrain?

Answered: 79 Skipped: 301



ANSWER CHOICES	RESPONSES	
Your desired destination is not near a Caltrain station	56.96%	45
Your house is not near a Caltrain station	40.51%	32
Carrying the things you need with you	18.99%	15
Transporting other people	17.72%	14
The amount of time you have	39.24%	31
Other (please specify)	26.58%	21
Total Respondents: 79		

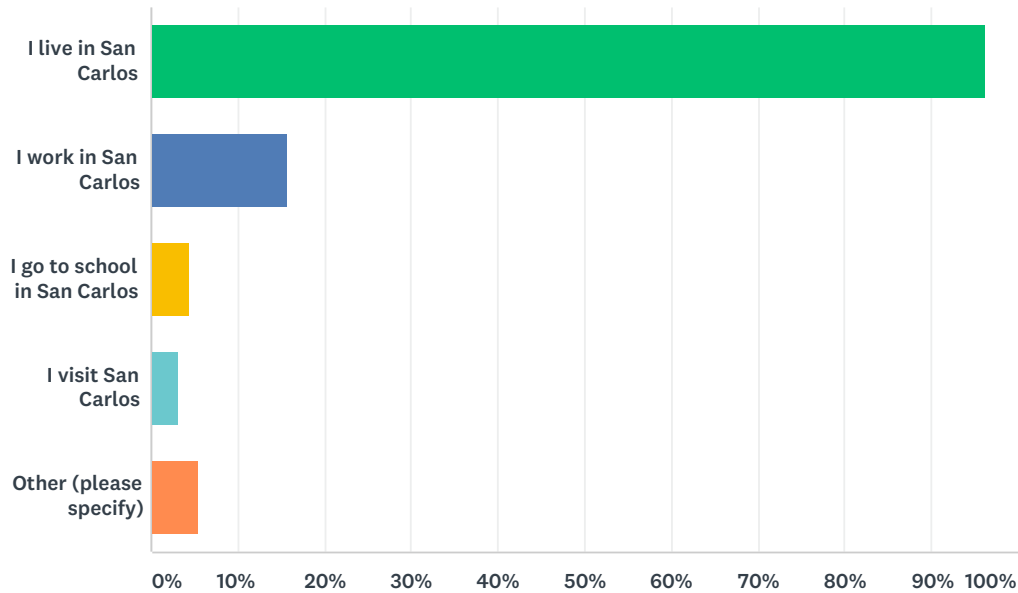
#	OTHER (PLEASE SPECIFY)	DATE
1	no need	3/15/2019 6:49 AM
2	Used to use it a lot, but don't need it now due to work location.	3/1/2019 5:35 PM
3	easier to take the car	2/25/2019 6:13 PM
4	Other options are more convenient.	2/23/2019 9:09 PM
5	I have used CalTrain to go to San Francisco	2/20/2019 11:36 AM
6	Handicapped access to locations beyond train stations. no good way to get to final destination	2/19/2019 7:03 PM
7	My work is not near Caltrain.	2/18/2019 8:23 AM
8	Caltrain services a downtown SF only employment, I work far away from downtown SF, it would take me 3 hours 1 way to get to where I need to go...a mule would be faster	2/17/2019 11:05 AM
9	train are very infrequent.	2/14/2019 3:02 PM

San Carlos BPMP

10	caltrain price VS car/bike, slowness + delays, frequency in the weekend	2/6/2019 10:45 AM
11	Parking, and my job is not near Caltrain drop off.	1/24/2019 6:28 PM
12	It's not cheap.	1/18/2019 9:22 AM
13	train runs infrequently during non commute hours	1/16/2019 9:52 AM
14	construction near San Carlos Caltrain station, winter rain	1/16/2019 8:22 AM
15	We drive when traveling outside of San Carlos	1/15/2019 11:29 AM
16	No concerns	1/13/2019 9:38 AM
17	Not a convenient way to do errands	1/10/2019 3:20 PM
18	a) Caltrain don't run south of San Jose in the morning nor north in the evening b) parking is cheaper than two people taking the train round trip. Why aren't we taxing parking to subsidize mass transit?	1/10/2019 12:42 AM
19	Car is more convenient.	1/9/2019 8:00 PM
20	Getting from Caltrain to other parts of city/needng to transfer to other provider.	1/9/2019 7:23 PM
21	Don't need it	1/9/2019 5:09 PM

Q18 What is your relationship to San Carlos? Please select all that apply.

Answered: 371 Skipped: 9



ANSWER CHOICES	RESPONSES	
I live in San Carlos	96.23%	357
I work in San Carlos	15.63%	58
I go to school in San Carlos	4.31%	16
I visit San Carlos	3.23%	12
Other (please specify)	5.39%	20
Total Respondents: 371		

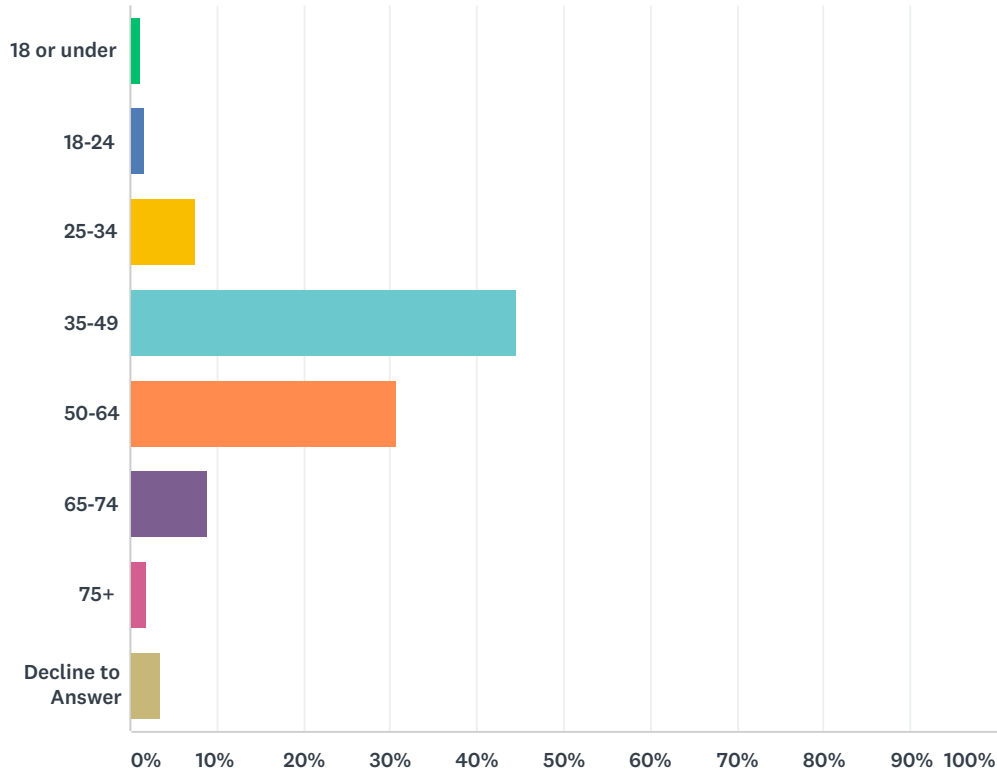
#	OTHER (PLEASE SPECIFY)	DATE
1	shop, exercise, etc...	3/6/2019 10:59 PM
2	I work in redwood shores	3/5/2019 5:27 PM
3	Children go to school in San Carlos	3/4/2019 11:43 PM
4	lived here my whole life as well as my grandparents and parents still	2/27/2019 7:02 AM
5	Kids go to school in San Carlos; hold lots of meetings in San Carlos; have colleagues, friend, and family stay at hotels in San Carlos	2/16/2019 9:11 PM
6	My kids go to school in San Carlos	2/14/2019 10:47 AM
7	My children go to school in San Carlos too	2/12/2019 1:44 PM
8	I've been a resident for 67 years (except for going away to college)! I'm retired but spend a lot of time volunteering in SF.	2/4/2019 7:06 PM
9	Have lived in San Carlos for 21 years	1/22/2019 9:12 PM
10	I live in Belmont, but use the San Carlos caltrain occasionally and often dine downtown.	1/19/2019 5:05 PM
11	kid's school	1/16/2019 10:58 PM
12	One child attends school in San Carlos	1/16/2019 10:53 PM

San Carlos BPMP

13	My son goes to school in San Carlos, my husband works in San Carlos	1/16/2019 4:26 PM
14	I shop in San Carlos because many places businesses have convenient bike parking	1/10/2019 11:40 PM
15	My daughter goes to school in san carlos	1/10/2019 8:58 PM
16	I work at Sequoia High School, which is close to my house in SCarlos.	1/10/2019 5:35 PM
17	I do very little in San Carlos.	1/9/2019 7:53 PM
18	95% of our shopping and dining is in San Carlos	1/9/2019 6:42 PM
19	My kids go to school in San Carlos	1/9/2019 6:25 PM
20	I operate a service business	1/9/2019 2:23 PM

Q19 What is your age group?

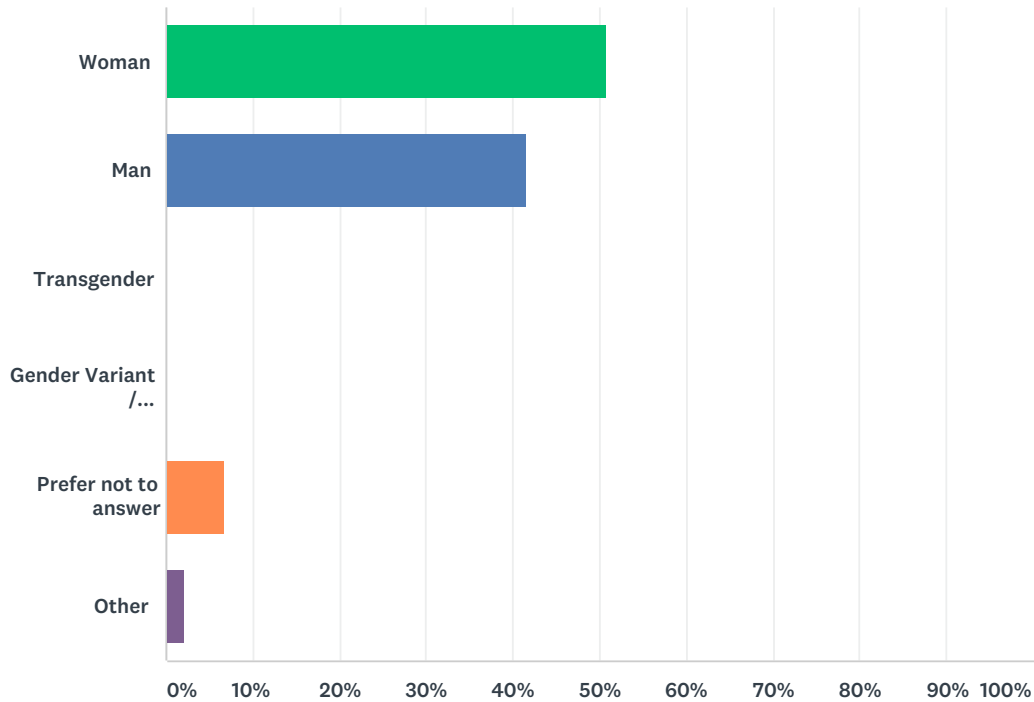
Answered: 371 Skipped: 9



ANSWER CHOICES	RESPONSES	
18 or under	1.35%	5
18-24	1.62%	6
25-34	7.55%	28
35-49	44.47%	165
50-64	30.73%	114
65-74	8.89%	33
75+	1.89%	7
Decline to Answer	3.50%	13
TOTAL		371

Q20 With which gender identity do you most identify?

Answered: 370 Skipped: 10

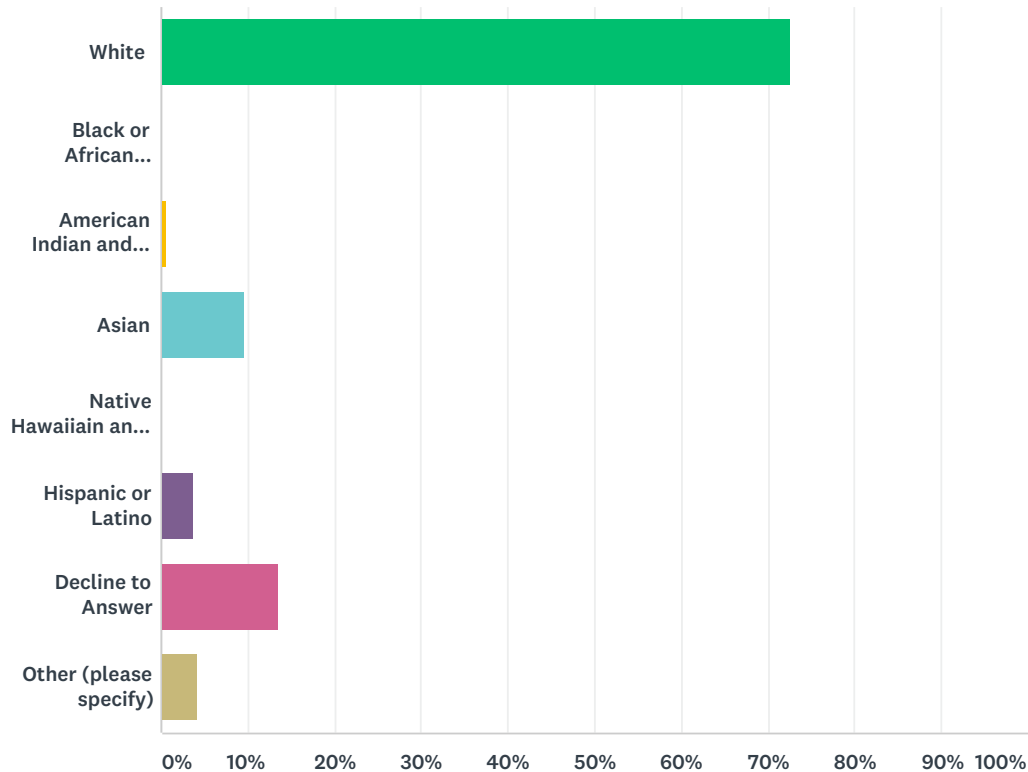


ANSWER CHOICES	RESPONSES	
Woman	50.81%	188
Man	41.62%	154
Transgender	0.00%	0
Gender Variant / Non-conforming	0.00%	0
Prefer not to answer	6.76%	25
Other	2.16%	8
Total Respondents: 370		

#	OTHER	DATE
1	Why does this matter? I am a woman but not sure how gender affects travel.	2/18/2019 8:24 AM
2	does not matter to you!	2/10/2019 2:28 PM
3	This shouldn't matter for a traffic survey? Everyone has to move, not target gender/age	1/19/2019 2:57 PM
4	Real Man	1/14/2019 10:38 AM
5	"Most identify"??? This place is like living on Mars	1/13/2019 10:45 AM
6	100% Male	1/10/2019 11:38 AM
7	Apache helicopter	1/10/2019 9:56 AM
8	what does this question have to do with a transportation survey?	1/9/2019 2:23 PM

Q21 Most people think of themselves as belonging to a particular ethnic or racial group. How do you identify yourself? Choose all that apply.

Answered: 366 Skipped: 14



ANSWER CHOICES		RESPONSES	
White		72.68%	266
Black or African American		0.27%	1
American Indian and Alaska Native		0.55%	2
Asian		9.56%	35
Native Hawaiian and Other Pacific Islander		0.27%	1
Hispanic or Latino		3.83%	14
Decline to Answer		13.66%	50
Other (please specify)		4.10%	15
Total Respondents: 366			

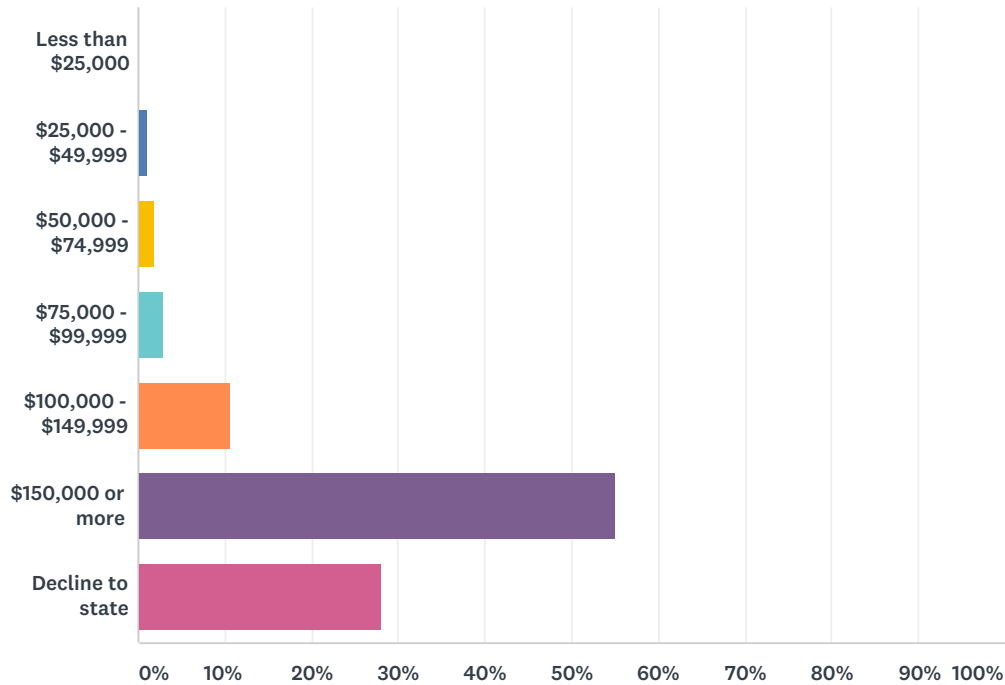
#	OTHER (PLEASE SPECIFY)	DATE
1	multi-ethnic	3/6/2019 10:59 PM
2	East European descent	2/23/2019 9:10 PM
3	multi-ethnic	2/20/2019 7:00 PM
4	Biracial	2/18/2019 7:34 PM

San Carlos BPMP

5	Why does this matter?	2/18/2019 8:24 AM
6	We are a mixed family of Irish and Chinese	2/17/2019 11:06 AM
7	San [REDACTED] Carlos	2/10/2019 2:28 PM
8	American of Asian descent	2/6/2019 12:20 AM
9	Why do you care? Address the problems don't make up new ones.	1/19/2019 2:57 PM
10	Two races	1/16/2019 2:55 PM
11	Why do you need to know this?	1/12/2019 9:08 PM
12	Middle Eastern	1/12/2019 7:52 PM
13	I thought we were a color blind community. Insulting question.	1/10/2019 11:38 AM
14	Persian	1/9/2019 10:16 PM
15	see above answer.	1/9/2019 2:23 PM

Q22 What was your total household income in the last 12 months?

Answered: 365 Skipped: 15



ANSWER CHOICES	RESPONSES	
Less than \$25,000	0.27%	1
\$25,000 - \$49,999	1.10%	4
\$50,000 - \$74,999	1.92%	7
\$75,000 - \$99,999	3.01%	11
\$100,000 - \$149,999	10.68%	39
\$150,000 or more	55.07%	201
Decline to state	27.95%	102
TOTAL		365

Q24 Are there any other comments you would like to share regarding walking and biking in San Carlos?

Answered: 234 Skipped: 146

#	RESPONSES	DATE
1	Please cut the curbs on St Francis and Cordilleras in the White Oak neighborhood. Streets need to be much better lit at night, especially on St Francis. Thank you.	3/17/2019 9:38 AM
2	there should be a bike lane on san carlos ave!	3/16/2019 7:18 PM
3	Thank you for working to make it easier to walk and bike in San Carlos!	3/11/2019 9:35 AM
4	I would like to see sidewalk improvements along Brittan Ave between Alameda de Las Pulgas and Milano Way. The sidewalks and visibility are poor and it's very dangerous for walkers, strollers, bikers and drivers. We have many walkers who use this section to get to the hiking trails off of Brittan and residents to get to neighborhoods in the flats as well as downtown. Brittan is being used more and more by commuters traveling through San Carlos so improved, connected sidewalks with crosswalks would dramatically improve safety and access.	3/10/2019 1:48 PM
5	Bike boulevards like Palo Alto where there are fewer stop signs are a big help with being able to quickly move through city streets while still keeping bikes off major roads like El Camino and San Carlos St.	3/10/2019 12:52 PM
6	a) Holly St b/w Industrial/101=widen, use full RofW to provide bike lanes b) possibly undercrossing of Caltrain at St. Francis Way (tied to Whipple Ave grade separation project c) linear park/ped/bike lane along Caltrain tracks as part of any passing sidings/tracks Caltrain needs to add. d) turn lanes on San Carlos Ave improvement project at side streets so can be in turn lane and vehicles can pass on the right while waiting for a break in the opposite traffic to make left turn e) thank you	3/6/2019 11:09 PM
7	I live around the corner from Arundel school. The walking situation to Arundel has been awful for all the twenty years I've lived here. It is unsafe, and I think it's a miracle no one has been injured or killed. I was almost run over walking my daughter to kindergarten fifteen years ago, and I heard of a car that came within three feet of hitting a child. There are a lot of simple things that could be done to improve pedestrian safety walking to and from Arundel. Residents have repeatedly communicated ideas to the city but the only thing that was done was to repaint lines (ruined recently by a terrible paving job) and to install a curb that is invisible to turning cars and has only led to blown tires. Residents were told, among other things, that a stop sign would be too expensive to install. Walking routes to Arundel need sidewalks, stop signs, and no parking zones so that kids don't have to weave in and out of parked cars to get to school.	3/5/2019 6:25 PM
8	one way streets....we need them !	3/5/2019 5:28 PM
9	I don't understand, with all the construction approved around the train station, why more bike lockers weren't added. Daily, monthly, yearly lockers would be appreciated. Until that's fixed, it's difficult to use the train unless you drive. And now that you moved the parking lots so far away (!!!!) it is very clear you are not encouraging Caltrain as a regular commute option. Very unfortunate. I understand that we had (have?) an in-town shuttle, but the closest pick up to me was not a huge savings in distance, and then I need to time my travel around the shuttle, so that was not helpful.	3/4/2019 11:48 PM
10	It's not safe for kids to ride their bikes on San Carlos Ave to get to school. Also, I walk on San Carlos Ave to the train and there are a few places where there is no sidewalk and it's dangerous to walk in the dark because cars drive so fast.	3/4/2019 8:19 PM
11	Should continue the Peninsula Bikeway through San Carlos that currently runs from Mountian View to Redwood City at the San Carlos border. Should run though San Carlos north to Belmont and San Mateo.	3/4/2019 5:53 PM
12	I would like to see biking and walking "pools" to schools. More buses to accommodate more schedules. Smaller buses that can navigate our narrow streets. I would like to see one way streets in San Carlos as many streets are far to narrow for two passing cars (like Belmont, Howard, Chestnut, etc.)	3/4/2019 1:51 PM

San Carlos BPMP

13	Traffic calming improvements installed on St Francis Way recently didn't go far enough to slow traffic between Park and Cedar. Why is there a missing crosswalk at the 4th leg of St Francis Way and Park Ave? Plenty of cars still travel over the speed limit in this stretch and it can be difficult pulling out of the driveway during rush hour do to the volume of traffic.	3/3/2019 9:53 PM
14	Improving the future of biking and walking in San Carlos is very important both as a safe means of traveling throughout the city and also in support of the health of our residents.	3/3/2019 9:47 PM
15	I love walking from my home near Arundel to Laurel Street to go shopping and eat out. But the sidewalk / or lack of sidewalk on San Carlos Ave makes it challenging especially with small children! Would be great to see some improvements for pedestrians. Thank you!	3/3/2019 5:27 PM
16	San Carlos sure has changed a lot in the past 5 years. Great to see a vibrant community. Not great to lose the small town nature. I hope biking and walking initiatives will be part of keeping the small town nature!	3/1/2019 5:37 PM
17	It would be great if there was better communication & enforcement of the 25 MPH speed limit on residential streets. Too many individuals drive in excess of the speed limit, which is a problem in neighborhoods without sidewalks, like ours.	3/1/2019 3:01 PM
18	I would love to have designated bike paths in San Carlos!	3/1/2019 1:12 PM
19	bike line marker on Arroyo would be nice as that's a common path to down town. Bike racks downtown are great. Maybe add a few more, as we often have to find a pole to chain to. not sure, but check what innovations they may have in Boulder CO recently for walkers, hikers, bikers, bladers..	2/27/2019 9:29 PM
20	The biggest challenges are the hilly nature of the town and the terrible design of our main roads-El Camino, San Carlos/Alameda,	2/27/2019 4:54 PM
21	I live and work in San Carlos. My children walk/ride/train to school. I want them to be safe as they travel. I believe that all the construction is making moving around San Carlos more dangerous.	2/27/2019 11:32 AM
22	Having a safe route to school plan from the EAST SIDE to arroyo and central for kids on bikes or walking especially since that there is no before school care for the younger kids who still need some safety.	2/27/2019 7:04 AM
23	I would like to see San Carlos adopt a Night Parking Ordinance like Menlo Park. San Carlos has turned into a virtual parking lot of parked cars, trucks, and service vehicles. Residents park the right wheels of their vehicles on the sidewalk, which is illegal in California. The residents of Menlo Park park their vehicles in their garages or on their driveways. It is so pleasant to drive down the streets of Menlo Park. There is room to pass oncoming vehicles without having to weave in and out of parked cars. It would take a major mindset change for the residents of San Carlos, because citizens would have to clean up the garages. The Menlo Park ordinance has been in effect since 1964 and its citizens seem to be surviving Okay without using their garages as storage units. Please look into this. It is so disappointing to come back to this city where I grew up in the sixties and see how it has deteriorated in this regard.	2/25/2019 7:30 PM
24	Palo Alto has done a lot to make their city bike-friendly. Specific routes and markings. Because of this so many more people bike to school, errands, etc. Palo Alto is a helpful case study for San Carlos.	2/25/2019 2:05 PM
25	We really would like a sidewalk on Belle Avenue (from Orange to Brittan Acres school). It feels like a dangerous area for pedestrians during school pick up and drop off. Thanks for your consideration and for soliciting feedback!	2/25/2019 1:28 PM
26	I think the safety of pedestrians and bicycles on Eaton Avenue needs to be addressed. I have seen near collisions between bicycles and cars at the stop sign at Garnet and Eaton several times with bicycles or pedestrians on the sidewalk on the north side of Eaton not seen by drives rolling through the Garnet stop sign.	2/24/2019 10:21 PM
27	the sidewalks are honestly an accessibility problem - there are tons of obstructions (power poles, signs, etc) that make it very hard to walk	2/24/2019 4:01 PM
28	I want better opportunities and better support for walking and biking in San Carlos, including more bike racks in front of the Post Office and other popular spots.	2/23/2019 9:12 PM

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29	I just entered 2 comments in the map portion, but I'll put them in here as well: 1) I love how I've seen in downtown San Mateo the pedestrian 'walk' lights come on several seconds before the green street lights do to give pedestrians a head start into the crosswalks before cars going in the same direction can begin to turn left or right 2) I feel strongly that any crosswalks on busy roads (there's one on San Carlos Ave near Prospect and there are some on El Camino) need yellow warning lights or some other indicator to drivers that pedestrians are using the crosswalk.	2/22/2019 9:49 PM
30	I'd like to see a bike path through the Black Mountain development project that makes access easier for hill residents like me with a longer, less steep grade than slugging it out up Melendy. Also, safe bike and walking routes to all schools, especially TL/Carlmont/Charter	2/22/2019 2:34 PM
31	Right now it has not been a problem but I would hate to see White Oaks School close the White Oak Way gates since I take that route when I walk to friends. If it was closed I would probably drive.	2/22/2019 1:51 PM
32	I love San Carlos and one of the parameters we put on selecting a home here was its proximity to downtown, public transit and schools. Much could be done to improve the safe routes to our schools (especially when crossing our city's busier streets), and for making biking for people of all ages and abilities safe. More trees would also make the community more aesthetically attractive and make walking A more comfortable and attractive option. Please make our town's people more of a priority than cars.	2/21/2019 8:22 PM
33	Thanks for thinking ahead and making plans for friendlier biking/walking paths for San Carlos	2/21/2019 5:40 PM
34	I just came back from 4 months in Sweden and they had more and well maintained walking and biking paths. I was able to bike everywhere with ease. It would be nice to have better traffic lights and smoother bike paths to promote more biking around San Carlos.	2/21/2019 5:09 PM
35	I encourage San Carlos to be BOLD and INNOVATIVE re initiatives for walking and biking. Consider closing off sections of Laurel to all car traffic. Create new parking/bike lane configurations. While there will be massive inertia and pushback, I want my city to lead in this area, as other cities have done in the world. Habits can, and must, be changed regarding reliance on cars. And.. downtown traffic has gotten so bad anyway, that walkers/bikers must be incentivized and encouraged! Thank you!	2/21/2019 3:31 PM
36	I truly believe a bike/pedestrian bridge over highway 101 is crucial to the safety of all community members - the drivers and bikers/pedestrians.	2/20/2019 11:38 AM
37	The north side of San Carlos Ave near Vista Ave needs a sidewalk, some sort of barrier to keep oncoming traffic from drifting into the current bike path, or a crosswalk to allow pedestrians to cross to the sidewalk on the south side of San Carlos Ave.	2/19/2019 11:55 PM
38	With massive extent of new development, and attendant traffic increases, biking conditions are going to become even more hazardous. The only mitigation is dedicated bike lanes.	2/19/2019 7:20 PM
39	Please be conscious of those who need to drive and park to and in downtown when considering changes	2/19/2019 7:05 PM
40	There are NO sidewalks in my neighborhood above Arundel school. The children and parents have to walk in the middle of the street. Walking up and down the stair that are a short cut to San Carlos Avenue is dangerous. The stairs are not maintained or lit. Walking in my neighborhood is not safe for people of any age. the street are very narrow and with parked cars even worse. Boats, plugged in electric vehicles and cars parked on the no parking side of the street makes things worse. Never see any of these vehicles marked.	2/19/2019 2:35 PM
41	I would love to see San Carlos build a connected network of separated bike lanes (with physical barriers to prevent accidents due to aggressive drivers.) I also feel strongly that we need real, well-maintained sidewalks on EVERY street in San Carlos. Lastly, I would love it if we could explore options for traffic calming (e.g. speed bumps) on small residential streets like Clifton Avenue where I live, so drivers will be less tempted to drive there at reckless speeds just to save a few minutes by avoiding the main thoroughfares.	2/18/2019 10:46 AM
42	Since more people are and will be living in the same amount of space walking and biking can we have better lighting at night so it is safer for them?	2/18/2019 8:25 AM
43	public transport, people? we need a lot more. these biking/walking is fine but not everyone lives in downtown and you fill the city up with condos which causes horrible traffic - who wants to ride a bike in that??	2/17/2019 9:15 PM
44	Lack of bike racks in laurel area	2/17/2019 5:28 PM

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45	Close Laurel to cars. Make it bike and pedestrian only.	2/17/2019 3:57 PM
46	My adult sons regularly use their bikes to commute in San Carlos and also take their bikes on Caltrain. I often see bicycles that annoy drivers by not following the rules of the road or passing cars stopped at a traffic light - instead of waiting in line with everyone else. More law enforcement or rewarding good riders would be great.	2/17/2019 2:02 PM
47	there zero encouragement and little information from city to make people use the bikes. people who are pro healthy lifestyle use cars to get to yoga studios or gymns. the same is with city employees. these people should inspire the others. its not happening :(2/17/2019 1:08 PM
48	We walk from Devonshire to Laurel Street, and it is just not safe enough, stretches are, and stretches are not. 1) Devonshire to Arundel is not safe enough...please add a railing to separate cars from people 2) Arundel to Alameda is safe on one side of the street only, unsafe on the north side. 3) Alameda intersection needs better pedestrian crossing stopping of cars...my dog almost got killed and I was hit by a car at that intersection since cars are looking how to get onto San Carlos Ave from Alameda, NOT LOOKING FOR PEDESTRIANS...and I had the crosswalk light flashing. 4) Alameda to Cedar is safe on south side, NOT SAFE ON NORTH SIDE. Add a railing to separate 5) Cedar to Laurel is perfect	2/17/2019 11:12 AM
49	Making a few blocks of downtown laurel Street pedestrian only, much like they do in Europe to establish a center of town, would eliminate much of the heavy traffic we now see on Laurel. The now unused street can be changed to a provide more area for restaurant seating and/or part like settings. Provide access to a parking structure nearby.	2/17/2019 9:25 AM
50	I see kids crossing Arroyo after school to get to the park where thereisnt a crosswalk all the time. Last year a child was hit. There have been many near misses this year. Something needs to be done.	2/17/2019 8:25 AM
51	Thanks for the energy and thoughtfulness you put into making living in San Carlos a joy	2/16/2019 8:45 PM
52	More bike paths in and around town on the east side would help. Also, the Holly overpass can be difficult to navigate with aggressive drivers	2/16/2019 1:30 PM
53	Please, please do not remove any more parking or driving lanes to add bike lanes. That is the last thing we need.	2/16/2019 1:19 PM
54	The first two blocks beginning at El Camino on San Carlos Ave are extremely dangerous for bikers. My suggestion is to make those blocks one lane and include a bike lane. Left hand turn lane could be added at intersection for cars. So, you would have diagonal parking, bike lane, car lane, and then left hand turn lane at intersection. Cars do not abide by the three foot rule for bikers. Also, more enforcement needs to be provided when it comes to right hand turns at El Camino and San Carlos Ave. Cars don't even stop when making turns on a red light, especially going south on El Camino onto San Carlos Ave.	2/16/2019 12:28 PM
55	The path from parking stalls to the Caltrain platform at the new station is NOT pedestrian friendly. One is required to walk along El Camino where cars are driving by at over 40MPH or walk through the parking lot where there is no separate walkway for pedestrians.	2/16/2019 12:24 PM
56	I would love to bike more instead of driving but feel that it's not safe for my children.	2/15/2019 11:08 PM
57	Improving pedestrian/bike safety around busy intersections along Alameda de las Pulgas, such as at Howard and Eaton would be great and more encouraging to use.	2/14/2019 9:53 PM
58	Increase parking in parking garages. So laurel street can become a pedestrian only area. I travel less to downtown since it's hard to park. Rather use amazon. And been eating out less since it is not convenient to go down town. With our time crunch schedule. Uber makes it too expensive for the evening and it's not safe to ride the bikes in the dark with young children. San Carlos is becoming less easier to raise a family	2/14/2019 3:07 PM
59	San Carlos Avenue is very dangerous. Cars drive way too fast, don't seem to have much respect for the bike lane, there's more than a few blind spots. We need more police speed enforcement. I also suggest "smart" speed bumps on San Carlos Ave between Alameda and Devonshire. https://thefw.com/smart-speed-bump/	2/14/2019 11:01 AM
60	I live at the top of Brittan. We need sidewalks to be able to walk down from the top of Brittan all the way to San Carlos downtown. Currently there are no "continuous" sidewalks that go all the way down.	2/13/2019 11:48 AM

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61	San Carlos Avenue needs to provide separation between motorized traffic and pedestrian/bicycle use. There is clearly a danger having fast traffic and slower bike or walking movement especially during commute hours. Cars making left turns off San Carlos Ave. to get to the freeway often don't see pedestrians. I would like to see higher curbs, bollards between pedestrians and cars. Additional protected pedestrian crossings. Perhaps widening the sidewalks to provide a bike lane separate from the street is an idea. Rules of the road don't protect bikes and people in the end.	2/13/2019 6:38 AM
62	Add push button pedestrian light signal to Brittan Ave. and Cordilleras. Belle and Cordilleras is also dangerous, impatient and aggressive drivers don't do a full stop when kids are crossing intersection to get to BA Elementary.	2/12/2019 11:06 PM
63	This is very tangential, but can we please plant more trees, particularly in the flats? It would be much more pleasant to walk and bike around San Carlos if there were more trees. Whenever I'm further south (Palo Alto, Menlo Park, Mountain View), I'm always struck by how many more trees those communities have compared to San Carlos. The El Camino corridor is particularly bereft foliage, but the whole city could use some additional greenery. I'm happy to create a San Carlos Tree Committee or whatever if anyone wants to get on board with me. :-)	2/12/2019 9:19 PM
64	I am very concerned with the safety of kids biking to school. The streets are narrow and challenging because there are many cars parked in the street. However the most important issue I see is MANY kids are NOT wearing a helmet properly. I live on the flat part of Eaton Ave and see many teens biking (I think mostly going to/from Sequoia). I think this is great but there are many cars zipping down the street and weaving around parked cars. I see MANY kids with a helmet but it is NOT latched on or it is hanging from their handle bars!! This is needs to be addressed. People absolutely need to wear a helmet properly if they bike. I want to live in a place where everyone can bike but kids (mainly teens) need to be educated of the importance of wearing a helmet. Tell them they how silly and stupid they look going down the street with it not latched on. Cars need to slow down too and be aware that a few more minutes getting to a place are not worth the price of an accident.	2/12/2019 1:58 PM
65	We need more of the orange flags at intersections, particularly those near schools.	2/11/2019 3:33 PM
66	My primary concern is with traffic, and with safety for my children. The corridor along San Carlos Ave and Alameda is particularly unsafe for them to bike to school.	2/11/2019 5:49 AM
67	I am starting a San Carlos business to address distracted driving and Walking. this is a dangerous place to live a disabled life.	2/10/2019 2:31 PM
68	My primary concerns are kids scootering and biking to school at Central and eventually Sequoia. And then good access to walking and biking for exercise/recreation	2/10/2019 12:35 PM
69	Cars parking in pedestrian rights-of-way and over sidewalks is a huge problem that is getting worse. I would love to see more pedestrian stairways in our hilly neighborhood, although that might pose a problem in terms of the ADA. Thanks for doing this!	2/9/2019 10:04 AM
70	we need better solutions for commuters. I know people commuting from Palo Alto to San Mateo by bike almost every day, and they call it quite pleasant except in San Carlos and Belmont. It's also less safe for me at some crossroads (e.g. turning left from Holly to Old County) and road condition pretty bad on OCR too, not to mention cars going in and out mechanics etc.	2/6/2019 10:49 AM
71	Improved sidewalks would help when walking around our beautiful city. Create bike paths and walkways to make it easier and safer for pedestrians and bicyclists to get around San Carlos.	2/6/2019 12:24 AM
72	we need side walks on all main street - san carlos ave on both sides.	2/4/2019 10:23 PM
73	Both flow and length of travel are factors. Some barriers such as the train tracks should have tunnel crossings every 2 blocks to facilitate development and access to the east side of the tracks. You shouldn't have to walk 4 blocks or more to just get to the other side. Bike racks could be better placed sometimes. Parallel to the street and far enough from the curb that a car can't hit it and it won't block someone from getting to the sidewalk from the car or jaywalking. Maybe they should be up next to the building rather than at the curb. Take back the sidewalks from the restaurants. They should all have signs stating that any seating in the public path is available to anybody whether they are a customer or not. People should be able to sit and eat their own food. These are public thoroughfares. Either that or put the seats and tables in the street and let the cars figure it out!-} Vehicles that are too long to fit in the parking spaces should be towed as soon as possible. Trucks and SUVs forcing vehicles to cross the center of Laurel should be ticketed and towed immediately.	2/4/2019 7:14 PM
74	Bike parking is an issue for running errands by bike on Laurel. I'd like to bike more with my kids but I need protected bike lanes to do so	2/4/2019 5:24 PM

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75	High priority should be placed on making pedestrian and bike paths safer and separated from traffic. People desire to walk and bike more, but it's currently unsafe, especially for children.	2/4/2019 2:39 PM
76	Walkability for myself and my family is one of the reasons I chose to live in the flats of San Carlos. It is one of my favorite parts of San Carlos and improvements to make it better for all would be great!	2/4/2019 1:38 PM
77	Please put a STOP sign on Crestview either at Melandy or Leslie. Rush traffic is vicious and kids can not safely cross the road to take public transportation to school.	2/4/2019 11:00 AM
78	Curb Cuts for bikes, wheelchairs, strollers are inconsistent. You'll often see one that doesn't have a matching ramp on the opposing side of the street. Even major roads like Cordilleras don't have curb cuts on every corner. One of the most egregious examples (that is actually unsafe) is the curb just between Central Middle School and Burton Park (corner of Arroyo and Chestnut. See image on Google Maps here: https://goo.gl/maps/9yrUPnbHPgR2). Kids with bikes/scooters and parents with strollers should be able to walk from CMS to Burton Park while staying safely on the sidewalk	2/4/2019 10:43 AM
79	We need better cycling routes especially East to west and expanded connections to bike paths along the Bay. It is insane that there is no good, safe way to get from West to East to use the bike path, either on Holly or Brittan. Whipple is just a scary joke. Please fix this. Thank you!	2/4/2019 8:22 AM
80	I live back in Devonshire canyon and I've seen the city do some great things to improve pedestrian safety, but with the number of kids walking on the main streets to and from school (Alameda, Beverly, Windsor, Torino), I really think we need to do more to let kids safely walk to and from school.	2/2/2019 7:21 PM
81	Certain designated bike routes in San Carlos are too narrow/dangerous. For example, Old County Rd designated as bike route but has very heavy industrial traffic, intermittent bike lane markings, bad surface. Industrial Blvd has bike lane but it is too narrow to share with heavy trucks common on Industrial. Bike lanes often cluttered with rocks, metal and other cast-off material. Bike lane at Edgewood and Scenic Drive has solid white line and is painted red, but commuter traffic regularly speeds through bike lane to pass cars turning on to Scenic; suggest install delineator posts at this location.	2/2/2019 6:28 PM
82	Connector roads lack continuous sidewalks (or smooth pavement surface) for ease & safety of pedestrians, especially San Carlos Avenue (from Beverly to Cedar, and Brittan (west of Alameda de las Pulgas to Hewitt).	2/2/2019 1:48 PM
83	I don't see a lot of bikers in SC. The streets are already narrow as it is so please don't put bike lanes in narrow streets. I used to bike a lot in bike lanes where I used to live and car doors opened on me a lot. One time I slammed into an opening door when biking in a bike lane, driver's fault for opening his door without looking. I imagine that happening a lot with these narrow streets. I love biking and want to with my kids, but since we live in the flatter area, we'll walk to downtown Laurel St for safety and ease.	2/1/2019 11:12 PM
84	IMO walking & biking facilities largely good but street lighting poor throughout the town esp. at junctions. Ability to see the stars is nice but surely there's a compromise possible esp at junctions; would benefit drivers also. Re: public transit: infrequency of services & lack of consideration for how it compares to private is disappointing. Twice the Caltrain frequency using half the carriages with alternating stop/bullet format & local bus connectivity would be the dream.	1/31/2019 12:57 AM
85	It'd be great to block off Laurel Street to cars at least once a week or month.	1/30/2019 7:04 PM
86	Happy to see this being undertaken and input being solicited. However, I fear that the safety of all our kids walking to school will continue to be compromised unless and until there is more rigorous, randomly timed and regular enforcement of speeding and other moving violations (including those of parents themselves driving to and dropping their kids off at school!)	1/30/2019 5:09 PM
87	San Carlos needs safer, protected bike lanes. Specifically, Laurel Street could use more designated bike lanes (this currently does not exist, San Carlos bike lanes should connect to other major ones (e.g. RWC, Palo Alto, etc.) in order to travel to companies in Silicon Valley and further down the peninsula (e.g. Google, etc.), and lastly, downtown SC could use more usable bike racks. The racks currently are more physically attractive rather than easy to access.	1/29/2019 11:40 PM
88	I hope that biking becomes expected by drivers and there's enough space on the roads for both. Plus, I hope there are enough places to lock my bike so I can run errands without a car now that car parking is getting more difficult. I walk for errands when the weather is right, I have time, and I can carry what I need easily. Otherwise, walking around San Carlos for recreation is pretty glorious and I love it here.	1/26/2019 9:48 PM

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89	I am a regular and passionate walker and cyclist. My son rides his bike to school and there needs to be a lot more work around ensuring safety for young walkers and bikers and reducing traffic off these corridors. Please fix old county road and through fairs and stop putting bike route signs up on roads that are really not safe or appropriate for riding. Please do something about el Camino and holly overpass and make the industrial shopping area walkable and bikeable. Please add stop signs on elm to prevent people from running through the other stop signs. Please slow down traffic and add bike lanes and sidewalk wherever possible. Make the walking dog friendly and add ramps to all sidewalks so you can roll wagons and rolling shopping cart/bags when you want to walk and shop and for kids on skateboards and scooters. Please have all the engineers working on this get on a bike and experience it along with those of us who do this everyday. I love that I am able to walk at night in San Carlos and feel safe. The sidewalks are good and helpful.	1/26/2019 3:09 PM
90	I hope the city is successful in this endeavor because it has become very difficult to drive and park in San Carlos.	1/25/2019 7:41 PM
91	Keep in mind that since you decided to increase our population without providing parking for everyone, making our streets have less lanes for a minority of bikers is really selfish. Also reducing parking on Laurel so that restaurants can provide more seating is also selfish. We're no longer the city of good living. You have caused increased pain & stress to your long time residents. It's tragic. I hope your enjoying your revenue surplus.	1/25/2019 1:42 PM
92	San Carlos is a great city; but there is a lot of building going on so planning such as this is important. Improving the walkability factor should be #1 as more people walk than bike. The major thoroughfares such as Brittan and San Carlos Ave (west of Alameda) need continous sidewalks - this is so obvious. Also, the city should be ashamed of the conditions of Brittan Ave and side streets with all the patch work black top.	1/24/2019 6:33 PM
93	My family and neighbors greatest concern is obtaining a flashing beacon at Brittan and Woodland. It would greatly improve our neighborhood and make it safe to cross at a place convenient to the east end of Burton Park and to nearby Arroyo school. Please make this a safer place to cross for all and for bike riders!	1/24/2019 11:42 AM
94	We need more lights at the crosswalks for safety. Especially at the crosswalk on Brittan and Woodland	1/23/2019 9:11 PM
95	I think the biggest problem with biking and walking safely is the number of cars parked on the street. It is particularly a problem on streets that are windy (eg, Eaton, parts of Elm, and Cordilleras) and those that are narrow. Also, the close proximity to corners that cars are parked. The density of street parking and at corners makes it very difficult for cars to see as well as the bikers and pedestrians to cross streets safely and bike in the streets. Personally I think there should be a significant decrease of street parking allowed. People should park in their driveways or garages and shouldn't be allowed to park within 20 ft of an intersection--this was a law when I took my drivers test--not sure why it isn't anymore.	1/23/2019 9:08 PM
96	Closer look at Brittan and Woodland intersection	1/23/2019 7:05 PM
97	I would love to see solutions for the crazy traffic with distracted drivers on San Carlos Ave that I see every morning when I walk my kids to school. It's simply not safe to have kids trying to get to school on the same very busy route that serves as the main artery for connecting 101 and 280.	1/23/2019 5:18 PM
98	Riding down Laurel Street is extremely dangerous for cyclists.	1/23/2019 1:29 PM
99	The intersections of Holly and El Camino, Holly and Old County Road, and Holly and Industrial Blvd need attention to make more pike and pedestrian friendly. Residents who live on Holly Street should be polled regarding their thoughts on improving traffic and safety. They need to be listened to.	1/22/2019 9:16 PM
100	It's very unfriendly to bikes - feels like you don't want cyclists. Roads and drivers aren't keen and no lockers near transit center.	1/21/2019 8:51 PM
101	I think distracted and rushed drivers make walking and biking very challenging in San Carlos.	1/21/2019 6:25 PM

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102	I think we should make downtown and the other side of Caltrain a walking/biking haven. If we can provide enough car/bike parking on the outskirts of downtown and close off downtown to be a permanent walking area, we can accomplish this. Cars don't actually "need" to go through downtown. More often than not, cars end up going in circles trying to find parking and pedestrians always run across the street to make sure cars don't get delayed. It's a fairly bad experience for all parties; No one actually saves anytime. Instead, if we open up more land from the Caltrain for parking and introduced much more bike parking on the outskirts of downtown, then everyone can walk nicely in the downtown area. We can use the space that is currently occupied by roads for fun kids facilities and sitting spaces. This will encourage a much more friendly social downtown while making it safe for families. The stress reduced from parking and dodging cars will be transformed into more willingness to walk. This can in turn encourage more diverse business to power activities in the Downtown area. The additional revenue can be used to provide even better education structures and extend to waste reformation. Furthermore, if we can reserve driving emissions for just work commutes, then we can aim to be a more green community. I believe San Carlos residents all believe in living in the City of Good Living. If we focus our energies, we can not only make it a haven for families but also be a role model to all cities around on what a modern green family community should be like.	1/21/2019 3:42 PM
103	Most residential streets are too narrow for bike lanes with cars parked on both sides. Cherry street is a good example. There are many streets rpthat have no sidewalks which make them unsafe for walking. Goal for pedestrian crossings that have no stop lights or stop signs should have pedestrian activated signals. Thank you SC for helping provide crossing guards near schools. Much appreciated.	1/20/2019 10:26 PM
104	Please check out streetlightdata.com and their just launched bicycle and pedestrian metrics.	1/20/2019 4:10 PM
105	It will be next to impossible to get families out of cars and transition to bikes, especially in the San Carlos hills.	1/20/2019 3:50 PM
106	yes. I live on Altura Way in Belmont. San Carlos (exeter ave) is my back fence neighbor. I often go for runs or walks from my house, and would like to access the hills and Arundel park more easily... but it is fenced off. I'd like to see access through the mid peninsula water tank site at the top of my cul-de-sac. Many of my Belmont and San Carlos neighbors would also like the same. We also drop off friends in SC often by car, and it would turn a 2 mile hilly trip in a car to a 1/4 mile walk. Similarly I'd like to see the Pulgas Open Space reserve gate open at the top on Crestview, and connected to the water district cross country track Sheep Camp trail. I don't understand why they are fenced off right now.	1/19/2019 5:09 PM
107	Thanks for the public input map; that's a great idea.	1/19/2019 4:31 PM
108	- Survey didn't address handicap/mobility issues - Survey did not address drivers perspective and what could be done to improve traffic flow wrt ped/bikes/buses/trains - Survey did not address new construction and population volume changes / impact	1/19/2019 3:01 PM
109	The Holly St and Old County Rd intersection is dangerous but our only way of accessing town from our house. Improving pedestrian safety there would be much appreciated by the members of the Greater East San Carlos neighborhood.	1/18/2019 9:31 PM
110	Please fix the dangerous cracks in the sidewalks, the non ADA-compliant sidewalks, and the potholes in the streets. Please mark intersections clearly, especially on major streets like Laurel, Brittan, and San Carlos Ave.	1/18/2019 4:13 PM
111	Taking out the Eucalyptus trees along San Carlos Ave. eliminated some of the fear of walking under them on windy days. Thanks for that!!! When are the rest of them coming down?	1/18/2019 4:07 PM
112	The most important thing to me is better bike parking at the caltrain station.	1/18/2019 8:28 AM
113	Bike lanes would be wonderful - it's scary crossing el Camino to get to the Caltrain	1/18/2019 12:40 AM
114	San Carlos Ave NEEDS better side walks and barriers from busy traffic. I would walk to/from downtown more with my kids, but it's not safe due to lack of sidewalk on either side. San Jose installed 12inch bright green reflective tubes/cones as a reminder/barrier along some heavy car and pedestrian traffic areas. There has already been one death on SC ave. Also, there are some places where residents tore up their sidewalk and put plants in their place. When reported to the city, the response has been, it's been that way for a long time and nothing we can do. It is especially frustrating when it is near schools and limits children's ability to safely walk to and from. It would be nice to see if there are some alternative paths to get to downtown that may not involve as busy of roads as San Carlos ave or alameda and Brittan.	1/17/2019 10:01 PM

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115	I've seen an accident or near accident on the Holly overpass. With CalTrain nearby an business in Redwood Shores it makes sense for people to ride bikes. I believe the bike overpass has been approved but don't know the timing.	1/17/2019 8:25 PM
116	Crosswalk at Woodland and Brittan need flashing light. My family and myself have always avoided this crosswalk since drivers do not stop. We have lived in San Carlos for 25 years and that crosswalk has always been unsafe and has gotten worse.	1/17/2019 7:36 PM
117	Please provide continuous, high quality bike facilities to popular destinations. A shared lane on a busy street is not nearly good enough.	1/17/2019 3:27 PM
118	For our main transit corridors, we should have either protected (not just painted, but with physical barriers) bike lanes OR we should have parallel bike friendly roads. Bryant St. in Palo Alto is a good model that parallels Alma & El Camino to get from one end of the city to another.	1/17/2019 3:11 PM
119	I love to discover hidden pathways...the little pathway from Arroyo to the Bianchini's parking lot. The stairway off of Orange (I think), the old Postman's Path off of Carmelita (now closed...but I wish it was open.) Little delights like these make walking and discovering new things fun. Would love to see more of that. Also, downtown. Would love to see more restaurants spilling out into the street. More gathering areas. Maybe make it a one way street (South to North). This is not a fun biking town...would love to bike more. Not sure the answer, but more bike paths and lanes would be great.	1/17/2019 2:30 PM
120	I have been almost hit by cars on Brittan at the Woodland crosswalk. That intersection needs a flashing beacon at the very minimum.	1/17/2019 12:53 PM
121	Laurel St. very busy with cars, people. Impossible to ride bike there. Should emphasize getting the word out on the Dutch technique for opening car doors. Lessens "doorings". Need more designated, painted bike paths. Thank you.	1/17/2019 12:46 PM
122	I walk into town at least 4 times a week. I bike in when I have my husband with me maybe 2 times a week.	1/17/2019 11:48 AM
123	People drive incredibly fast considering the roads are so narrow. I do not feel safe opening my car doors when exiting for fear of getting hit or my door taken off. I could not imagine cars being able to safely share the road with bikes due to the current driving behaviors and narrow roads.	1/17/2019 9:49 AM
124	Need crossing guards at Alameda and Brittan	1/17/2019 2:16 AM
125	I was so impressed how many people bike easily and safely in Vienna! Too many cars here, congestion with more and more housing, people are in a hurry, motorists don't all yield to peds and bikes. Is dangerous here to mix cars and bikes, even with the signs that bikes have full use of lane. Seeing the Lime bikes around town is good to plant the seed for people to ride. Thanks for looking into this.	1/16/2019 11:04 PM
126	San Carlos Avenue is a major artery, but especially hard to use between Cedar St and Devonshire Blvd - the sidewalks are terrible and uneven for parents and nannies with baby strollers, and many drivers do not respect the bike lane	1/16/2019 10:56 PM
127	Very concerned about the intersection of Woodland/Brittan - flashing light crosswalk needed. Also, Garnet Avenue is a cut-through for traffic and cars speed down the street often.	1/16/2019 10:51 PM
128	Is there anything you can do to make it safer to cross over 101 on Holly. It's truly terrifying! Biking to/from TL, Carlmont, Charles Armstrong, etc. along San Carlos Ave is also super dangerous. Biking should be much easier/safer for students. Thanks.	1/16/2019 10:00 PM
129	Would love better public transportation options in hills. Crosswalk on Crestview & Melendy is a must so that my kids can take bus to school and even for recreational walkers.	1/16/2019 6:10 PM
130	Cyclists don't like stop signs - that's why you get people complaining that they don't stop. Pushback on pressure from residents to add more and look for creative alternatives. There are countries the do not use stop signs so it is possible. Paint red non parking lines at junctions, especially those where one street does not stop. For example on cordilleras & edgehill. Vehicles parked on Cordilleras obscure view of traffic coming from right when coming down edgehill which makes it dangerous, especially for kids going to school. People should park their cars in garages or driveways.	1/16/2019 5:57 PM
131	More bike sharrows please (share arrows painted on safe bike routes) and educate drivers to slow down and share the road with cyclists.	1/16/2019 5:52 PM

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132	To provide a safer walking and biking environment, drivers need to drive safer. Stop at stop signs, yield to pedestrians in crosswalks. An increase in traffic enforcement for autos not following the laws. But should go for cyclist too.	1/16/2019 4:42 PM
133	I would like to see a bike/pedestrian bridge to get over the freeway near Holly. I would use that daily.	1/16/2019 4:14 PM
134	I think providing safe and convenient public transit routes/corridors for walking and biking is important for the future success of San Carlos	1/16/2019 4:05 PM
135	I really wish the city would offer transportation solutions for residents of the hills. There are no buses except very limited service on school days. Biking and walking aren't really feasible to downtown.	1/16/2019 3:46 PM
136	Please extend the sidewalk on San Carlos Ave. to the downtown area. This will make it easier and safer for kids to get to Arundel school and for all of us to get to the shops and restaurants.	1/16/2019 3:00 PM
137	The crosswalk on Brittan and Woodland Ave is dangerous. I would like to see a flashing beacon installed. This is a major crossing for school age children to school, park, youth center, library. Many people avoid the intersection because it is unsafe.	1/16/2019 1:37 PM
138	My biggest concern is that we make sure the BEST bicycle and pedestrian facilities are in place to encourage and protect people and bikes while in transit and bikes are parked.	1/16/2019 12:50 PM
139	Like that we have a central downtown with lots of shop/dine choices!	1/16/2019 12:19 PM
140	not safe to bike down laurel street.....	1/16/2019 9:53 AM
141	Please consider making a portion of Laurel St. as a pedestrian/bike only zone. The traffic in downtown San Carlos is a deterrent for bicycles. In addition, I now work near a Caltrain station so hope to take Caltrain to work once the construction near the San Carlos station is complete.	1/16/2019 8:24 AM
142	Oppose efforts to restrict curb parking which will just turn the arteries into highways (even more than they already are). Support efforts to ease traffic including adding stop signs, speed bumps. Strongly support bike lanes and street closures during school hours!	1/15/2019 8:41 PM
143	Sidewalks and walk ways in poor condition-many streets do not have sidewalks. Traffic is heavy and congested. People do not follow road signs. No crossing guards on San Carlos Avenue for students. Traffic on San Carlos Ave is heavy-congested-fast paced.	1/15/2019 7:43 PM
144	Let's make San Carlos more bike/ped friendly!	1/15/2019 7:04 PM
145	The drivers in San Carlos have turned awful in the last several years. They are inconsiderate, uncivil and sometimes, dangerous. I would love to see more enforcement of driving violations under than speeding. For example, every single day I see people doing stupid and illegal things on Laurel St., but they are never ticketed. If people know nothing will happen, they continue to drive like idiots.	1/15/2019 6:11 PM
146	Downtown Laurel street should be a pedestrian zone. No car traffic. This would improve the quality of the downtown business area tremendously	1/15/2019 3:05 PM
147	There is a dangerous section of San Carlos Ave that I've had multiple near misses with cars while riding my bike. There's a curve between Sycamore and Cordilleras where cars frequently drive in the bike lane. And the crosswalk before this curve needs flashing lights on the road because no one stops for pedestrians there.	1/15/2019 2:51 PM
148	Tripping hazard because of uneven sidewalks is concerning. Crosswalk at Howard and Alameda is not always honored by autos	1/15/2019 11:34 AM
149	Laurel and Walnut Streets are not bike friendly so it is difficult to access businesses downtown. The corner of ECR and San Carlos Blvd. where everyone crosses to go to Caltrain is very dangerous. Cars pop right turns from SCB into the pedestrians all the time. Riding a bike down San Carlos Blvd between Laurel and ECR is very dangerous The Holly Street overpass is not safe for bikes or pedestrians. Totally ridiculous.	1/15/2019 11:09 AM
150	Do not take away car lanes for bike lanes. And do not paint and bike lanes.	1/14/2019 10:40 AM
151	Left turn lanes can't detect my bike so I don't get green turn and have to go on red	1/14/2019 1:02 AM
152	Day use lockers for bicycles and scooters at train station. Too many thefts from locking bikes/scooters up.	1/13/2019 5:15 PM

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153	The improvements that would most benefit our routes would be sidewalks on San Carlos Ave near Arundel and bike lanes near Tierra Linda.	1/13/2019 2:39 PM
154	In order to encourage biking, we need clearly marked, seperated bike paths that are not on the road. I know that may not be possible, but biking on Old Country road is pretty miserable, despite there being little bike icons painted on the street. If it's not cars trying to pass you, it's the road condition shaking your bones, or cars trying to turn in front of you. Also, for instance the bike lanes connecting to Alameda de las pulgas, you need to cross two traffic lanes to make a left turn. Drivers often have no idea what bike hand signals mean. Just the other day I was trying to cross these two lanes to get into the left turn lane on San Carlos Ave N to get onto Alameda de las pulgas, I was literally shaking my arm up and down, fully extended, and a driver still did not recognize this and did not let me into the turn lane. Also, bike parking at home is a big issue in multifamily homes. If we want to make it easy for folks to bike, we need to make it easy to park, get on and get off the bike. I started biking only when I was able to park my bike in my condo's parking garage (because then it was as convenient as a car). Having to haul your bike in and out of your home each day before biking is a huge barrier.	1/13/2019 2:36 PM
155	I would like to see the plan focus on 1) helping people get around San Carlos where its reasonable connecting schools and parks... like getting good bike lanes on San Carlos Ave (which also gets people to Caltrain. 2) Don't get sidetracked on something silly like trying to put in bike lanes connecting San Carlos Ave and Crestview (too much elevation change... people who do it for a workout, don't need bike lanes here)	1/13/2019 2:20 PM
156	The downtown needs way more bike accessibility including bike lanes, bike parking (need way more than 1 bike rack per block). It's difficult for a family to ride downtown when there is no bike parking for the whole family. I would LOVE to see Laurel street off-limits to cars, with car parking located on the adjacent streets.	1/13/2019 1:29 PM
157	We've lived in San Carlos since 1990 and raised our family here. San Carlos is such a great city in most ways except for support for biking, and I'm hoping it can do something really transformative in becoming a model biking city for other cities in the US. Other than the silly couple of inconvenient bike "racks", I really don't see any significant support for biking in San Carlos. I was disappointed to see that there doesn't even seem to be bike racks/lockers in the new parking garage!? Thank you for developing a plan and soliciting community feedback.	1/13/2019 11:16 AM
158	Sidewalk conditions in nearly all residential areas near downtown are terrible for strollers. Bumpy, narrow sidewalks with steep driveways every 15 feet make it challenging.	1/13/2019 10:55 AM
159	More sidewalks and less street parking.....not rocket science	1/13/2019 10:46 AM
160	I think the shopping/dining blocks of Laurel should be closed to car traffic and the space should become a pedestrian plaza.	1/13/2019 10:39 AM
161	I am very, very, very concerned about the bike and pedestrian paths to our north-side schools (Arundel, TL, Mariposa). To access these schools, there are missing sidewalks and poor visibility. As there is very little access for parents to reach TL/Charter/Mariposa, children mostly walk, ride bikes or take SamTrans and it is awful. I can't believe that our city can't do better for our children. Also, we walked to Arundel for five years daily and there are parts of the pathway that don't even have sidewalks on roads where two cars can't pass one another. It is horrific!	1/13/2019 10:36 AM
162	Would be nice to have line bikes in San Carlos. I walk around redwood shores and foster city and the line bikes are great for getting around.	1/13/2019 9:39 AM
163	Walking is getting extremely dangerous in this town. Drivers do not stop for even lighted crosswalks. They barely slow down for a right turn on red. I am very worried about kids walking to and from school. I've already asked the police to patrol more on the SC Ave corridor, but the sidewalk issues, along with lack of proper crosswalks, make me very nervous sending my son to school! Please consider more than the White Oaks area and Laurel Street as areas of concern. I walk San Carlos every day and there are many unsafe areas in town. Thank you for doing this survey!	1/13/2019 9:08 AM
164	We need to take aggressive action to reduce the number of drivers using San Carlos as their cut through. The situation is out of hand.	1/12/2019 10:12 PM
165	The most obvious thing to be done to allow my kids to go to school by bike is to create a real, secured bike lane (with separators avoiding cars and trucks to drive on it) on San Carlos avenue from downtown to the schools (TL and Carlmont)	1/12/2019 9:57 PM

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166	We could and should be doing so much better in this regard. We are one of the few communities on the peninsula that does very little to keep up with that is going on all around us. Times are changing and we need to keep up.	1/12/2019 9:09 PM
167	Traffic, especially downtown is insane. I live downtown and walk almost daily and even in cross walks with a stroller car will just plow through. I am surprised there are not more accidents. The city council has been completely oblivious the past 5-10 years with traffic. San Carlos is anything but the city of good living for downtown residents. I suggest closing Laurel street between Olive and San Carlos avenue making it a walking street. And charge for parking, but offer free shuttles to encourage people not to drive. Create a bike / walking overpass to the Bay trail.	1/12/2019 8:41 PM
168	I support (and really appreciate) any and all improvements to make San Carlos more walkable and bikeable, as it was one of the reasons I chose to live here. One small gripe with the current walking experience is the closure of the sidewalks along El Camino real adjacent to the Caltrain station; it is incredibly frustrating that they have been inaccessible for so long as it impedes access to both the station and bus stops. I'm fully in support of more housing, but I would ask that any construction works make an effort to minimize their impacts on walkability (both the amount of public sidewalk space which they appropriate, and the length of time they take to give it back.) Final note - thanks for allowing online feedback!	1/12/2019 8:25 PM
169	I am unclear on what this project plans to do. I was hoping that taking this survey would give me a better idea, but it has not.	1/12/2019 8:09 PM
170	There's no great bike route to get from the laurel street area to the train station without having to ride on el Camino, San Carlos ave, or laurel st which all have heavy vehicle traffic and no bike lanes.	1/12/2019 7:53 PM
171	I would like to bike for recreation & would like bike paths to connect with other peninsula cities. Also, a safe bike/pedestrian path across the 101 freeway would be awesome to connect with Redwood Shores & the bay trail.	1/12/2019 7:28 PM
172	No concerns walking or biking in San Carlos	1/12/2019 6:50 PM
173	I would love to see a continuous, dedicated, bike path paralleling 101 along the bayshore from San Mateo to San Jose. When commuting to my job in the south bay by bike, the best route is to take Middlefield the whole way, which adds at least 1/2 hour of time stopped at red lights to my commute. San Carlos has a decent segment along Shoreway, Skyway and Bair Island. But it breaks down in Redwood City, East Palo Alto and points south. This is a regional issue, not just a San Carlos issue!	1/12/2019 1:19 PM
174	Beware of false hopes. San Jose spent a lot of money on removing a car lane from Chynoweth to make a bike lane hardly ever used!	1/12/2019 11:42 AM
175	Lack of sidewalks and safe bike paths is one of the worst parts of living in San Carlos. I don't trust the drivers and the tight roads to let my son bike to school. I hate walking home in the street, in the dark. I drive to downtown when I could easily walk.	1/11/2019 8:59 PM
176	I prefer to walk around San Carlos than bike. There need to be a positive balance between walking, biking, and cars. People still need to drive a car and park near your destination.	1/11/2019 8:28 PM
177	I Would like to see sidewalks on the 700 block of Cordilleras that connect to other blocks for increased pedestrian safety	1/11/2019 8:16 PM
178	I would like a dedicated bike lane	1/11/2019 8:04 PM
179	I would like a safer bike lane on the Southern part of Cedar Street from Brittan to Eaton because I know that a lot of people from White Oaks and Sequoia High School and some kids from Brittan Acres and Central Middle School bike along these roads and that parked vehicles tend to get in the way of us and the drivers.	1/11/2019 7:27 PM
180	Our family is a fan of a bike/ped overpass over 101, we would like safe access to Redwood Shores and often see others attempting the overpass in the dark with crazy traffic and worry for them.	1/11/2019 6:11 PM
181	There are so many nice neighborhoods throughout san carlos but without reopening things like the many postman's paths then they are largely inaccessible/diverted to much less pleasant high auto traffic routes	1/11/2019 6:06 PM

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182	1. Survey should assess EBikes as they are a game changer with sudden high adoption. Need to plan for this. Eg we will reduce by one car and buy 2 ebikes to replace it in our family. 2. Key commute and recreation routes have the most traffic where they connect into Emerald Hills, Belmont Crystal Springs and Bay Trail, and Redwood City Bay and southbound to other areas. Ie San Carlos optimizations Of bikes is insufficient w/o coordination regionally. 3. Residents don't know how to imagine what a fantastically walk/bike friendly place SC could be. What about Super Blocks from Barcelona. Or Danish bike parking? Or truly segregated safe San Carlos Ave biking that could entice 10X bike traffic and alleviate cars/parking issues downtown? It's time for radical imagination and a whole new town around bikes for fun, easy, cheaper, lower carbon But this requires out of box plan to make bikes safe and fun here!!	1/11/2019 5:16 PM
183	It is completely unsafe to biking in San Carlos because we don't have designated bike lanes. There is a bike lane on Old Country road that is a death trap. There isn't a shoulder and road isn't wide enough to accommodate both cars and bikes at the same time. The only way a driver can get around the bike is by passing into on coming traffic which is dangerous. We have the same problem with industrial road. The bike lanes need to be clearly marked with green paint so the drivers can see the lanes.	1/11/2019 11:35 AM
184	I think of a lot of our bike/pedestrian issues as not just an urban planning/design issue, but an operations issue. Usually there's an end-goal in mind such as: get to downtown or to the bay trails. The "operations" part of this is addressing all of the "bottlenecks" in getting to that goal. What's in my way to reach my goal that makes it unpleasant or not even feasible? Presumably, addressing those bottlenecks makes the whole system and city so much better. I look forward to this conversation progressing. Thank you for initiating it in this manner.	1/11/2019 9:57 AM
185	Please link up with separated bike lanes on ECR from RWC	1/10/2019 11:41 PM
186	I am glad this topic is being addressed. The speed of traffic and distracted drivers on San Carlos Ave. and Devonshire Blvd. is often very concerning. We hope future improvements will allow our children will be able to some day walk, bike, skateboard, etc. safely in our neighborhood, to school and around town.	1/10/2019 11:26 PM
187	biking in San Carlos is tricky. The streets are narrow and there are many parked cars which make it difficult to navigate on a bike. I don't always feel safe biking in San Carlos for this reason and sometimes will opt to walk to my destination instead (if I have time). Otherwise, I will drive.	1/10/2019 11:25 PM
188	Thank you for making this a priority. Safety is the biggest concern for me. As we all are aware that people drive too carelessly and there have been too many lives changed and lost because of this.	1/10/2019 11:19 PM
189	Speeding up hill on Orange at night happens commonly. Dangerous for dog walkers. Speeding is inviting coming from San Carlos Ave direction on Cedar and only needing slight turn onto Orange. Suggest apples bump towards the bottom of Orange.	1/10/2019 10:24 PM
190	it would be a HUGE improvement to put in a real staircase on the hill behind St Charles Catholic Church. I would immediately connect San Carlos residents who live west of Alameda to walk to Laurel and Brittan Acres school. Right now, a ton of people use it to walk to/from Brittan acres, and to/from eucalyptus street during halloween and xmas, and it's not safe at all. People are already using the hill as a walkway, we should consider making it safer and more pedestrian friendly.	1/10/2019 9:05 PM
191	Yes. Two things. First the sidewalks on Brittan west of alameda are a disaster. They just end, forcing people to cross in a dangerous place. The should be extended. Second, I really wanted to be able to bike to Caltrain and take the train. But the second day I did it, my bike was stolen. (Not a super nice bike, a 25 year old beater I bought in college.) when I called the police, they said bikes get stolen "all the time" and I should forget about it. Bikes may be cheap, but the fact that I can't bike to the train station is a real problem.	1/10/2019 8:57 PM
192	Would be nice to have a bus like SCOOT back again.	1/10/2019 8:29 PM
193	--Re: #7 I use SanTrans to travel between SCarlos to SFO or Palo Alto. My transportation needs cross city boundaries; it seems counterproductive to plan as if transport is only needed within the city. --If I need to go to RWC at night, walking between Sequoia HS and my White Oaks home feels unsafe (I don't feel that way about walking at night in San Carlos).	1/10/2019 5:45 PM
194	I would certainly appreciate law-enforcement regulating cars encroaching in crosswalks in particular El Camino Real to Old County Road	1/10/2019 5:33 PM
195	Major work on San Carlos Avenue to make it a safe place to walk would be awesome!	1/10/2019 5:24 PM
196	Separate bike lanes would be outstanding!	1/10/2019 4:08 PM

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197	Walking downtown with all the restaurant seating on sidewalk is not fun. People are rude and not enough room to walk. Those sidewalks were meant to walk on. NOT SIT and EAT.	1/10/2019 3:25 PM
198	Drivers routinely speed (30- 35+ in 25 zone) and often fail to come to a complete stop at stop signs. This makes riding a bike challenging because of safety concerns. Simply stated, our streets are narrow, especially given the size of many vehicles. This wouldn't be such an issue if people would drive responsibly.	1/10/2019 1:54 PM
199	I am looking forward to better sidewalks and better traffic control on San Carlos Avenue	1/10/2019 1:31 PM
200	Walking is great. Cicyling is just too dangerous given the amount of cars on the road and the density of the population. Put the financial resources into improving the ingress and egress of San Carlos access to highway 101. A northbound ramp at Brittan onto 101 should be the city's top priority.	1/10/2019 11:41 AM
201	Need to enforce laws and rules for bikes. Also bikes on bike paths should be single file, not spilling out to roadways.	1/10/2019 10:36 AM
202	i have 3 kids that bike or take samtrans to school. My biggest concern is aggressive/distracted drivers and getting across big intersections like san carlos ave/el camino/holly. Would like better bike access on old county, my son takes that route to HS (Dtech) on his bike. There is no bike lane on that street.	1/10/2019 8:50 AM
203	Think about creating a showy bike parking lot on Laurel to encourage ridership. No reason why people can't ride to dine or ride to shop...	1/10/2019 12:46 AM
204	The biggest risk to pedestrians in San Carlos is from drivers who do not look for or expect to see them in the crosswalks. This is a constant danger, experienced daily by those of us who walk several miles a day in the city. A reasonable solution might include education, signage, and far more traffic enforcement. Thank you for requesting citizens' input.	1/10/2019 12:05 AM
205	Love that you're trying to do this. Thank you and keep up the good work, it is worth it!	1/9/2019 10:25 PM
206	-wider sidewalks -dedicated bike lanes	1/9/2019 10:18 PM
207	We need many more bike racks in retail business sections of SC	1/9/2019 10:08 PM
208	No	1/9/2019 9:13 PM
209	I would like to see skateboards and scooters not allowed on Laurel street between San Carlos Av and Arroyo. The sidewalks are not wide enough. I have seen the elderly almost hit by children especially on Wednesdays when school gets out early. I would also like to see Laurel closed between Arroyo and San Carlos. Since most restaurants have outdoor seating, the sidewalks have narrowed for walkers. Bikers nearly get run over by drivers backing out of parking spaces. Golf carts could be used to transport people who cannot walk to their destinations between Arroyo and San Carlos Av.	1/9/2019 9:06 PM
210	I'd love to make walking and especially biking in San Carlos safer, so that more people choose to experience our city that way. As an added bonus, a trip done outside of a car reduces vehicle miles traveled (VMT) which reduces traffic for those still in their cars. It's a win-win for drivers and non-drivers alike!	1/9/2019 8:50 PM
211	We really need to make more and safer routes to get to downtown. Hopefully people will walk/bike more if these routes exist. Our city should prioritize walkers/bikers and their needs to create a better community!	1/9/2019 8:47 PM
212	When bike lanes were placed on San Carlos Ave, the merge lane from Upland was eliminated. This has made it much more difficult to get out of my area going toward town. We therefore more frequently go west or north into Belmont to bank, shop etc. The loss of that merge lane was very hostile engineering. Why not let the bikers share some of the cost of pavement with a new license or tax.	1/9/2019 8:04 PM
213	I think that two blocks of Laurel should be pedestrian only. You can barely drive through anyway.	1/9/2019 8:03 PM
214	For me the largest issue is having safer sidewalks and cross walks	1/9/2019 7:52 PM
215	Yes. We need left turn zones (basically one car length before each stop sign clearly painted/signed/designated for bike crossover). "Bike friendly community" signs would help. Putting "the Dutch reach" & "look right for bikes" in community newsletters would help.	1/9/2019 7:26 PM
216	Narrow streets with cars parked on both sides make it difficult to navigate safely.	1/9/2019 7:17 PM
217	Too much traffic and too many aggressive and speeding drivers....no traffic enforcement!	1/9/2019 6:53 PM

San Carlos BPMP

218	Some streets in the White Oaks area, for instance St. Francis get tons of work on it when other streets like Upper Eaton get none. We have huge traffic issues also due to Clifford school yet no sidewalk ramps have been done in this area. We pay just as much taxes so why aren't we getting the same attention?	1/9/2019 6:44 PM
219	We need a crosswalk at Cordilleras and Carmelita, please.	1/9/2019 6:26 PM
220	I would LOVE to see safe biking routes in San Carlos, for myself and my kids. We will not bike today because there is no way to do it safely. Concerned about moving cars AND car doors being opened into bike lanes. But I could see the City of Good Living set a new standard for bike safety and commuting with some simple changes. Look at Montreal and SF, where with simple repainting of traffic lines, parked cars and bike lanes are flip flopped, so the parked cars protect the bike lanes from traffic. This wouldn't require widening roads on bigger streets like Brittan, Alameda, San Carlos - just changing the order of what flows where. Would help with traffic congestion with growth of city - we could actually bike to Laurel St, schools, train station, Bay Club, friends' homes!!!! + for health and environment too.	1/9/2019 6:14 PM
221	We need crossing guards at the Devonshire/San Carlos Ave light. My kids can't cross the light alone because the drivers do not pay attention when it is green light for the pedestrians. If we can't get crossing guards I suggest it should not be allowed to turn right on red.	1/9/2019 5:12 PM
222	Stop the constant restraints of automobile traffic and creating walking and bicycle obstacles.	1/9/2019 2:43 PM
223	Something needs to be done at the intersection of Laurel St and San Carlos Ave. to make it safe for pedestrians and motorists alike.	1/9/2019 1:04 PM
224	I live on Prospect Street and walk on San Carlos Ave. to downtown every day. The traffic on San Carlos Ave. is TOO fast and I never see any traffic control. It is dangerous for pedestrians to cross the avenue even when we have the green light. The city seems oblivious and non-responsive to this issue. When will something serious be done? The intersection of Prospect and San Carlos Avenue is one of the most dangerous in San Carlos just waiting for a horrible accident. The City Council obviously does not care but the citizens do.	1/9/2019 12:57 PM
225	Why isn't there more of a shoulder on Howard or Brittan from El Camino to Industrial? It seems like there's room.	1/9/2019 12:24 PM
226	It is of the utmost necessity that cars going eastbound down San Carlos slow down. I've witnessed at least 2 accidents on this street (and one hitting a pedestrian). Cars speed down towards El Camino, no doubt to get on the freeway. This will only get worse when more people move in with all the construction going on. It is dangerous especially at the the intersection of Prospect and San Carlos Ave.	1/9/2019 12:11 PM
227	While I do walk and bike in San Carlos, we need to balance this with the amount of traffic during peak times and not reduce lanes for cars. The city already decided to put all of those Trestle apartments which have increased cars without any infrastructure road improvements -- we cannot take away lanes because traffic is already awful during commute times.	1/9/2019 9:11 AM
228	There is no sidewalk in Dartmouth Ave, and there are a lot of kids walking to Tierra Linda, Charter and Carlmont Schools on that street every day. That is really a hazard that should be taken care before a tragedy happens.	1/8/2019 11:03 PM
229	I am concerned about road safety. When I bike or walk with my my daughter, I often feel unsafe with her. She recently almost got hit by a car coming home from school. I would like to feel confident that she will make it home safely. We need to inforce traffic safety in our city for all bikers and pedestrians. If there were projects to enforce safe biking or walking around San Carlos that would be great for my family and others.□□	1/8/2019 10:18 PM
230	I would like to see improvements for biking on Old County Road, El Camino Real, San Carlos Av and Alameda.	1/8/2019 2:40 PM
231	<p>*Cycling* A complete North / South & East / West class 2 bike lane is needed through the city. Sharrows are OK in some circumstances, but they are typically poorly aligned throughout san carlos and encourage people to bike in the door zone and/or side-swipe zone (old county / cedar / san carlos ave). As an example, the sharrows on old county north of Holly should be centered in the middle of the road as there is not enough room for a bike and car to co-exist. (I used to ride ~100+ miles / week bike commuting to SF. San Carlos lags behind other cities in safe bike routes)</p> <p>*Walking* Walking at night is deadly. Even with reflective clothing and flashlights, I have had many near misses as no one looks for pedestrians at stop signs. Some of the busier intersections should have flashing lights to indicate pedestrian presence (ex: Cedar/Brittan).</p>	1/8/2019 2:30 PM

San Carlos BPMP

232	Need to complete and link bike lanes. Walking in generally safe and easy.	1/8/2019 1:52 PM
233	Please prioritize sidewalks heading too and from our schools, especially on the busy streets!	12/11/2018 5:26 PM
234	I'm concerned about spending resources on improving bike and pedestrian infrastructure when basic traffic enforcement seems to be lacking and public frustration about congestion and narrow roads is very high.	12/7/2018 11:53 AM

Input Map Comments

Input Map Comments	
Submission Time	Comment
10/19/2019	Isn't the improvement at Phelps slated to be a stoplight? Please make sure it is set as a sensor for Phelps traffic at non-rush times (the way the new light at Ralston & South is, as opposed to the awful stoplight at Holly & Laurel that seems to stop all traffic dead.)
10/19/2019	What is really needed are protected bike lanes along the length of San Carlos Ave. This would greatly increase the number of students who would bike to school.
10/20/2019	Traffic is going too fast for bikes, but the lack of a shoulder is a huge part of the problem on this stretch of road (from where Alameda Ts in up to Ralston.) And then please work with Belmont to make it possible to ride through town.... because all transit is regional anymore. Thanks!
10/23/2019	The stoplight planned at Phelps Rd is illegal. There are more than several reasons first of which is that it will be placed over 9 feet into, over 5 feet into our yard from the public right of way at the expense of a beautiful live oak that is older than the city. I have asked repeatedly that this be stopped before this illegal action is performed. Sadly, it appears I am going to be forced to take the whole thing to court unless the city pulls the plug right now.
10/26/2019	My two sons and I would love to bike to TL/Carlmont Highschool but we currently do not because we fear for our lives riding a bike on San Carlos Avenue. Making San Carlos Avenue a safe bike route to be able to commute to schools would be a huge improvement.
11/7/2019	Add a two-lane bike lane for east and west travel that is segregated from vehicle traffic with a concrete curb
11/18/2019	No stop signs.
11/22/2019	Could there be a break so cars can make a left out of Phelps onto San Carlos Ave? Currently it's hard to get across since the cars line up right there. This would reduce some Phelps traffic and avoid a stop light. It'd be great for cars that really just want to get into the right lane and make a right on Alameda.
10/23/2019	As a property owner at this location - A common issue at this location is that cars turning do not completely move into the turn lane. This causes cars behind them to cut into the bike lane to get around the turning car. If the bike lane was protected (with a concrete curb) as it went around the corner, cars would be unable to cut into the bike lane. Another extremely common issue is that cars speeding out of downtown drift into the bike lane when going around the corner.
10/24/2019	Very dangerous on the West bound side - blind curb and cars going fast and into the bike lane

Input Map Comments	
Submission Time	Comment
11/7/2019	Add a two-lane bike lane for east and west travel that is segregated from vehicle traffic with a concrete curb
11/19/2019	Would be great to add a crosswalk that has blinking lights. Very hard to see in both directions at certain times especially with sunrise and sunset.
10/20/2019	depends on what it is - the crossing is okay, but then after that there's no shoulder and it's uphill and twisty. Bikes need space or figure out a different route. This is on our Do Not Ride list.
10/23/2019	The natural crossing for most bikes and pedestrians is NOT along San Carlos Ave across Alameda... instead most bikes and walkers opt to use Carmelita (safer and quieter!) so the crossing you need to "fix" is Carmelita crossing Alameda De Las Pulgas for east-west movement from suburbs to downtown and back.
10/23/2019	The pictures do not really tell us much as to exactly what will be done. The images and descriptions seem good, but it tells us nothing as to what is going to be done at our intersection. Sidewalk improvement is fantastic. What is going to be done about the intersection and the excessive speed on SC Ave?? Have you seen the improvements on Brewster in RWC? Very smart with the speed bumps. That is a busy street as well, but they have been able to slow the traffic down successfully without fail.
11/2/2019	Are there pictures here? People keep referencing photos which are not visible on the website
11/2/2019	Removing any of the islands from this intersection will bring back the high speeds that terrified pedestrians. This was one of the few improvements in the past couple decades which should be left alone
11/7/2019	Add a curb to separate bike traffic from vehicles with something more robust than paint
10/26/2019	"My two sons and I would love to bike to TL/Carlmont Highschool but we currently do not because we fear for our lives riding a bike on San Carlos Avenue. Making San Carlos Avenue a safe bike route to be able to commute to schools would be a huge improvement.
11/3/2019	Removing the islands from this crossing will increase vehicle speed and make it significantly more dangerous. I fear it may take a terrible tragedy for the city to understand this because they have forgotten the history of this intersection
11/7/2019	Add a two-lane bike lane for east and west travel that is segregated from vehicle traffic with a concrete curb

Input Map Comments	
Submission Time	Comment
11/8/2019	Side walk on San Carlos is very narrow at this intersection - particularly on San Carlos Ave between Alameda de las Pulgas and Phelps where the right turn car lane brings the cars very close to side walk,
11/22/2019	Agreed- we'd love our Arundel kindergartener to be able to bike to school, but the sidewalks are bumpy and the road is way too scary.
10/20/2019	please also put route finding to direct bikes OFF the Alameda, which is not good bike infrastructure! (too many lane changes and merges)
11/8/2019	there is no ADA accessibility. Kids go into street to avoid potholes at this intersection
10/23/2019	The timing and relation between traffic signal and pedestrian signal is confusing and long. There is a long time when traffic has green and seems that there should be a pedestrian green too
11/18/2019	With the disastrous change of light operations at Laurel and Holly, more people now try to turn left to make it through the El Camino light. Problem is, San Carlos light is not sequenced properly and now with the new light at Cherry it's too many lights too close together that are all not synchronized properly. This is a mess.
11/20/2019	Careful planning is needed before too many lights/stop signs are added on Brittan too close together. Unless all stop signs are replaced with traffic lights and have them all synchronized properly these new changes will only make the traffic worse and cause more stress to both commuters and cyclists/pedestrians.
12/13/2019	There is really bad bikeability here in both directions to the underpass across El Camino.
10/22/2019	This is an extremely dangerous crossing due to the heavy traffic on Brittan. Motorists are not inclined to stop and look for pedestrians because it is not well marked as a crossing. To cross, pedestrians are usually left with no choice but to walk INTO the intersection, and make their presence known - and hope the cars stop.
10/23/2019	Heavy traffic due to Crestview/Brittan being used as alternate to Edgewood/Alameda route. Add traffic speed monitor/feedback light to discourage speeding up center lane on north bound Crestview. Need traffic light at intersection of Crestview and Edgewood. Unsupervised heavy traffic is dangerous.
10/26/2019	A four-way traffic light would be better, at Brittan and cedar.

Input Map Comments	
Submission Time	Comment
11/6/2019	Cars drive too fast and are often unprepared to stop. I have heard of stop lights that come on only if someone is speeding--it actively discourages speeding by providing a disincentive (have to stop if they do).
10/20/2019	Crestview is NOT rideable for the majority of people!! Neither is Club drive. Please spend resources on more practical routes (in the flatlands....) No one is going to ride here. Have you been to any of these streets??
11/18/2019	After living on Brittan for almost 25 years I wonder why it has taken so long to construct a sidewalk connecting Brittan to Alameda. I strongly support this proposal especially as the number of pedestrians as well as vehicles have increased.
11/19/2019	This is not for bike riders necessarily, but for pedestrians. There are many dog walkers and kids crossing throughout the day and at school hours to catch the bus. On weekends, the fields at Crestview Park are used heavily. Please, we need some safer crossing on Crestview further up, near Crestview Park and also near Melendy.
10/20/2019	maybe also some way-finding would be good here.
10/24/2019	This would be great! Crossing here as a pedestrian is quite dicey.
10/23/2019	Cherry crossing El Camino... and similar bike throughways should have a BICYCLE HEAD START at the light when bikes are present. IE not a pedestrian button, but a 15 second head start so bikes can clear the intersection first when bicycles are present.
10/24/2019	Not sure what "pedestrian beacon" means. I thought there was a full traffic signal here now. Why would we take that out? If "pedestrian beacon" is a flashing red light at a sidewalk, drivers in this area don't seem to know what those mean. They added one in Redwood City on El Camino, and cars just sit there indefinitely even when the pedestrians have finished crossing.
11/18/2019	Isn't this already a light. Can we remove the light and just have the beacon instead? Too many lights and too many stops signs now around the downtown area.
10/19/2019	When this light was a stop sign, it was annoyingly inefficient. Since it was turned into a normal stoplight with a timer, it has been causing worse traffic. If it's going to be on a timer, you could at least match it to the light at El Camino. It is exceedingly frustrating to see that light be red for all of the cars at the intersection while the green is active for an empty approach.
10/24/2019	This traffic signal has already been an improvement over the stop sign, but it's not timed correctly to match El Camino. Correcting that would be very helpful.

Input Map Comments	
Submission Time	Comment
11/18/2019	Too many signals too close together and not properly synchronized. Consider removing the left arrow or limit the times to help east/west flow and shorter light times.
10/21/2019	There should be speed bumps within 60 feet of this intersection. There should be speed bumps or raised crosswalk within the intersection. Neighbors on this block have had homes and parked cars crashed into.
11/18/2019	If this means a separate overpass for walk/bike, then yes! Otherwise, crossing Holly overpass with cars is too dangerous as is, then I'd say no. There is no improvement over this overpass that will be safe as long as cars and bikes are crossing each other.
10/24/2019	Didn't even know this existed. Cool!
11/14/2019	I agree with the suggestion of a stop sign on Brittan at Hewitt. Living on Brittan is like living on a freeway. It is almost impossible to back out of the driveway at times with drivers speeding by.
11/1/2019	The lack of a bike lane on San Carlos Ave between Club and Beverly makes for a particularly hair-raising ride. Despite signs urging others to share the road, motorists pass dangerously close, speed past and even swerve wildly into the opposite lane to pass. Any improvement would be welcome.
11/7/2019	Let's make this a Class IV bike lane with the cars parked away from the curb and a bike lane between parked cars and the sidewalk.
10/28/2019	Very hard to select these proposed bike lanes in the UI. So, more people might want to be liking them than you are going to get counts for.
11/2/2019	For cars driving on San Carlos Ave, I suggest 'No Left Turn' onto Laurel Street at the intersection with Laurel. As it is, cars stop at the light in the left lane, and then when the light turns green, they try to turn left. However, there is so much oncoming traffic the other way, that they just block their lane until the light turns red again. It occurs in both directions. It's not a sensible place to try to turn left, and other intersections nearby have a dedicated left turn lane.
11/18/2019	Having a dedicated bike lane, through downtown, and out to an overpass for 101 is a great idea! Please, everywhere possibly let's do Separated Bikeways, or Buffered as the second alternative. NEVER those ridiculous bike boulevards, they are too dangerous.

Input Map Comments	
Submission Time	Comment
11/19/2019	A cross walk is needed at the corner of Crestview and Melendy for children to reach the SamTrans bus stop on Melendy from the west side of Crestview Drive. This is a high traffic area, with cars often exceeding the speed limit, and not yielding to children waiting to cross. Even cars turning left from Melendy onto Crestview don't stop to allow children to cross!
11/5/2019	Just FYI, this is a terrible corner if you're trying to turn left from Cherry onto Cedar. There are always lots of cars and trucks parked along Cedar so you can't see if any cars are coming until you're in the middle of the street. It's bad for cars and bikes. I suggest extending the "no parking" zone to give better visibility. Thank you.
11/18/2019	Please, either make it a bike lane, or don't. But, do not mix cars and bikes on the road thinking it's helpful, it's only dangerous.
10/24/2019	No one-way streets
10/24/2019	No one-way streets
11/2/2019	What route is this? It is a parking lot at a small church.
11/2/2019	What route is this? It is a small church parking lot.
10/24/2019	Cedar should be improved for biking, not just pedestrian as it is a major route for kids to school. A major way to improve safety is to eliminate some of the parking, especially near corners
10/24/2019	Cedar should be improved for biking, not just pedestrian as it is a major route for kids to school. A major way to improve safety is to eliminate some of the parking, especially near corners"
11/6/2019	Sharrow placement puts cyclists at risk for getting "doored" as they are placed too close to parked cars. Either get rid of the sharrows or place them further away from parked cars so cyclists are encouraged to "take the lane".
11/19/2019	Curb extension, Shade Trees, Pedestrian scale lighting would increase visibility for pedestrians, bikers and pets day/night. Decreased width car lanes=decrease speed. Sometimes cars are backed up heading Northbound on Cedar from Arroyo all the way down the block past Arbor School. This block of Cedar between Arroyo and Brittan is CRITICAL for safety. Visibility is very poor for children headed to 4 schools Central, Arroyo, SPNS preschool, Arbor Bay School. Excessive speed, common on this block.
10/22/2019	Crossing improvement. Suggest a four-way stop with additional crosswalk across Cordilleras.

Input Map Comments	
Submission Time	Comment
10/23/2019	Traffic calming measures (speed humps, etc.) would be very helpful on the stretch of Laurel between Hull and Bush/Magnolia. In particular, between Oak and Bush/Magnolia, the road narrows, curves, and rises to a slight hill with reduced visibility, but there is nonetheless a high volume of car traffic cutting through off of El Camino at high speeds. Adding speed humps to reduce non-local car traffic (and/or slow it down) would greatly increase bicycle safety along this route.
10/23/2019	I agree that speed humps on Laurel between Hull and Bush/Magnolia would be a greatly improve bicycle and pedestrian safety along this road.
10/24/2019	No one-way streets
10/24/2019	There are always road hazards on Old Country Road, like peddles, dirt and sometimes pretty large rocks. Could you make sure the industrial businesses are responsible for their mess and keep the bike lanes clean?
11/6/2019	Horrible road to bike on, but no other viable North / South options. It is not possible for a car and cyclists to safely share the lane in sharrow-land. Cyclists are at risk of being doored (NB) or side-swiped (SB) based upon their current orientations. Sharrows should be adjusted further away from the parked cars / Caltrain wall so cyclists are encouraged to "take the lane".
11/7/2019	Many cyclists commute along old country road. Need a protected bike lane that is well paved.
11/19/2019	Warwick is a narrow road that Sequoia H.S. students travel by bike, car, student drivers, it is highly traveled narrow street that cars often need to pull over to let opposite cars go by, where does that leave bikers?
10/24/2019	Please consider "no right turn on red" Devonshire during school hours. Drivers are too quick to turn and don't notice people crossing! I've almost been hit numerous times by drivers only looking for cars coming down the hill.
10/28/2019	This whole section needs proper sidewalks on both sides, or at the very least, a real sidewalk on one side all the way from downtown to Beverly.
11/13/2019	Need a 3 way stop sign at Brittan and Hewitt
11/14/2019	Need a 3 way stop sign at Brittan and Hewitt
10/19/2019	Would absolutely *love* to be able to take Carmelita through to Cedar on bike rather than having to deal with San Carlos.

Input Map Comments	
Submission Time	Comment
10/28/2019	This path was closed approximately 20+ years ago. A neighbor of the path who still lives in the cul de sac recalls a steady stream of cars coming down the street to use as a drop-off, speed of cars was an issue. Path was a long tunnel; issues that occurred included property being set on fire, a homeless person living in tunnel, fights occurring and partying occurring at any hour. Broken beer bottles are still found after 20+ years. Reopening the path is a city liability.
11/1/2019	PG&E gas lines run under the creek next to the path and when trees come down, rupture of the lines is a serious risk especially with the current CA wildfire situation. Rupture of the lines has previously occurred due to a downed tree. The path was closed in 1990 due to arson and accidental fires. I strongly oppose the re-opening of this path as it is a threat to public safety and to my neighborhood.
11/2/2019	Wow, I had no idea this existed back when. Would be an interesting addition. I think it is worth investigating the pros/cons. Other places in the city have cons but we deal with them because we accept them there. I deal with cut through traffic in my neighborhood because the street exists. Should the street be closed to solve that? I don't think so. We each have to give something in our areas to make the whole better. Ultimately, this may not make sense to add. But it is worth investigating.
11/7/2019	Reopening (and perhaps redesigning for safety) would be a huge win for kids walking or biking to Central and Arroyo The only alternative is SCA or over the hill. I can imagine parent drop-off was an issue in the 80s because all of San Carlos attended Central. But no more -- the kids needing this route live only a couple blocks from it.
11/20/2019	Definitely need to reopen this path. PG&E issues are non-issues and in fact with open access way will make it easier for PG&E and others to monitor/access if needed. Given the amount of traffic that is now on San Carlos Avenue this path is sorely needed for the safety of pedestrians. This seems to me like the perfect example of building safe pathways throughout San Carlos. Plan to open this path now and investigate the safest way to do so.
11/20/2019	I appreciate the comment that the path was closed 20+ yrs ago but that is exactly the purpose of this survey. San Carlos has changed significantly in the last 20 yrs. The traffic on San Carlos Avenue is now too dangerous for young children and families when we can reopen paths such as this one. I think the bigger city liability is if one of our residents is killed while trying to navigate the traffic on SCA vs. safely arriving downtown or to the school. I agree we should reopen this path

Input Map Comments	
Submission Time	Comment
11/20/2019	Didn't realize there was a tunnel here. If the concern is a tunnel, it seems like this could be an open pathway along the creek. Worth investigating and reopening. Would be great to walk and bike to downtown on this route.
11/21/2019	This is fantastic!!!
11/21/2019	My family would love this.
11/22/2019	This is a great example of San Carlos building safe routes. I like the other suggestions on this map but I believe the ones that will be the most impactful and truly building safe routes will be things like this where we can completely separate automobile traffic from bicycle and foot traffic. I don't know many other areas around the city where this could happen but thinking to the future where auto traffic is only going to increase throughout the city, we should find more paths like this.
12/9/2019	Re-opening this path would be a bad idea. It was closed 20+ years ago for many reasons. One of the most important was the safety of the kids using the path. It got so bad that parents wouldn't let their kids use the path without an adult going with them. The city, school and community tried many things for years to resolve the situation, but had to eventually give up and close the path. The same issues will come up again if it's reopened.
12/9/2019	Wow, I can't believe anyone who knows the history would even THINK about re-opening this path. There were knife fights, drug use, vandalism, under-age-drinking, fires, and more. This is not an exaggeration - there are public records. There was much debate and everyone tried everything, but it was hopeless and the city closed it. Are you going to try and tell us that kids are that much better today than 20 years ago, so we won't have the same problems? Keep our kids safe - keep this path closed!
12/9/2019	The easement runs through our backyard, and 20 years later, we're still finding broken beer bottles all along the path. There used to be a garden shed on our side of the fence, but some kids set it on fire and it's not there anymore. They tried lots of things back then but couldn't keep it safe. We don't need to relearn old lessons. We are strongly opposed to reopening this path.
12/9/2019	Bad idea! For those who don't remember what this was like, the homeowners on both sides built fences along the path. So, we had a 300-foot curving tunnel, and you can't see the middle from either end. The same thing would happen again (or hundreds of people will be running through people's backyards). So that's where the trouble happened. Fights, graffiti, fires, drinking, you name it. Keep it closed!

Input Map Comments	
Submission Time	Comment
12/9/2019	Someone here suggested "redesigning for safety". This path goes through people's backyards (including mine). What are you proposing? Taking away parts of people's yards? If not, the path is going to look exactly like it did 20 years ago. Do you want the city to put a path through your backyard where hundreds of people will be using it every day? And kids will be going through it at night? How do propose stopping that?
12/9/2019	Bad idea. Caused all sorts of problems way back when, why go through all that again?
12/9/2019	If there were so many problems when the path was open before, won't the city be liable if they reopen it and the same things happen again? Doesn't seem like a very smart idea.
12/9/2019	At first this seemed like a good idea, but after reading through these comments, I didn't realize all the problems they had before. I can see those happening again. We should probably leave this the way it is.
12/9/2019	I have lived on the corner of Carmelita and Cordilleras for 30 years. I remember when the path was open and it only caused troubles, vandalism, etc. for us (mail stolen out of our mailbox, mailbox trashed, etc.) While I appreciate the general public's wishing it would open again, I believe the opinions of the San Carlos homeowners who live near the path should take priority. We have strongly disagreed with the re-opening of the path!! Thank you for the opportunity to comment.
12/9/2019	According to March 1990 City Council meeting minutes, the City abandoned its pedestrian easement at the recommendation of the police, fire departments, pub works & the City attorney. There were many incidents of arson, accidental fires, criminal behavior & accidents between pedestrian and cyclist. There were 5 years of public hearings & many attempts to resolve the issues. The path was about 1,000 ft long, 8 ft wide with 6 ft fences on each side. Re-Opening would expose the city to liability.
12/10/2019	There is nothing "safe" about this path if it were to be reopened. I agree with the comment that the homeowners should have their safety take priority on this decision. You can't stop people from tossing beer bottles & burning cigarette butts or having strangers approach your children over the fences. In addition, it empties onto Orange which is a blind curve and there were constantly near accidents when bikes came off the path. I feel this section should be removed from your plan- Thank-you.

Input Map Comments	
Submission Time	Comment
12/10/2019	One of the other commentators suggested: "If the concern is a tunnel, it seems like this could be an open pathway along the creek." But the path is NOT along the creek. It runs between people's backyards. If the city re-opens the path (bad idea), I guarantee you the homeowners will put fences back up (max height, closed fences) Those fences will be on private land, so you or the city can't tell the homeowners they're not allowed to protect themselves and protect their own privacy.
12/10/2019	My property is at the cul-de-sac of Carmelita Drive. I am opposed to the reopening of the Postal Path for safety reasons. The path was closed in 1990 for numerous safety concerns including: lack of access by police and firetrucks due to narrow walkway, vandalism, and illicit activities. For these reasons I am against reopening the path.
12/15/2019	Both the Fire Department and the Police Department recommended closing this when it was open before. Crime, fires, and safety problems. Anyone familiar with the history of this path would advocate against it.
12/16/2019	This was closed in the past by the city council or the following reasons: Public safety (Fights, Drugs, Sexual delicts) Expected high initial investment and ongoing cost Fire risk Vandalism Wildlife impact Trash None of these conditions or risks have changed. We, the Mirabel Place HOA, hence oppose reopening the path.
12/17/2019	This path was closed nearly 30 years ago due to ongoing crime including arson, assault, vandalism, theft, pet poisonings and more. Reopening the path places the entire SC community at risk for fires and other public safety hazards. We strongly oppose reopening this path.

Input Map Features			
ID	Geometry	Likes	Dislikes
143	Point	30	3
144	Point	10	0
145	Point	11	0
146	Point	20	1
147	Point	21	1
148	Point	10	0
149	Point	12	0
150	Point	6	1
151	Point	20	1
152	Point	12	1
153	Point	19	2
154	Point	18	1
155	Point	21	3
156	Point	9	3
157	Point	4	1
158	Point	4	0
159	Point	9	1
160	Point	13	4
161	Point	8	2
162	Point	14	0
163	Point	13	1
164	Point	18	1
165	Point	6	2
166	Point	15	0
167	Point	7	0
168	Point	8	0
169	Point	16	0
170	LineString	1	0
171	LineString	5	0
172	LineString	2	0
173	LineString	0	0
174	LineString	1	0
175	LineString	1	0
176	LineString	0	0
177	LineString	2	0
178	LineString	1	0
179	LineString	1	0
180	LineString	0	0
181	LineString	1	1
182	MultiLineString	1	0
183	LineString	0	0
184	LineString	0	0
185	LineString	1	0
186	LineString	0	0

Input Map Features			
ID	Geometry	Likes	Dislikes
187	LineString	0	0
188	LineString	0	0
189	LineString	0	0
190	LineString	3	0
191	LineString	1	0
192	LineString	2	0
193	LineString	3	0
194	LineString	1	0
195	LineString	0	2
196	LineString	2	0
197	LineString	0	0
198	LineString	2	3
199	LineString	4	2
200	LineString	0	1
201	LineString	6	3
202	LineString	1	2
203	LineString	3	2
204	LineString	1	0
205	LineString	1	0
206	LineString	0	2
207	LineString	0	0
208	LineString	1	0
209	LineString	2	0
210	LineString	1	0
211	LineString	1	0
212	LineString	2	0
213	LineString	1	0
214	LineString	3	0
215	LineString	4	1
216	LineString	1	0
217	LineString	0	0
218	LineString	0	0
219	LineString	3	0
229	LineString	2	1
238	LineString	16	37

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/8/2019	Route	Getting to and from Caltrain Station by bike is challenging. Old County Road has only intermittent bike lanes and car traffic is fast. Recommend adding lanes along Cherry or Arroyo to connect main routes on Cedar etc. to ECR.	4	0
1/8/2019	Route	Crossing 101 is very challenging to get to the Bay Trails. Only safe option is to use the Ralston ped bridge. Another ped bridge at San Carlos/RWC edge is needed. Expensive, but many businesses on both sides of the freeway would benefit.	2	0
1/8/2019	Route	Need to complete bike lanes along San Carlos Ave all the way to Carlmont to protect the High School commuters	2	0
1/8/2019	Barrier to Walking	On West side of Cedar, there are very large curbs here than need to be turned into ramps.	5	0
1/8/2019	Barrier to Walking	On West side of Cedar, the sidewalk disappears near this intersection and makes it difficult to continue walking along Cedar. The sidewalk should continue over land triangle.	1	0
1/8/2019	Barrier to Walking	There needs to be a curb ramp here	2	0
1/8/2019	Route	There should be a bike lane here (preferably parking protected)	2	0
1/8/2019	Route	Bike lane here should be parked protected to be safe from fast moving traffic	2	0
1/8/2019	Other	Continue the Redwood City Class III bike route from Warwick Street and link through Stanford Lane and	7	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
		Eaton Avenue to the San Carlos Cedar Street Class III bike route.		
1/8/2019	Other	Caltrain	2	0
1/8/2019	Barrier to Walking	Path to school without sidewalk (I am very concerned by seeing kids walking to/from Tierra Linda, SCCLC and Carmont schools on the road while cars rush down the hill)	4	0
1/8/2019	Barrier to Biking	Very dangerous to cross the 101 by bike. I tend to use the Belmont pedestrian bridge although it adds a considerable detour to my route. It would be awesome to have a similar bridge in San Carlos (ideally between E. San Carlos Ave and Monte Vista Dr to benefit people who use Caltrain)	26	0
1/9/2019	Route	Sidewalks should be on both sides of street to promote walking path on Los Vientos. One side is obscured by brush and the city never maintains it -- it could be a nice walking path towards Pulgas Open Space area.	0	0
1/9/2019	Route	I would love to see a bike lane on El Camino!	6	1
1/9/2019	Barrier to Walking	This intersection is very dangerous for pedestrians! Cars have to yield left turn on green and often don't see pedestrians or are rushed to turn (and cut off peds) because of the high traffic volumes.	8	0
1/9/2019	Other	Need the safety of this intersection to be prioritized. Pedestrians are not safe here; near-misses too numerous to convey.	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/9/2019	Other	Walking/biking eastbound can be dangerous with cars going so fast down San Carlos Ave. There needs to be either a stop sign or some slow-down method for cars as they are approaching the busiest area (with adult center, shopping) of San Carlos. My husband and I walk all the time and see horrible speeding...	12	0
1/9/2019	Barrier to Walking	This intersection needs pedestrian safety treatments. Autos move very quickly through this intersection without much regard for pedestrians. Suggest speed bumps or something to slow them down. Also, the pedestrian signal/walking man should illuminate with *every* light cycle. There is no reason to require pedestrians to walk out of their way to push the button - there are *always* people crossing there. It's just unnecessary and doesn't feel welcoming as a pedestrian.	8	0
1/9/2019	Other	We don't have continuous sidewalk on Brittan from Hewitt to Alameda de Las Pugas which means it is very risky for us to walk with kids as we have to walk on the road and cars drive much faster than the posted speed limit of 30mph. Please get us a continuous sidewalk on one side of the road.	12	0
1/9/2019	Route	safety must come first	0	0
1/9/2019	Other	Please install a "pedestrian scramble" where Laurel St meets San Carlos St (Starbucks and Blue Line and Chinese restaurant on corners)	3	0
1/9/2019	Other	San Carlos Ave needs more sidewalk/bike path to feel safe walking and biking.	4	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/9/2019	Barrier to Walking	While this is on the San Carlos Redwood City border, there needs to be partnership between the cities on improving this sidewalk section of Alameda between Eaton and Edgewood. There is no sidewalk on the North-East side of the street, and trees overhand the South-West side in addition to it being extremely narrow. Ideally there would also be bike lane considerations added for the Edgewood Rd commuters.	8	0
1/9/2019	Barrier to Walking	Please add at minimum a crosswalk with pedestrian warning lights here	1	0
1/9/2019	Route	Please re-open the old "postman's path." This would give access for a whole neighborhood to Central Middle and downtown. It's much shorter and flatter than alternates and much safer than San Carlos Ave.	5	0
1/9/2019	Route	Very dangerous place to walk. Needs a continuous sidewalk on at least one side.	2	0
1/9/2019	Route	Biking westbound on San Carlos Ave, it's very dangerous to make the left turn on to Cordilleras -- uphill (so you are slow) plus a curve and, in the afternoon also shadows. Cars cannot see you trying to get to the left lane. Need some safe way to make this turn.	2	0
1/9/2019	Route	Need a bike bridge. The bike lane over Holly is crazy dangerous, between 2 lanes of merging traffic.	8	0
1/9/2019	Barrier to Walking	Need to slow traffic on Orange where there are no sidewalks but lots of foot traffic. This is especially true of parents driving their kids to Central.	4	0
1/9/2019	Other	Town restaurant in downtown	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/9/2019	Other	Vivace restaurant	0	0
1/9/2019	Other	Waterdog restaurant	0	0
1/9/2019	Other	Seiya Restaurant	0	1
1/9/2019	Other	Bianchini's grocery store	5	0
1/9/2019	Other	Lunardi's grocery store	0	0
1/9/2019	Other	Reading Bug bookstore	0	0
1/9/2019	Other	Blue Line pizza	0	0
1/9/2019	Other	Retail and restaurants in downtown area.	0	0
1/9/2019	Other	Home Depot	0	0
1/9/2019	Other	REI store	0	0
1/9/2019	Other	Belmont via San Carlos Ave.	1	0
1/9/2019	Other	Redwood City, the peninsula, Woodside, Portola, and the coastal hills	0	0
1/9/2019	Barrier to Walking	Sidewalks switch between being on the North and South side while walking on San Carlos Ave.	11	0
1/9/2019	Barrier to Biking	Cars parked on either side of the road cause an unsafe narrow stretch for biking with close proximity to oncoming traffic and no way for cars behind to overtake.	1	0
1/9/2019	Other	During commute time in the morning this is a dangerous intersection. Even a four-way stop isn't sufficient for safe crossing.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/9/2019	Route	Very active walking, biking and scooting routes from the White Oaks area to schools.	1	0
1/9/2019	Route	These are busy streets for kids going to and from school.	2	0
1/9/2019	Other	City hall dog park	2	0
1/9/2019	Other	San Carlos Library	3	0
1/9/2019	Barrier to Biking	Going north on Alameda to turn left on Brittan - light does not change automatically for cyclists to allow them to turn left. Dangerous to get in left turn lane due to traffic and uphill. Going West on Brittan - steep hill while crossing intersection, difficult to get going and many cars turning right.	1	0
1/9/2019	Barrier to Walking	No sidewalk on north side of Brittan, lots of vegetation and hills make it hard to see pedestrians, no crosswalk to get to the other side when the sidewalk ends.	9	0
1/9/2019	Barrier to Biking	No bike lane on El Camino - I suggest an elevated bike roadway that parallels tracks and avoids all traffic signals.	4	1
1/9/2019	Route	More over or underpasses to get to old county rd (which is much safer to ride than El Camino).	1	0
1/9/2019	Route	An overpass to get to the bike trail on the other side of 101 would be great.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/9/2019	Route	People walking from the new apartments on El Camino north of Holly into downtown have to cross ECR and Holly, both of which are long intersections and look poorly protected. I anticipate this intersection to become busier with pedestrians (I haven't attempted it personally) so the city should consider what improvement opportunities there are at this intersection for pedestrians.	0	0
1/10/2019	Other	my kids bike down San Carlos ave to get to school	1	0
1/10/2019	Route	There is no bike lane along this street and my son travels it to school	0	0
1/10/2019	Barrier to Biking	This intersection is hard to navigate on a bike	11	0
1/10/2019	Barrier to Walking	There is no crosswalk on Cordilleras (not even at San Carlos Avenue), the closest one is at Elizabeth Street. Anyone from our neighborhood trying to walk to downtown or Central Middle School, has to dodge traffic on Cordilleras to get across. If we could have a painted crosswalk at Carmelita, that would help?	7	0
1/10/2019	Other	Cuisinett	6	0
1/10/2019	Other	La Cornetta	4	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/10/2019	Barrier to Walking	I work at the transit district (1250 San Carlos). This intersection is a barrier to walking because of the unprotected left turns which frequently put vehicles in conflict with people walking in crosswalks. I myself have almost been hit by left-turning vehicles several times while crossing legally. I think, at a minimum, protected left turns would help to prevent left-turn/ped conflicts. Other improvements to be considered should be leading pedestrian intervals and enhanced crosswalks.	5	0
1/10/2019	Other	We would like to be able to bike to the Hiller Aviation Museum from San Carlos South Cordes Area	7	0
1/10/2019	Barrier to Biking	It is very difficult to bike over the 101 to get to the bay trail or the Aviation Museum	14	0
1/10/2019	Route	Stop signs on Elm at Olive would protect both pedestrians and cars turning onto Elm. There's a blind curve between Arroyo and Olive. Cars turning left from Olive onto Elm can be partway into the intersection when a car speeding north on Elm appears. Pedestrians trying to cross Elm sometimes wait several minutes before there's a break in the north- and south-bound traffic. Even then, a car speeding north may appear around the blind curve.	0	0
1/10/2019	Route	Real sidewalks are needed on both sides of San Carlos Ave, so that walkers are safely separated from cars, particularly between Beverly Dr. and Cedar St. It is scary to walk this portion, especially with a stroller. It also seems unconscionable that kids trying to walk to Arundel do not have adequate and safe sidewalks around the school.	4	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/10/2019	Barrier to Biking	The inconsiderate Bicyclists are constantly running stop signs, going in the middle of the streets which Cars can't get around-throughout San Carlos...E. San Carlos Ave., Old County, Brittan, Laurel St., El Camino Real...just to name a few. REDUCE the Bike Lanes!	0	11
1/10/2019	Other	We need a crosswalk across Alma at Cordilleras and across Carmelita at Cordilleras and across Cordilleras at Carmelita. This is a safe route to school and it is a very dangerous place to walk, bike, or scoot.	3	0
1/10/2019	Barrier to Walking	Right around here, the sidewalk on the north side of Brittan ends, forcing pedestrians to cross the street right at the bottom of a hill which cars can't see over. Incredibly dangerous. We want to walk downtown, and let our kids walk downtown, but this is just so dangerous. Please fix this!!	8	0
1/10/2019	Barrier to Walking	This block has narrow sidewalks which are often blocked by cars parked far onto sidewalk, forcing pedestrians with strollers or walkers onto the street. please enforce the parking rules here.	5	1
1/10/2019	Route	This block was a problem area for bikes, even before the Jan 2019 home construction and large vehicles that regularly park here. Traffic calming bumps on St Francis Way pushed auto traffic to Eaton. Eaton is narrower and has more curves. This block is heavily traveled by bikes. Also, Warwick needs to be repaved by the stop sign - cars encroach on bikes trying to avoid asphalt bumps. This is also the terminus of the peninsulabikeway.com route that apparently doesn't include San Carlos.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/10/2019	Barrier to Walking	The sidewalk it too narrow here for the number of kids that use it for walking and waiting for the bus. At peak bus waiting times, kids are spilling onto street. make the sidewalk wider here.	3	0
1/10/2019	Barrier to Walking	Do we really need a fence here?	4	0
1/10/2019	Barrier to Walking	San Carlos cars speed; blind corner from San Carlos north into Beverly; drivers do not respect "unmarked street crossing"	2	0
1/10/2019	Barrier to Walking	Reopen this public right away - postman's path	15	0
1/10/2019	Other	walkway staircase to connect alameda and belle ave.	5	0
1/10/2019	Other	I would bike or walk to stores across El Camino from San Carlos but usually opt not to do this because of the heavy traffic.	2	0
1/10/2019	Other	I enjoy walking into town on a fairly regular basis. But, biking into town is a different story. The streets are very narrow and there are many parked cars which make it hazardous at times to bike into town. I don't have any answers for this because you really can't widen the streets. Not really sure how to solve this problem.	5	0
1/10/2019	Other	We could use a crosswalk on Cordilleras somewhere around Carmelita or Alma. There is no safe way to cross the Cordilleras at this location currently.	5	0
1/10/2019	Route	Almost all the destinations I'm trying to access are within one block of this street and crossing it is a problem	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/11/2019	Barrier to Walking	For biking + walking The intersection just "feels" unsafe: Pedestrians are right on top of the car traffic. Sidewalks are narrow. Two ugly strip-mall parking lots shove pedestrians like sardines between concrete walls & brush onto a narrow sidewalk.	7	0
1/11/2019	Barrier to Biking	There are "bike lanes" along Old County, but as a driver I do not feel safe driving with bikers on this road.	2	3
1/11/2019	Barrier to Walking	This may be irrelevant once the development on El Camino+San Carlos Ave complete, but crossing from Caltrain to downtown should be easy, and welcoming. This is a gateway to our town and should be super pedestrian friendly. Right now, it's a bit like playing Crossy Road/Frogger, where I clutch my children tightly and think of the intersection as a hurdle not a part of a nice walk	15	0
1/11/2019	Other	Caltrain station	0	0
1/11/2019	Other	Burton Park	1	0
1/11/2019	Barrier to Walking	The section of Brittan between Milano Way and Alameda does not have consistent sidewalks. This issue, combined with fast moving cars and short distance visibility due to the hills makes it dangerous to walk on the shoulder and cross the street.	6	0
1/11/2019	Barrier to Walking	crosswalk at Club/Exeter is very unsafe during school pickup/drop-off times; a sidewalk on both sides of Club between Bayview and Exeter could help so a crossing guard could be shared.	2	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/11/2019	Other	Bay trail	0	0
1/11/2019	Other	There is a postman's path here that connects Carmelita and Orange. It would be the perfect spot to add a walking/biking path so that there is easy access to the downtown area without having to walk/bike on the incredibly congested san carlos avenue. I've seen several instances where pedestrians were almost struck by cars as they crossed cordilleras and San Carlos Avenue	10	0
1/11/2019	Other	There are several unmaintained trails throughout this open space. Seems like a great spot to add more walking trails	6	0
1/11/2019	Barrier to Walking	Seems like a perfect spot to add more walking trails in this vast open preserve	6	0
1/11/2019	Barrier to Walking	Why are there not more walking/hiking trails throughout this open area. Seems like the perfect spot to add another sorely needed park with playing fields for San Carlos	2	0
1/11/2019	Barrier to Walking	There is a path here that my family would love to take to get downtown and to Caltrain. Please reopen	11	0
1/11/2019	Barrier to Biking	The southern end of Cedar is congested with parked cars and moving vehicles on a narrow street for cyclist safety. This street collects students from San Carlos on their way to Sequoia HS -would like to see a street that runs north/south with a bike lane that is safer for kids coming from the northern end of San Carlos.	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/11/2019	Barrier to Walking	Would like to see sidewalks along this stretch for pedestrian safety.	4	0
1/11/2019	Other	The Valley Rd/Sycamore/Manzanita route to downtown has no sidewalks and lots of cars going too fast.	1	0
1/11/2019	Other	There are "bike lanes" along Old County, but as a biker do not feel safe with the drivers on this road.	4	0
1/12/2019	Barrier to Walking	The main access to the new housing from the west side of ECR should be further north, maybe at Oak Street	1	1
1/12/2019	Barrier to Walking	We walked to the end of this street and there was a walking path that is no longer open, would be great to reopen it so that we can walk downtown and to Central	10	0
1/12/2019	Other	We can't walk or ride a bike across Holly Street. It is too dangerous with the traffic exiting and entering the freeway. It would be nice if we had a pedestrian bridge so we get get to redwood shores or redwood city without having to drive.	7	0
1/12/2019	Barrier to Walking	This path has been shut down, not sure why, great pathway to orange avenue and downtown	8	0
1/12/2019	Barrier to Biking	There is no safe biking route that goes all the way to the Caltrain station. Neither El Camino, San Carlos Ave nor Old County have a bike lane at the station.	5	0
1/12/2019	Route	There is no room for bikes on this part of Brittan. It should not be labelled as an "existing bikeway". It's very unsafe for bikes.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/12/2019	Other	We need a secured bike path on San Carlos avenue to go from downtown to the schools (TL & Carlmont) - Bike cannot share the same road as cars, it is way too dangerous.	1	0
1/12/2019	Other	San Carlos Ave connects to various schools and also point of interests. It will be great to have sidewalks throughout San Carlos Ave as there are many stretches with no sidewalks and no way to take kids walking to school.	4	0
1/12/2019	Other	Going to school walking with kids through San Carlos Ave is a nightmare as there are no sidewalks connectivity to schools.	5	0
1/12/2019	Route	The sidewalk on these stretches are broken or nonexistent	0	0
1/12/2019	Barrier to Biking	Re: Crossing El Camino at Cherry from the train station. The light isn't long enough for a cyclist to safely cross El Camino.	3	0
1/12/2019	Barrier to Walking	The postman's path is a great piece of San Carlos history and a wonderful way for neighborhood kids to get to Central/Arroyo. It has fallen into disrepair. Either convert it to stairs, like in Berkeley or Park City, or repave it.	2	0
1/13/2019	Barrier to Biking	Huge pothole right on the bike path. Failed to go around it at dark, and almost crashed (the hit was so hard that I broke a spoke on my wheel). Big hazard, cyclist be aware! Hope the city can fix it soon (has been over a month)	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/13/2019	Barrier to Biking	Huge pothole right on the bike path. Failed to go around it at dark, and almost crashed (the hit was so hard that I broke a spoke on my wheel). Big hazard, cyclist be aware! Hope the city can fix it soon (has been over a month	0	0
1/13/2019	Barrier to Walking	Please reopen this walkway - much more direct and pleasant route to downtown	10	0
1/13/2019	Barrier to Walking	Path from Carmelita to Orange/Cedar was closed (in the '80's?). Reopening path would provide off-road access close to Central Middle School.	6	0
1/13/2019	Barrier to Walking	Path from Carmelita to Orange/Cedar was closed (in the '80's?). Reopening path would provide off-road access close to Central Middle School.	10	0
1/13/2019	Other	Charter School, Mariposa Bridge School, Tierra Linda Middle School	0	0
1/13/2019	Barrier to Walking	Trail from Devonshire leads up to here, then there are numerous Private Property-No Trespassing signs between here and any road access. If one ignores the signs to try to hike out towards Crestview, residents on Normandy confront them and discuss calling the police. Would be a good place for an official trail without police threats.	8	0
1/13/2019	Barrier to Biking	No bike lane or shoulder at tight turn just past intersection, which cars can't readily see around- this is why I don't let my kids ride bikes to school.	0	0
1/13/2019	Barrier to Biking	No bike lane on major road within blocks of multiple schools, and no good alternative routes.	4	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/13/2019	Barrier to Walking	Open the gate to enable access to trails in the Open Space Preserve.	8	0
1/13/2019	Barrier to Walking	Open an access from Crestview, Quail Lane, or Normandy Ct to unofficial trail beyond.	6	0
1/13/2019	Barrier to Walking	Lack of sidewalk makes it sketchy to walk to Carlmont on Beverly	2	0
1/13/2019	Route	Convert Laurel St in downtown area to 1-way Northwest for cars. Cars can use El Camino to go Southeast. Shift left/West parking towards car lane. Install 2-way bike/scooter path where left parking was. Would have to weave around on-street dining, though.	1	0
1/13/2019	Barrier to Walking	Assure sidewalk dining along Laurel St leaves 36" clear path for walking, without chairs, podiums, heaters, umbrellas, or awnings interfering with pedestrians.	4	0
1/13/2019	Route	Connect existing Cedar bike route to Downtown Belmont via Laurel St/6th	0	0
1/13/2019	Barrier to Walking	Very narrow street with parking on both sides and no sidewalk results in 1-way-at-a-time car traffic and very dangerous pedestrian and bike conditions. Remove parking from one side, and add sidewalk.	1	0
1/13/2019	Other	Would be nice if there were good bike lanes between Terra Linda and Arundel	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/13/2019	Barrier to Biking	I almost got run over here by a driver who did not let me change lanes out of the bike lane (going West on San Carlos, turning onto Alameda). You have to cross a full speed traffic lane with very short notice to get into the turn lane.	5	0
1/13/2019	Other	I bike to here to get on the train.	1	0
1/13/2019	Barrier to Biking	Getting out of the bike lane onto the sidewalk is awkward. I'd much rather have a lowered curb where I can decelerate the bike and turn off the main road gradually, instead of having to come to a full stop and walk the bike out of the lane at the pedestrian crossing.	2	0
1/13/2019	Route	The bike lanes on alameda de las pulgas work. It would of course be nicer if they were protected from traffic somehow, but I'm glad they are there.	0	0
1/13/2019	Barrier to Biking	The shortcut under the train tracks to get to old country road is nice, but is not well marked, and is unpaved near F street. Also, it's unconnected to Southbound El Camino. I had to ride on the sidewalk, and then cross through the shopping center with the CVS to get the light at Hull Dr to be able to cross El Camino when using this shortcut.	3	0
1/13/2019	Barrier to Biking	Crossing El Camino by bike out of this shopping center can be tricky. We need to make sure light timings are long enough to allow cyclists through.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/13/2019	Other	I would like to suggest no right turn on red for the intersections by Arundel and TL during the drop off and pick up times. Cars roll through all the time. Red light (hawk style) crosswalk in front of Arundel on Sc Ave and the same down Sc Ave where currently there is nothing but a crosswalk. No one stops for kids there. Also, at alameda and alma. Please pay attention to this side of town for once. We like to walk and bike as well.	2	0
1/13/2019	Barrier to Biking	Biking from the train station west on San Carlos Avenue is dangerous	6	0
1/13/2019	Barrier to Walking	Agree with the other comments. This pathway would be soooooo much safer way to walk to downtown, Caltrain, etc.	4	0
1/14/2019	Barrier to Walking	Adding my comment as well - agree, my family would like to use this path	7	0
1/14/2019	Barrier to Walking	A radical suggestion: Close San Carlos Ave to cars between ECR and Walnut. Replace with pedestrian malls. Allow cross traffic but route traffic to Holly and Walnut to feed the new parking structures.	2	2
1/14/2019	Barrier to Walking	San Carlos Avenue has incredibly high traffic - need to find ways to divert pedestrian traffic off San Carlos Avenue	3	1
1/14/2019	Barrier to Walking	Unmaintained hiking trails throughout this entire open preserve but lack of clear trail markers make it less welcoming	4	0
1/14/2019	Barrier to Walking	Where are all the trailheads to identify all the trails through this open preserve?	6	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/14/2019	Barrier to Walking	Postman path needs to be reopened	5	0
1/14/2019	Barrier to Walking	Great hiking trail but needs to connect to the other trails in this area.	5	0
1/14/2019	Barrier to Walking	Hiking trails start here but don't have an exit	2	0
1/14/2019	Barrier to Walking	Vista park has several trails leading into the open area but not very clearly marked or maintained.	2	0
1/14/2019	Barrier to Walking	This map isn't accurate. Oakley doesn't connect to Club but there is a trail here that is not clearly marked or well-maintained. There is a trail that connects to vista park which would be great to maintain.	0	0
1/14/2019	Barrier to Walking	Please reopen walking/biking path here to downtown. Would much rather walk downtown than drive.	4	0
1/15/2019	Other	Access to Redwood Shores workplaces and activities.	3	0
1/15/2019	Route	Westbound on Holly feels extremely unsafe as a bicyclist at night. Traffic crosses the bicycle lane, and the road is not well lit. I often cross the street and head east on the sidewalk on the other side, but the sidewalk is narrow and there are no pedestrian signals at the crossing on-and off-ramps. The left turn signal from Holly onto Industrial is also usually not triggered by a bicycle.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/15/2019	Route	The eastbound bicycle lane on Brittan disappears after crossing Laurel St., which makes Brittan a difficult corridor for bicycle commuting. Cars do not "share the road" well on this stretch of Brittan because traffic is relatively heavy and fast. There is no sidewalk on one side, and a narrow/obstructed sidewalk on the other side. I usually take Old County Rd. to Commercial St. to bypass Brittan, but ideally Brittan would have bicycle lanes in each direction.	0	0
1/15/2019	Other	Trader Joes	0	0
1/15/2019	Other	Post Office	0	0
1/15/2019	Other	Adult Community Center	2	0
1/15/2019	Route	Very dangerous access to train station via bike. Cars constantly pulling out from perpendicular parking spaces into traffic flow	0	0
1/15/2019	Route	Hazardous to bike down this stretch without occupying entire lane. Cars pulling out of perpendicular parking spots.	2	0
1/15/2019	Route	When Black Mountain property is developed a walking/biking path should be built connect alameda to highlands park.	1	0
1/15/2019	Route	Despite the bike markers on the road this is a very dicey stretch to ride. Cars overtake dangerously all of the time.	0	0
1/15/2019	Route	Total nightmare walking or biking. Overpass should be closed to bikes until improved.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/15/2019	Barrier to Biking	Something should be done about this curve on San Carlos Ave between Sycamore St and Cordilleras Ave because cars frequently drive into the bike lane. I'd feel safer riding this stretch if there were at least plastic poles separating the bike lane from traffic.	9	0
1/15/2019	Other	Laurel street should be free of car traffic.	6	1
1/15/2019	Route	Bicycle riding from Alameda to Milano Ave is difficult/unsafe due to poor sightlines but could be immensely improved with bike lane markings on the road (both sides of Brittan Ave.). Also, if there is a way to improve pedestrian crossing at Brittan/Fairbanks, that would be helpful for many people that walk along Brittan Avenue.	0	0
1/15/2019	Barrier to Walking	No sidewalk on the north side of Brittan between Alameda and Fairbanks, or south side of Brittan between Milano and Fairbanks, and no crosswalk at Fairbanks to allow for safe transition.	7	0
1/15/2019	Route	This section of Brittan between Alameda and Milano has poor site lines for drivers, heavy vegetation affecting visibility and no sidewalks which makes it difficult for drivers to see cyclists/pedestrians and creates a situation where drivers tend to drive fast. Adding a bike lane or bike friendly markings on the road would help cyclists feel safer riding in this area. With no such markings, many children/families do not bicycle downtown or to schools due to safety concerns.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/15/2019	Barrier to Walking	Cars use this route as a back way to get to or around downtown and many do not slow down where Elm curves between Arroyo and School St. As a pedestrian, it is hazardous to cross Elm due to this blind curve at School Street. A pedestrian crosswalk, "slow - blind curve" sign to help slow down traffic, or at least a mirror to improve visibility of cars coming northward on Elm is needed.	5	0
1/15/2019	Barrier to Walking	Extremely hazardous crossing	0	0
1/15/2019	Barrier to Biking	hazardous crossing	1	0
1/15/2019	Barrier to Walking	Carmelita is used as a short cut for TL/Carlmont drive traffic, making the street dangerous for pedestrians & cyclists. Too many cars, at too high speed on a narrow road, part of which has no sidewalk	1	0
1/16/2019	Barrier to Walking	Barrier to walking and biking at Carmelita and Alameda de las Pulgas intersection. The best way to walk from western San Carlos to downtown is on Carmelita Dr (Not too busy unsafe San Carlos Ave). Dozens to hundreds of people walk to cafes, restaurants, train station, groceries down Carmelita across the Alameda. But that crossing is SUPER dangerous with no help for walkers/bikers. Going to San Carlos Ave crosswalk is inconvenient so NOBODY does that. This intersection needs safety help!!!	2	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Barrier to Walking	Current route from western SC to downtown used by hundreds... forces walkers and bikers on to San Carlos Ave. A GREAT improvement would be to open a bike path from Carmelita to Cedar along the creek. I think it used to be there. This short new route, currently ending in cul de sac, would be a dramatic improvement for walkers/bikers coming from Devonshire and Arundel and Northwest SC areas to downtown!!	4	0
1/16/2019	Route	Frequent commuter and recreation route is through San Carlos to downtown, through train station onto Old County Road (which needs segregated bike route and intersection help!!), into Belmont turning right on street just north of Ralston and then onto bike overpass at Ralston/101 to get to northbound bay trail.	2	0
1/16/2019	Route	Many of us go to cafe's, restaurants, and groceries (like to Trader Joe's on El Camino) via downtown and Laurel St. The Carmelita/Alameda intersection is terrible for bikers/walkers. San Carlos Ave and Laurel St are a menace to bicyclists, which prevents MANY residents from biking instead of driving. San Carlos Ave needs physically separated Bike Lane & Laurel should be one way south or segregated bike lane protected by parallel parked cars... or similar. All of West SC uses this bike route.	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Route	This is the route used by many/most cyclists going from San Carlos on the Alameda to Emerald Hills RWC and into Woodside toward Skyline and the coast. This is used lots weekdays but even more on weekends from recreation. Bike lane on the Alameda is "ok" - should be physically separated with better intersections for bikers. Should be dedicated/easy!	0	0
1/16/2019	Route	Key route from N-S San Carlos into Redwood City downtown (movies, restaurants, farmers market, Kaiser medical) and also to Regional Peninsula Bikeway into Atherton/Menlo Park to Mountain View etc. These are the side streets we use in practice. Not enabled or helpful for bikes at all but quieter. How do we make this route ATTRACTIVE/Safe/Easy?	2	1
1/16/2019	Route	Missing route!! How do we get across San Carlos onto south bound bay trail near airport, then south through Redwood City to Palo Alto and to San Jose?? This should be a major south bound commute and recreational route. Today you'd have to cross on Holly St into Redwood Shores ... way too dangerous and unattractive for anyone to do!	0	1
1/16/2019	Other	Shopping	0	0
1/16/2019	Other	Shopping	0	0
1/16/2019	Other	Laurel Street shops & restaurants	0	0
1/16/2019	Other	Work	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Route	Frequent recreational routes... 1) Above Devonshire Blvd into Belmont parks by mountain bikers 2) Even more people trying to get from San Carlos through Ralston Ave or Waterdog Lake park to Crystal Springs and to Sawyer Camp Trail. Needs regional improvements and safe, segregated bike route from San Carlos to climb on or near Ralston to Crystal Springs. There have been bike deaths on Ralston... it's a mess right now. This route also serves Carlmont Shopping from SC... not bike friendly at all	0	0
1/16/2019	Barrier to Biking	Arroyo / El Camino is the only real way for bikes to cross railroad tracks to Old County to access north/south and Redwood Shores. Riding San Carlos Ave or Holly is too dangerous for bicycles. This Arroyo/El Camino intersection gives no clues to Drivers that bikers may be there. Needs "Dutch" style offset of bikes ahead of cars, longer light to cross El Camino, segregated and striped bike lanes so cars/bikes know how to turn/cross to tunnel under tracks.	5	0
1/16/2019	Barrier to Walking	Laurel/San Carlos intersection. Could this have pedestrian only 4-way session when pedestrians are present? Or better yet Laurel Street super block so no south bound traffic on Laurel and no turns from San Carlos Ave onto southbound Laurel? This would transform downtown to a walker's paradise and increase business there!! Now that would be a city of good living!!	2	1

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Route	How would a bike get from downtown area to REI and Home Depot shopping areas... safely and attractively???? I'd never attempt it today but our plan should make it attractive for San Carlos shoppers to make the short/flat trip from downtown area to REI and back with easy intersections and safe/segregated bicycling somehow!! Brittan and intersections today are impossible for walkers/bikers to navigate safely.	0	0
1/16/2019	Barrier to Biking	Need a "designed for Bikes too" intersection at Devonshire/San Carlos Ave. That means safe ability to get into turn lanes, safe way from bike to trip traffic light, better paint for bikes turning, connected bike path turning from Devonshire onto San Carlos bike path and vice versa. Same upwards on Wellington at same intersection.	4	0
1/16/2019	Barrier to Biking	Bike path on San Carlos ave is almost useless. Drivers cut corners. Near death hits on pedestrians/bikes are common especially at curves and at night as well as day. Needs physically barricaded bike and walk path on full length of San Carlos Ave to keep any vehicles (cars vs bikes) going >10mph different speeds away from each other. This is a key artery of SC & needs total attractive safety for bikers/walkers. Terrifying today so BIG deterrent to abandoning cars!	8	0
1/16/2019	Other	Want to go to downtown cafes, restaurants, shopping 2-4 times per day by walk/bike from Devonshire area. Not friendly to walkers or bikers today... route and bike parking are bad. Using expensive e-bike or road bike ... also need safe or well-designed parking next to shops!	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Other	Want to get to REI or Home Depot shopping areas by walk or bike from Northwest San Carlos. 100% car oriented today. Ugh.	3	0
1/16/2019	Other	Trader Joe's and south Laurel Street areas.... totally car oriented today. Needs good bike route from Northwest San Carlos and good bike parking for e-bikes or expensive road bikes that's secure and next to stores too!	3	1
1/16/2019	Other	Commuters and recreationists need a way to get down San Carlos Avenue, across El Camino, and into new SC Train Station. With all the construction, there's no clue how bikers should safely, attractively get to train station... ideally directly down San Carlos avenue and across the street. It's a scrum every day with cars and bikers cursing each other and chaos reigning. LOTS of people need this route... making bikes detour to Arroyo/El Camino (another bike mess) is inconvenient and a deterrent.	1	0
1/16/2019	Other	Farmers Market and similar regular events downtown on Laurel St are a huge draw, causing parking shortages and traffic. Many more people could walk/bike if there were better routes, safe and secure parking for bikes. Many just don't go because driving is the only option and it's a hassle today. With Ebikes this could be very different & draw more people with the right routes/bike parking.	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Barrier to Walking	Crosswalk at Brittan and Woodland is not safe. Many times, cars do not stop for pedestrians. A flashing beacon would make this intersection much safer and it would be used more frequently.	3	1
1/16/2019	Route	The streets are very narrow and busy and make riding bike to town difficult. I usually have my kids ride on sidewalk which is not where they should be. A safe route from Burton park area to Laurel would be helpful. I do like the new bike parking in the Wheeler garage. That is helpful.	1	0
1/16/2019	Barrier to Walking	Morse and Rosewood is a four-way uncontrolled intersection. The yield signs in one direction do nothing to make it safer. Cars coming into this intersection don't slow down, because it is unmarked in all directions. Biking and crossing the street here is unsafe. Stop signs and cross walks would help.	0	0
1/16/2019	Barrier to Walking	This intersection is unmarked, only yield signs on one side. Cars do not slow for bikes or pedestrians. Needs to be clearly marked, with crosswalks and stop signs	0	0
1/16/2019	Barrier to Biking	Bike lanes on Brittan, to make it safer for cycling and drivers.	2	0
1/16/2019	Other	Pedestrian/bike bridge	2	0
1/16/2019	Barrier to Walking	Stop light at the intersection of Brittan and Cedar. Autos barely slow down at this intersection. There are a lot of children crossing to get to and from the park.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Barrier to Walking	For people walking between Melendy Dr. and downtown (or St. Charles, Central/Arroyo schools, Burton Park), it would be very valuable to improve the rough trail currently connecting Alameda de las Pulgas and Belle Ave. Installing proper stairs, grading and installing a real trail, and lighting this area at night would make this more usable and safer. It would increase the walkability into town for those living in the neighborhoods above and below Highlands Park.	3	0
1/16/2019	Other	A designated bike path on the side of El Camino near the railway. Lots of unused area south of the parking lots.	2	0
1/16/2019	Barrier to Biking	There should be a bike lane here as Old County is a bike corridor for commuters. The current setup pushes cyclists into traffic.	4	0
1/16/2019	Barrier to Biking	Howard is a common road for cycling since it connects to Alameda de las Pulgas, but cars don't seem to understand that we belong there. Needs Share Lane paint and "share the road" signage.	2	0
1/16/2019	Route	No bicycles (even kids) should ride on the sidewalks here due to the amount of pedestrian traffic and cafe seating. This should be walk your bike on the sidewalk (or ride in the road)	1	0
1/16/2019	Route	Common cycling route since it's a connector, but cars don't seem to understand that we're allowed here. Needs share lane paint and "share the road" signage for cars	0	0
1/16/2019	Other	Train station	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/16/2019	Barrier to Walking	The intersection at Howard and Alameda is extremely dangerous for pedestrians and cyclists. The crosswalk is not adequately marked and cars--most of which are speeding/distracted-- rarely stop. There used to be a small crosswalk sign on the yellow line in the road, but it was hit so many times it fell out and was never replaced. For any San Carlos residents living west of Alameda, the only fully controlled places to cross at Eaton and Brittan are 0.7 miles apart.	0	0
1/16/2019	Barrier to Walking	The location of the fire hydrant and overgrown shrubs force walkers (esp. with strollers) into Brittan exactly where drivers come very quickly around the corner.	1	0
1/16/2019	Route	Lower Lupin is very unsafe with cars that drive too fast and there is no bike lane or sidewalk to walk on. Pedestrians have to walk on the street. Lots of close calls around the corner.	0	0
1/16/2019	Barrier to Walking	Crosswalk needed. People don't stop.	0	0
1/17/2019	Barrier to Biking	I have to go all the way to the Belmont Bike bridge and then back on the other side to get to the bike trail leading south along the marsh into Redwood City	1	0
1/17/2019	Barrier to Walking	The crosswalk at Woodland & Brittan that connects to Burton Park, Sports fields, the Youth Center, 2 schools &	2	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
		downtown is VERY unsafe. Brittan is a main artery & cars drive fast on this road. They often do not stop for, or just don't see, pedestrians. A young boy was hit there a few months ago. People avoid this crossing & tell their kids to avoid it because it's so unsafe. Yet, with all the youth activities at the park, it's not a far stretch to imagine that kids will still cross here.		
1/17/2019	Other	I would love to be able to safely bike over the 101 Overpass to connect to the bike paths in Redwood Shores. I don't commute by bike to RWS, but I'm sure more people would do this if there was a safe way to get over the highway.	2	0
1/17/2019	Route	I drive on Greenwood many mornings to take my son to school. There are many middle school kids on foot, bikes and scooters traveling along Cedar at this time. There are also a lot of cars (large ones) parked right up to the street, so I can't see the kids coming very well. I would like the curbs painted red before the intersection to improve visibility for crossing Cedar	0	0
1/17/2019	Other	Destination	0	1
1/17/2019	Other	Old County should be a great bike route since it is relatively flat and connects multiple cities, commercial corridors and train stations. Unfortunately, the facilities provided are nowhere near comfortable. Traffic volume is high and so are speeds. Things like free personal property storage (aka parking) and landscaped planters need to go to make safe space for people to ride bikes.	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/17/2019	Route	Allow people to ride a bike comfortably to and through the train station. Laurel and San Carlos is a very dangerous intersection with frequent crashes. Take a lane of traffic off San Carlos Ave to create a safe place to ride a bike.	0	0
1/17/2019	Barrier to Biking	Access to the tunnel is not designed well. Remove barriers so people can ride in a straight line to/from Arroyo and the tunnel.	4	0
1/18/2019	Barrier to Biking	Need secure (lockers) bike parking at the transit center. You get folks eyeing bikes and there's no good place to lock them. Not just for the train, but also for the buses you need bike parking	3	0
1/18/2019	Barrier to Walking	This is a dodgy intersection for everyone, including cars. Elm does not have a stop sign, but with the bend in the road it is impossible to see if there are cars coming until you are in the road. Elm south needs a stop sign, and please don't tell me you need a fatality first. It doesn't hurt anything to put one in.	1	0
1/18/2019	Route	Need a bike route on the southern edge of town that is on the west side of El Camino/RR tracks.	0	0
1/18/2019	Barrier to Walking	One side of el Camino has a sidewalk. The other side has a fence that pushes you into the traffic. Be nice if that didn't happen.	1	0
1/18/2019	Barrier to Walking	I avoid walking on San Carlos Ave because of the out of control speeding traffic. Several people have been killed,	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
		2 -3 within a block or 2 from my house, 1 on my corner (Charlton St). Lack of traffic enforcement in San Carlos is rampant		
1/18/2019	Other	I walk here with my young children regularly and crossing Holly Street and even using the sidewalk along Holly Street feels dangerous.	4	0
1/19/2019	Other	Need access to this bike trailhead for my commute to work seaport area of redwood city	3	0
1/19/2019	Other	Second harvest food bank—access by bike desired	1	0
1/19/2019	Other	Cider and brewery- bike/ped access desired	3	0
1/19/2019	Route	I take San Carlos Ave for walking but there are no side properly continuous sidewalks that connects to schools and downtown.	0	0
1/19/2019	Other	Would like to restore pedestrian/bike access to bay trail on Skyway Road, on side of SAM TRANS yard. Gate was closed several years ago. This is a safety concern due to no sidewalks between Twin Dolphin drive and Shoreline Rd/Airport Dr along Holly street. Opening for driving would be a plus, but is an "Easy Button for Ped/Bike.	0	0
1/19/2019	Barrier to Walking	Since there is no other input option, I'll submit input here. I suggest that all ped traffic be limited to south side of 101 overpass and flashing walk lights be placed at crosswalks in four places	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/19/2019	Barrier to Walking	Suggest reviewing ped crossing patterns at Holly/Old County/El Camino. Very dangerous crossing for pedestrians and any pedestrian crossings seriously screw up traffic light sequence/traffic flow. Tough problem that needs attention with new construction in the area.	5	0
1/19/2019	Other	Sidewalk seating along Laurel (especially at Town) severely limits foot traffic. Especially during large parties. This has got to stop! Suggest working with merchants to apply decorative borders to sidewalks indicating no standing/furniture zones.	1	2
1/19/2019	Barrier to Walking	There is an issue with foot traffic on Laurel clearing line of sight with drivers and not looking before crossing. While pedestrians have the right-of-way, they shouldn't walk blindly into intersections where cars are already crossing. Suggest a pedestrian safety campaign and/or sidewalk reminders to "Look Left & Right" before crossing. The status quo is dangerous and it's only a matter of time before someone gets squished.	0	2
1/19/2019	Other	I bike to the train station (and have a bike locker there).	2	0
1/19/2019	Other	This is the midpoint of the farmers market, which I usually get to by bicycle. Occasionally the bike racks are full; it'd be nice to have a couple more at that location.	3	0
1/19/2019	Barrier to Biking	The left turn from Cherry onto Cedar is a little daunting, due to poor visibility (cars parked on Cedar).	3	0
1/19/2019	Barrier to Biking	Can we get bike racks put here? There are shops right near here that I frequent, but the racks over by Bianchini's are a bit far and I'm nervous about my bike getting vandalized.	2	1

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/19/2019	Barrier to Walking	The water company has this fenced off. It would make the trip from Belmont to SC hugely easier if there was a path constructed here. I've talked to the water company people and to some local Belmont politicians. It is feasible, but wrapped within an overall walking improvement plan would be great.	0	0
1/19/2019	Route	The sidewalk on Highlands ends here at about Northam. Kind of annoying and a bit dangerous for running. Would be nice to get it fixed for this stretch.	0	0
1/19/2019	Barrier to Walking	The trail here through Devonshire canyon connecting Crestview and Devonshire Blvd. is great. but needs some maintenance. And the access point at Crestview is covered in No Trespassing signs. The right of way should be opened and the trail rehabilitated.	6	0
1/19/2019	Barrier to Walking	This gate is currently closed. No idea why. Would be great to get it opened and access from Crestview to Pulgas park.	5	0
1/19/2019	Barrier to Walking	There is a fence blocking off the Sheep Camp trail from Pulgas Park. Again, I've no idea why. Would be very nice to have it opened up and connected.	4	0
1/19/2019	Route	Going from Pulgas park to Edgewood park and crossing Edgewood and going up Crestview is pretty hairy as a pedestrian (runner). Could do with flashing lights, sidewalk and crosswalks I think.	0	0
1/20/2019	Route	Bike to Caltrain	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/21/2019	Barrier to Walking	Exactly the type of path that we should have in San Carlos - pulls foot traffic/bike traffic away from heavy car traffic routes and is a beautiful walk along the creek. We should reopen this path and look for others too!	3	0
1/21/2019	Barrier to Walking	This needs to be made a 4 way stop sign. Cars go extremely fast here and it's right next to a school.	3	0
1/21/2019	Route	My commute route from the Caltrain Station to near my house.	0	0
1/21/2019	Barrier to Walking	Needs curb ramps	0	0
1/21/2019	Other	We should make this road a permanent walking path.	1	0
1/21/2019	Other	Make Laurel street a permanent walking/biking area.	3	0
1/21/2019	Other	Please survey pedestrians and drivers to see if: 1) Do drivers NEED to drive there? Where are you trying to go? Do you live in San Carlos? Do you come here often? Would you mind walking < 5 minutes after parking? 2) How many times have pedestrians need to rush/run across roads to let cars go? 3) How many families feel safe walking there? 4) Ask everyone: How much do you enjoy the farmer's marketing experience? Would you like that to be every day?	0	0
1/21/2019	Other	If we can block off Laurel st as a permanent walking area, then it might make sense to make a bike tunnel	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
		starting from laurel street that crosses El camino and the train tracks. This way bikers will only need to experience a few stop signs/lights on smaller streets and could get to laurel or to other side of 101 easily.		
1/21/2019	Other	Open up this land to be free parking while closing off Laurel St as a permanent walking area.	1	0
1/22/2019	Barrier to Walking	So much safer route to get to/from downtown and Caltrain, this path needs to be reopened	2	0
1/22/2019	Other	Intersection is not pedestrian or bicycle friendly.	4	0
1/22/2019	Other	Intersection is not pedestrian or bike friendly	4	0
1/22/2019	Other	Intersection is not pedestrian or bike friendly.	5	0
1/22/2019	Other	East San Carlos Ave. needs to be restored to its historic width for pedestrian and bike reasons. Businesses being allowed to move parking into the ROW is not right. Prior City Council members have agreed with community leaders, but nothing has been done about this.	3	0
1/22/2019	Other	A future high school should not be placed at this location by the Sequoia High School District. It's congested enough. Please find the school district another piece of land in San Carlos to improve and encourage the City to purchase these properties to be added to Laurel Park.	1	1
1/23/2019	Other	Bike lane	0	0
1/23/2019	Barrier to Walking	Stop signs and crosswalk	0	0
1/23/2019	Route	This is a typical route that I take on my bike to go downtown.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/23/2019	Barrier to Biking	Wow! Just saw that there is a bike/walking path here. This is huge - significantly safer route vs. San Carlos Avenue which is very dangerous for walker/bikers. My family and I will definitely use this much preferred path.	3	0
1/23/2019	Barrier to Walking	Please put in a bike/pedestrian path to connect the dead end of Carmelita to Cedar street. This would SAFELY connect all of the streets off of Carmelita to downtown, and provide a safe walking/biking route along Carmelita to downtown for kids, so that they don't have to take San Carlos ave.	1	0
1/23/2019	Barrier to Biking	Please install a tunnel across this intersection for kids walking to Arundel for school. The distracted rush hour traffic at this intersection is outrageous. No parent I know lets their kids walk to Arundel alone because of this intersection.	1	0
1/23/2019	Route	It's completely unsafe to send kids biking and walking down San Carlos Ave, particularly during rush hour. Is there a way to decompress some of the traffic onto alternative routes? Or make Carmelita a walker/biker route with a new path between the dead end of Carmelita and Cedar, so that kids can safely get to the library/downtown/activities at schools?	0	0
1/23/2019	Route	I walk my kids to Arundel every day. I would love to sidewalks on the two blocks of Molten between Plymouth and Devonshire. These two blocks get busy with parked & moving cars, and it's simply not safe for young children to walk alone along here.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/23/2019	Other	Frequently bike or walk through downtown to get here. The extensive street parking, especially on windy streets makes it dangerous	0	0
1/27/2019	Barrier to Walking	This is one of the stranger streets in the entire city with an upper and lower roadway. it is very narrow and the driveways are short. On a recent weekend day, I counted around 20 cars parked on the sidewalk. Makes it impossible for a child to learn to ride a bicycle or for someone in a wheelchair to maneuver. It's illegal and it's wrong to be parking vehicles on sidewalks. Period.	1	0
1/28/2019	Barrier to Biking	Would much prefer my children to walk to school through this path vs. having to walk back out to San Carlos Avenue. Much safer.	0	0
1/29/2019	Other	Would love to bike/walk to Burton Park.	2	0
1/30/2019	Other	Would be great to have hiking trail/path from the end of Camborne to the end of Cranfield. Maps show a public right-of-way (future road) connecting the two. Would require coordination with Belmont, as some/all is on their side of city border.	0	0
1/30/2019	Other	Would be great to have hiking trail/path from the end of Shrattin to the end of Cranfield. Maps show a public right-of-way (future road) connecting the two. Would require coordination with Belmont, as some/all is on their side of city border.	0	0
1/30/2019	Other	Would like to see public path and bike lanes connecting Alameda to Vista del Grande as part of any development of these lots.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/30/2019	Other	Would like to see public path (and bike lanes, if there is a street) connecting Alameda to Highlands Park via Dundee Lane as part of any development of these lots.	0	0
1/30/2019	Barrier to Walking	There is no way to cross Crestview safely between Brittan and Club; the road is wide, curved with monumental train, especially at commute and school times. A pedestrian count will yield no reliable info, since anyone who has access to a car uses it to cross. Parents having to drive children to school because it is too dangerous for them to cross to the Melendy-Crestview stop add to the traffic near Terra Linda. Even negotiating the T intersection off Melendy in a car is dangerous!	0	0
1/31/2019	Barrier to Walking	Cars are parked on the sidewalk on Howard and make it impossible to get by with a stroller. Additionally, Howard needs ramps on every corner.	2	0
2/2/2019	Barrier to Walking	It is difficult for pedestrians on Laurel St. to walk on the sidewalks due to big intrusions by many restaurant's "sidewalk-located tables&chairs" into walking spaces. On busy days, it is VERY difficult for the elderly, the infirm/injured, families w/ strollers and everyone else; besides being unpleasant, it also creates an unsafe condition (trip /fall) for elderly folks using canes, walkers or carts. We need far tighter limits: 36"+ MIN CLEAR/OPEN SIDEWALK WIDTH (e.g. ADA compliant)!	1	2

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/2/2019	Barrier to Walking	I realize the gray areas are county not city, but many of the people (children!) walking in this area are residents of San Carlos. Many junior and high school students walk down Windsor Drive from TL and Carlmont, and the lack of some kind of pedestrian safety is an issue. The intersection of Torino, Windsor, and Winding Way is truly egregious	0	0
2/2/2019	Barrier to Walking	There are two sections on Beverly that do not have a sidewalk. It's a steep road, people drive fast, many cars do not stop at the stop signs here to at least slow down at the end of Club. The city did a nice job making the bottom half of this road safer, and I'm confident you can do something about the top.	0	0
2/4/2019	Other	Burton Park and the elementary schools should be accessible by bike lane.	2	0
2/4/2019	Other	BA elementary school should be accessible by bike lane from downtown and Burton Park	2	0
2/4/2019	Other	train station with plenty of bike parking.	0	0
2/4/2019	Barrier to Walking	getting to other side of railroad tracks can be a long walk. More tunnels are needed. At least one every two blocks.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/4/2019	Route	<p>It would be great if there were some primary walking/sidewalk routes that essentially ran parallel to each other and had wider sidewalks to access the downtown areas such as Laurel St. Saint Francis, Belmont, Howard, Brittan, Arroyo, Cherry, and San Carlos come to mind.</p> <p>It's easier to wander a bit on a bicycle than it is to walk. Giving relatively direct routes to walk would be a big advantage.</p>	0	0
2/4/2019	Other	side walk	1	0
2/4/2019	Barrier to Walking	no side walk	1	0
2/4/2019	Barrier to Walking	Speeding on Elm St. makes walking or biking to Arroyo/Central unsafe for children. If this intersection was a 3 way stop (instead of only a stop for drivers on Olive) it would slow down drivers on Elm and allow children/pedestrians/bikers to cross the street. Please consider making this intersection safer for getting to school. Thank you!	3	0
2/4/2019	Barrier to Walking	Repainting crossing walks and perhaps adding flags for kids to use when crossing the street would help improve the safety of these crosswalks.	0	0
2/4/2019	Barrier to Walking	I often see kids crossing here when walking from Arroyo School towards Laurel Street. Cars come around this turn very quickly and I feel it is unsafe.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/5/2019	Barrier to Walking	I'm a long time San Carlos resident and as a youth I remember this path, I walked to school several times. Over the years, it became run down and they closed it off. I always thought that was a mistake - it's time to reopen it! It seems like the home living next to the path has just decided to take it over which is not fair to all the San Carlos residents that should benefit from this walking/biking trail.	0	0
2/5/2019	Other	commute	0	0
2/5/2019	Other	grocery	0	0
2/5/2019	Route	commuting on el Camino would be the ideal option, if made safe. if possible, a dedicated line track behind the new buildings and next to the train tracks would be even better	0	0
2/5/2019	Route	my usual commute, since el Camino is impossible. old county road has its own hazards though and, in many places, no dedicated bike lane	0	0
2/7/2019	Route	School route to TL, Mariposa, and Charter (also Carlmont). The section between Wellington to Exbourne doesn't have a sidewalk. This is a big hill with cars parked on both sides and a lot of kids go up it every day. I would suggest just monitoring this area for a week to see if this is an acceptable risk to the city. After the stop sign at the Dartmouth/Wellington intersection, the remaining intersections are uncontrolled.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/7/2019	Route	There is a crosswalk here, but it's also an uncontrolled intersection where the vehicles approaching the T-intersection believe they have the right of way and do not slow down or stop. The sight lines are diminished in this area and it can be perilous for walkers. I would suggest monitoring and see if this is a continued acceptable risk.	0	0
2/7/2019	Barrier to Walking	This is a controlled intersection (3-way stop), but vehicles rarely stop. Even in the morning when children are present and actively crossing the street. Many vehicles do not even reduce speed as they pass through the intersection. I would monitor and determine whether this is a risk the city is acceptable in having.	1	0
2/9/2019	Other	Daughter walks to/from school here.	0	0
2/9/2019	Barrier to Walking	Cars often park or drive in the green painted bike lane on the north side of the street. No protection for pedestrians. I don't walk here and tell my kids to cross to the other side.	1	0
2/9/2019	Barrier to Walking	Unimproved public right-of-way has been blocked off. Why not add stairs to improve access to Hillcrest Park and to Arundel school for families walking from the other side of the hill?	2	0
2/9/2019	Route	Stairway from Vista to Hillcrest improves our access to San Carlos Ave. and downtown; also used by families walking to Arundel School. Would like to see more of these in our hilly neighborhood.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/9/2019	Barrier to Biking	Cars speed around this corner and frequently drive in the bike lane. This is the major barrier to me and my family for biking downtown.	4	0
2/9/2019	Barrier to Walking	Blind corner with no sidewalk for pedestrian safety.	3	0
2/9/2019	Barrier to Walking	No sidewalks. Parked cars block pedestrian access to Arundel School.	1	0
2/9/2019	Barrier to Walking	Cars frequently parked over narrow sidewalk block pedestrian access to Arundel School and neighborhood.	2	0
2/9/2019	Other	We frequently walk downtown from our house near Arundel School. More sidewalks, pedestrian safety measures at corners, and traffic calming/pedestrian buffer zone on San Carlos Ave. would be very welcome.	1	0
2/9/2019	Other	I would like to be able to bike to Trader Joe's but the current setup on San Carlos Ave. near my house feels too dangerous.	0	0
2/10/2019	Other	Would like to have a bike path that goes from holly to the Bay Trail	1	0
2/10/2019	Other	Currently walk here. Along Alameda going south there are poor sidewalks	0	0
2/10/2019	Route	Britain Avenue connecting Eaton Park to Alameda doesn't have sidewalks	0	0
2/11/2019	Barrier to Walking	Lack of sidewalk between Elizabeth and San Carlos Ave on Cordilleras makes walking that route difficult and unsafe.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/12/2019	Barrier to Walking	There is no continuous sidewalk on upper Brittan between Milano Way and Alameda. Very dangerous to navigate this area on foot. It landlocks upper Brittan giving us poor walkability scoring., and poor access to Burton Park, Laurel Street, bus stops on Alameda. I see a lot of folks walking it but very dangerous. Some people cross to Brittan between this area as the sidewalk ends on north side then appears on south side by Trinity. This seems like such an easy fix to run a continuous sidewalk.	3	0
2/12/2019	Barrier to Walking	Impatient/aggressive drivers don't make a full stop. They also don't wait until you and the young children you are walking with are clear before they roll forward.	1	0
2/12/2019	Other	Cars come fast down Howard (from Alameda), they don't make a full stop at Cordilleras and keep rolling, regardless of whether or not you and children walking to school are clear, crossing the road.	0	0
2/12/2019	Other	Cars don't make a full stop, they roll ahead, even when they see you and children crossing the street during before and after school hours. There should be a police officer there writing tickets, it's clearly within an elementary school crossing.	0	0
2/12/2019	Other	Cars don't make a full stop, they roll ahead, even when they see you and children crossing the street during before and after school hours. There should be a police officer there writing tickets, it's clearly within an elementary school crossing.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/14/2019	Barrier to Walking	There should be 4-way stop signs here, not just 2-way. A lot of kids walk to Central and Arroyo this way and every morning when I walk with my daughters to the two schools, we see kids crossing un-safely over Cherry Street when they are walking on Chestnut.	2	0
2/14/2019	Barrier to Walking	There should be 4-way stop signs in this intersection, not just 2-way stop signs. I see kids crossing Holly from Walnut in this intersection every morning and it is very un-safe. Cars drive fast on Holly and don't stop.	3	0
2/14/2019	Barrier to Walking	My daughters and I cross here every school morning to walk to Arroyo and Central schools. Cars can turn right on a red light from Holly onto El Camino northbound. We wait for the pedestrian light and start crossing the street once it comes on, and inevitably a car will turn right on their red light there and drive right in front of us, so we have to quickly jump back to the sidewalk. It is very unsafe. Some mornings my oldest daughter walks by herself to school and I worry a lot about her safety	3	0
2/14/2019	Other	Like to ride bike with family during weekends.	0	0
2/16/2019	Route	Getting across the highway and past Industrial Rd (both directions) is terrifying. As an airport user, I would like to see this section improved.	1	0
2/16/2019	Route	Lights are not timed well for bikes. Also, difficult to turn left as it requires crossing multiple lanes of traffic.	0	0
2/16/2019	Route	A pedestrian walkway here would allow easier access to Skyway Rd. vs having to walk all the way to Airport Dr. - A crosswalk at the off ramp already exists at this location.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/16/2019	Other	Would love a safer way to access KSQL area from Caltrain/Downtown areas.	0	0
2/16/2019	Route	It would be good if bicycles and skateboard and scooter had someplace instead of the sidewalk	0	0
2/16/2019	Route	This should be a three-way stop. Cars going on NB Magnolia often do not respect the one crosswalk at this T-intersection	0	0
2/16/2019	Other	The intersection of Laurel and Greenwood is *dangerous*. Cars drive very fast, it is not well lit, there is a preschool near the intersection. Badly needs stop signs on Laurel!	0	0
2/17/2019	Other	This is an intersection that my family uses to take our kids to Arundel school, which takes place at a high commute time for cars using SC Ave as a thoroughfare. A slightly longer traffic light for the pedestrians might be nice, maybe more of a barrier between kids and cars at the intersection would be nice.	1	0
2/17/2019	Barrier to Walking	On San Carlos Ave, from Devonshire heading west to Arundel Rd, both north and south need a barrier between people and cars. It feels so unsafe to walk my dog on this stretch, and would never walk my kids on this part. At least add a railing, separating cars and people. The north side of that street terrifies me, and I lived in SF for 25 years.	1	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/17/2019	Barrier to Walking	2 of the busiest 4 streets in San Carlos meet here. Everyone is eager to get onto the road they want to. It feels SO UNSAFE walking my kids and dog here, we avoid this intersection at all costs, prefer crossing the Alameda at Carmelita feels safer than at the intersection. Lights on the ground, flashing lights for the pedestrians, have to make this safer for our kids' health and safety, when pedestrians are present.	1	0
2/17/2019	Barrier to Walking	South side has a walking path, which is great, but I would love a barrier between the thoroughfare and little people walking, the barrier now is 6 inches off cement; north side of the street has zero sidewalk and zero barrier between people and cars, extremely unsafe for people	3	0
2/17/2019	Barrier to Walking	Between Vista Ave and Sycamore St, on the north side of the street, there is no sidewalk, there is no barrier between cars and people, and cars wander into the bike lane on that turn, getting even closer to people and pets...very unsafe.	4	0
2/17/2019	Barrier to Walking	Please add a pedestrian crosswalk at the intersection of Carmelita and Alameda DLP, like the one you have already on Alameda DLP at Alma St... flashing lights in the cement, flashing lights from poles would be brilliant.	0	0
2/18/2019	Route	We need better lighting on Alameda from Carlmont High School to San Carlos Ave and down San Carlos Ave to Laurel.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/18/2019	Route	It would be nice to have a safer and smoother way to get from San Carlos/Holly St. to the ped. bridge over 101 at Ralston Ave. in Belmont. Currently biking on Old County Road is not optimal.	0	0
2/18/2019	Other	Lots of great restaurants here	0	0
2/18/2019	Barrier to Walking	Lack of sidewalks combined with blind turns creates a very hazardous condition for the many parents and children who walk to Arundel School along Clifton Avenue. We need sidewalks to protect pedestrians, and we need traffic calming to discourage drivers from recklessly barreling down Clifton at speeds of up to 35mph just to avoid a couple of minutes of congestion on San Carlos Avenue.	0	0
2/18/2019	Route	This part of Clifton Avenue has no sidewalks and is extremely dangerous for pedestrians, especially during the morning rush hour when kids are walking to school. We need sidewalks and traffic calming here!	0	0
2/18/2019	Route	This stretch of San Carlos Avenue is terrifying for pedestrians and cyclists. We need sidewalks on both sides of the street, physically separated bike lanes, and intersections that are designed for bike/pedestrian safety.	1	0
2/18/2019	Other	Pedestrian and bike	0	0
2/18/2019	Barrier to Walking	Barrier to walking and biking. There are no sidewalks. This is very unsafe.	0	0
2/18/2019	Barrier to Walking	walking and biking	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/18/2019	Route	This area is a barrier to walking and biking, with a single pass lane for up-and-down vehicles, there is no room to walk safely for kids, adults, children alike.	0	0
2/18/2019	Route	Venturing onto Elm north of the blind curve between Arroyo and Olive is dangerous for pedestrians, bicyclists, and motorists.	0	0
2/20/2019	Other	Walk to Caltrain	0	0
2/20/2019	Route	Walking route along San Carlos Ave could use sidewalk on the north side	0	0
2/20/2019	Other	I want to be able to bike to the Caltrain Station.	0	0
2/20/2019	Barrier to Walking	Please add a crosswalk from the south side of Elm st to Olive st at this intersection.	0	0
2/21/2019	Other	Would like to open up access from Sycamore st to Hillcrest park. There is already a public right of way and it could significantly improve walk ability of the neighborhood and provide a safe route from downtown to the hillcrest neighborhood that does not require pedestrians to walk on san carlos avenue	1	0
2/21/2019	Barrier to Walking	Would be great to provide access on the east side of canyon park so that you could cross to Brittan from the top of the hill	0	0
2/21/2019	Other	this is where I would start and end my commute to/from work on weekdays	0	0
2/21/2019	Other	I would love to walk or bike between our home in the White Oaks neighborhood to the train station	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/21/2019	Other	Alameda de las pulgas and connecting to RWC bike path	0	0
2/21/2019	Other	A designated bike path along Laurel would be a dream.	1	0
2/21/2019	Route	Safe bike route to downtown from White Oaks.	0	0
2/21/2019	Barrier to Biking	There is no good way to safely cross over to old county road	0	0
2/21/2019	Route	Designated bike lane on Cedar and San Carlos Ave to Caltrain Station	0	0
2/21/2019	Route	My ride to work	0	0
2/21/2019	Route	My ride to Trader Joe's	0	0
2/21/2019	Route	My ride to Bianchini's	0	0
2/21/2019	Barrier to Biking	Need safer clearance for cyclists and pedestrians turning from Alameda to San Carlos Ave.	0	0
2/21/2019	Barrier to Walking	Would be great to open access between McDougall park and tierra linda -it would do a lot to connect the neighborhood	0	0
2/22/2019	Barrier to Biking	I would not let my kids or encourage other young people to bike west of this intersection (towards Arundel); sadly, it's the only relatively flat access point to important destinations like Arundel and Tierra Linda.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/22/2019	Route	Towards el Camino: I was struck by a car on this straight a few years ago and had a fairly serious accident. The bike lane disappears and the 45degree parking spots led to a car pulling into me. Away from el Camino: Leaving Caltrain station bikers are left with few options, sidewalk or cut through a dangerous intersection to get on the right side of the road.	0	0
2/22/2019	Barrier to Biking	would like to see a multi-use path from Alameda through the upcoming Black Mountain development that connects with Dundee. This would make it easier (less gradient) and safer for walking and bike access for those living in the Highlands Park area.	1	0
2/22/2019	Barrier to Walking	please consider improving the trail/path from St. Charles school up to Alameda. This is a great short cut for walking access to downtown, but gets tough to do when it's wet. Making this official & nicer should be an easy quick win.	0	0
2/22/2019	Barrier to Walking	I believe it is here near Prospect where there is a supposed crosswalk for pedestrians. I have lived on this side of town for 16 years now, and have seen it used maybe twice. Thus, if it is used, it is a huge surprise to drivers. I've seen some crosswalks like this on El Camino as well. Very dangerous. Anytime there's a pedestrian crosswalk on a busy street, there should be yellow lights or some indicators that drivers can see.	0	0
2/23/2019	Route	Home to Library	0	0
2/23/2019	Barrier to Biking	Dangerous intersection for bikes: it is a choke point for cars turning every which way.	2	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/23/2019	Barrier to Biking	Holly Street between ECR and Industrial requires bikes and cars to share lanes in often heavy traffic conditions.	1	0
2/23/2019	Barrier to Biking	Holly Street overpass requires bikes to navigate cars weaving in and out of off and on ramps.	1	0
2/23/2019	Barrier to Biking	This section of SC Ave requires bikes and cars to share a lane as the roadway curves toward around past Cranfield Ave.	0	0
2/23/2019	Barrier to Biking	Bikes have to negotiate heavy traffic of cars turning in and out of school entrance during school start and end times.	1	0
2/25/2019	Other	Would like to walk to library	0	0
2/25/2019	Other	Would like to walk to Burton Park	0	0
2/25/2019	Other	Would like to walk to downtown	0	0
2/25/2019	Other	Need a 4 way stop sign here	0	0
2/25/2019	Barrier to Walking	Crosswalk would be helpful here. So many cars speed along Cordilleras, it would be ideal to have some support for pedestrians (and cyclists).	0	0
2/25/2019	Barrier to Walking	The flashing light at the crosswalk doesn't do enough because the cars come down Brittan so fast and they're looking down at the road, not up at the flashing light. The crosswalks with flashing lights on the ground & bollards in the middle of the street would be more attention-grabbing (like what RWC & San Mateo do).	3	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/25/2019	Barrier to Walking	There is a fire hydrant and a bush that make it nearly impossible to get my double wide stroller through (which is the same width as the ADA guide for wheelchair)	0	0
2/25/2019	Route	Old County is one of the most direct north/south options for bike commuters. It has tremendous potential and is the "safer" option than ECR. Much could be done to improve it, with a dedicated bike lane and clear markings. I would support removing car parking spaces to allow for better bike access, with the caveat that bike parking infrastructure be included in front of businesses.	0	0
2/25/2019	Route	Arroyo is a main arterial in San Carlos. For residents, it offers a direct route to Central Middle School, Arroyo School, Burton Park and Youth Center, and downtown/Laurel. For pedestrians much can be done to: <ul style="list-style-type: none"> - Enhance the walking path along the schools on the NS of the street (look to the park's path for inspiration) . - Enhance the block b/w Walnut and Laurel w/trees 	0	0
2/25/2019	Route	This is a major route for children and families walking to many of the schools, Burton Park, city amenities like the library, and home. Often cars fly down the hill on Arroyo at unsafe speeds. Preventative measures should be put in place. <ul style="list-style-type: none"> - The crossing at Arroyo and Cordilleras needs stronger safety markings (perhaps blinking reflective lights) - The crossing at Orange and Arroyo needs to be better demarcated - The crossing at Cedar and Arroyo is precarious due to the angle and car speeds 	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/25/2019	Route	NW of the Alameda/San Carlos Ave. intersection needs to be improved for bicycles. This is one of the only direct routes from the downtown/train station/flats to Arundel, Tierra Linda and Belmont. Currently, I do not feel it's safe for most people, let alone children, to bicycle this section. Please consider reconfiguring the road so bike and pedestrian facilities are next to each other, and parking/through cars are separate. See NACTO's latest guidelines/recs.	0	0
2/25/2019	Other	With progressive thinking, ECR could be the ideal bike/ped facility. Currently it's not a welcoming environment. As new developments occur, please consider Class IV protected bike lanes next to sidewalks and tree planting/landscaping requirements to make this a sustainable and attractive amenity that prioritizes people over single-occupancy and parked cars.	0	0
2/25/2019	Route	The library is a major destination for young families, students, and the senior community. The intersection at Cedar and Cherry could be much improved with blinking reflectors, better signage, etc. It always feels like a risk to cross Cedar with parked cars creating blind spots/blocking views and cars speeding.	0	0
2/25/2019	Route	It would be wonderful to once a month have an open streets event in downtown on Laurel (from San Carlos Ave to Eaton) which would encourage families to walk and bike on the street and enjoy downtown. Perhaps restaurants could even have pop-up eating in parking spaces adjacent to their lot. For more info see: https://openstreetsproject.org/	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/26/2019	Other	This intersection is dangerous to pedestrians as drivers tend to pick up speed on this straightaway section of San Carlos Ave. The crosswalk needs flashing light indicators similar to the ones at Arundel School, or stop signs.	0	0
2/26/2019	Other	This is a dangerous intersection that one could argue should have four stop signs rather than the current two. Drivers speed up and down Molton while cars regularly roll or blow through the stop signs on Plymouth. There are no sidewalks, so it's a very dangerous spot for children and pedestrians.	0	0
2/27/2019	Other	My kids cross at this intersection and I worry about drivers and construction making this crossing dangerous.	0	0
2/27/2019	Route	I appreciate the crossing guards for Brittan Acres, Central, and Arroyo so the kids can walk and bike safely.	0	0
2/27/2019	Barrier to Walking	Crossing Elm at San Carlos Avenue is very hazardous due to the unprotected left turn lane from SC ave to Elm north bound. Cars are focused on the oncoming traffic and not the peds crossing the street! There very much needs to be a protected left-hand turn at this intersection and at Walnut as well. This was supposed to happen as part of the Wheeler plaza, but apparently not yet!!!	0	0
2/27/2019	Barrier to Walking	A pedestrian over crossing should have been mandated for the transit village project!	3	0
2/27/2019	Route	Need to make this more walk/bike friendly. Would love to see the city encourage more biking/walking in this area.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/1/2019	Other	Easy access to public park for kids - going up Melendy to get to Highlands is not easy for kids on bike - unless they're much older (Say 10yrs+).	0	0
3/1/2019	Route	There should be additional parallel routes to Brittan (e.g., Howard or other) to get down to Laurel street. Brittan has LOTS of traffic, despite well-defined bike lanes. Consider adding barriers or additional coloring to Brittan lanes. There also needs to be a parallel route to get up Laurel without having to "ride" Laurel. Today Laurel is dominated with cars and tight space. Rather than banning cars, just add bike lanes on adjacent streets.	0	0
3/1/2019	Route	Route from school to downtown	0	0
3/1/2019	Barrier to Walking	insufficient sidewalks	1	0
3/1/2019	Barrier to Walking	sidewalks are uneven and are a severe tripping hazard to the students walking to/from the local schools	0	0
3/1/2019	Route	sidewalks are missing and there aren't good visual or physical barriers for the drivers barreling down SC Ave.	0	0
3/1/2019	Other	City Hall	0	0
3/1/2019	Route	The intersection at Holly and Laurel was recently switched from acting as a stop sign to a stop light. The change has certainly increased the amount of time it takes to walk through the intersection and also appears to be causing more traffic jams. Including backups that extend into the Holly and ECR intersection. The stop signs seemed to work really well and I believe they would be a better alternative.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/1/2019	Barrier to Walking	I'm unable to safely cross Alameda at this intersection. At commute time it's impossible. No vehicle has ever yielded the right of way. There is also a blind vertical rise where northbound vehicles cannot see a pedestrian until the last second. There is no crosswalk on Alameda between Eaton Ave and Howard Ave. At times I must go to Eaton Ave to cross safely. I recommend a crosswalk and pedestrian activated flashing lights be installed here.	0	0
3/1/2019	Barrier to Biking	This intersection would act much better as a three way stop and vehicles should not be allowed to park near corners. As is, traffic turning left from Oak St. cannot see vehicles coming from the left on Elm without pulling into the intersection and risking an accident with cars coming from the right.	2	0
3/3/2019	Barrier to Walking	Dangerous for bikes and pedestrians. Cars go very fast up the hill and veer into the bike lane. As a pedestrian, I often have to walk very close to the road/bike lane because of cars parked on the street -- it's very scary! Ideal would be to add a sidewalk for pedestrians and a barrier for the bike lane to give bikes and pedestrians more space.	0	0
3/3/2019	Other	Library	0	0
3/3/2019	Other	work	0	0
3/3/2019	Other	Framers Market	0	0
3/3/2019	Other	Burton Park	0	0
3/3/2019	Other	Trader Joe's	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/3/2019	Other	Saltbox	0	0
3/3/2019	Barrier to Biking	Barrier to both walking and biking. Crossing El Camino Real to access Caltrain Station	0	0
3/3/2019	Barrier to Walking	Barrier to walking and biking crossing San Carlos Ave at both Laurel and Walnut	0	0
3/3/2019	Barrier to Biking	Barrier to walking and biking. Free northbound ECR free right turn lanes on to Holly eastbound	0	0
3/3/2019	Route	Pavement breaking up on edges, potholes, needs repaving work	0	0
3/4/2019	Barrier to Walking	This three-way intersection at Laurel/Bush/Magnolia is a barrier to both safe walking and biking. There are stop signs at two arms of the intersection (Laurel and Bush) but not at the third (Magnolia). This is unsafe for pedestrians and bikers because cars coming off Magnolia drive through this intersection very quickly. (It is also unsafe for other drivers, who would reasonably expect that all directions would have a stop sign.) Adding a stop sign at Magnolia would greatly improve safety.	5	0
3/4/2019	Barrier to Walking	The one-block stretch on Laurel between Oak and Bush/Magnolia is extremely unsafe for biking and for pedestrians crossing the street. Cars frequently use this stretch of Laurel (which at this point is a residential street) as a bypass for ECR, even as the road here both (a) curves and (b) crests a small hill, leading to extremely limited visibility. This is an accident waiting to happen. Putting speed bumps towards the north and south ends of the block to slow traffic would help immensely.	5	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/4/2019	Other	Laurel street	0	0
3/4/2019	Barrier to Walking	I try and walk my elementary age kids to Arundel most days. There are also many middle school aged kids walking alone. There needs to be a stop sign and cross walk where Shelford and Dartmouth ave intersect and a cross walk at the bottom of Shelford to safety Shepard kids along the sidewalks.	0	0
3/4/2019	Route	I try and walk my elementary age kids to Arundel most days. There needs to be a stop sign and cross walk where shel ford and Dartmouth ave intersect and a cross walk at the bottom of Shelford to safety Shepard kids along the side walks into Arguello park	0	0
3/4/2019	Barrier to Biking	When I ride a bike from downtown to the East side I prefer to cross at Holly and proceed under the Caltrain station to Old County. This route feels much safer than crossing at ECR and Holly. That being said it would be nice to have an easier way to bike through the station and not have to dismount.	0	0
3/4/2019	Barrier to Walking	This intersection is difficult to cross safely because it is so large, and the west side of Woodland between Baytree and Aster doesn't have a sidewalk	0	0
3/4/2019	Barrier to Walking	A crosswalk is needed here. Many students walk down this street after school to access the downtown shopping area, and cross Elm Street at School Street unsafely. In fact, two crosswalks would be great - one north/south on School Street, and one east/west on Elm at School Street.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/4/2019	Other	I would like to bike here if there were bike lockers. I won't bike without a safe place to leave my bike (I don't know if you've noticed the cannibalized bikes locked to the racks, but I have, and no thank you to that).	0	0
3/5/2019	Other	The sidewalks are so unsafe, broken and cracked. These need to be fixed.	1	0
3/5/2019	Other	Why isn't there a throughput from El Camino to East San Carlos Avenue. What exactly is the current underpass contributing to? Last we heard the city was intending on moving the Historic depot cafe. If this is still the case, why not take advantage of this valuable piece of roadway and restore East San Carlos back to the throughput it was.... BEFORE Caltrain. That would solve so many things	0	0
3/5/2019	Other	Northbound overpass for both bicycles and cars	0	0
3/6/2019	Route	need sidewalk on Tierra Linda side of street. thanks.	0	0
3/6/2019	Route	The lane stripping is odd through this stretch. added middle turn lane but way not bike lanes, not many left turns; didn't mark street into Belmont (just did that part that got seal coated) =makes driving, biking, harder now; needed transitions to south and north. thanks.	0	0
3/6/2019	Route	please add sidewalk to Middle School side of street	0	0
3/6/2019	Route	Need to widen Holly from ECR to Laurel, use R/W owned by city, tight and hard to ride especially with "jammed"	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
		lanes at Laurel/Holly traffic signal. At Elm St/Holly St. need to restrict parking on Elm St., east side (short stretch) so can get through that area safely. Parked vehicles and two-way traffic and bikes :-(((
3/7/2019	Route	please add a sidewalk vs. gravel/dirt on right side of San Carlos Ave across from Carlmont HS.	0	0
3/11/2019	Barrier to Biking	Biking and walking over 101 via Holly is very challenging. It would be great to have a dedicated bike/pedestrian bridge over 101 (similar to the bridge at Ralston). Otherwise, if we can't build a bridge, then at least adding signage and improving the line markings would be helpful (e.g., make a solid green section indicating a bike lane).	0	0
3/12/2019	Route	Need to add sidewalks and formal bike lane on this stretch. Thanks	0	0
3/12/2019	Route	<p>A thought on this route:</p> <ul style="list-style-type: none"> - a additional bike/ped/scooter/etc. undercrossing of Caltrain tracks. Proposing St. Francis Way, but perhaps another place is better. This undercrossing could be constructed at the same time as Whipple Ave Grade Separation. - this route connects the south side of San Carlos and provides a way to get to Whipple Ave/101 overcrossing to get to the Bay Trail, etc. Industrial Rd in RWC has been made easier to ride on already. - Thanks 	0	0
3/15/2019	Barrier to Biking	Bollards on bridge are a hazard for bikes. They force bikes into the road without notice to cars	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/15/2019	Route	Alameda generally works well in both directions for cycling.	0	0
3/15/2019	Barrier to Biking	When traffic backs up on Alameda from Brittan, southbound cars turn left onto Howard without checking the bike lane. This creates a major hazard.	0	0
3/15/2019	Barrier to Walking	The speed limit may be 25, but few cars go ≤ 25 . Most are going 30+. There is no enforcement. I would love to see the limits enforced before someone is killed.	0	0
3/16/2019	Barrier to Walking	Students have difficulty crossing here. Better pedestrian facilities are needed.	0	0
3/16/2019	Route	City/County Right of Way would make a nice trail, or gravel road for emergency evacuation purposes	0	0
3/20/2019	Route	This is a high dangerous area for walkers, especially anyone using a stroller or anything with wheels. The sidewalk ends on the south side and resumes on the north side but there is not crosswalk or curb cutouts. The visibility is bad and cars often drive fast coming down the hill in both directions. Near the top of the hill at Sunset and Brittan there again aren't cutouts so it's difficult to cross. Many walkers use this path to access the trails in Brittan estates.	0	0
3/20/2019	Other	Caltrain	0	0
3/21/2019	Barrier to Walking	"Lynden Lane" public right-of-way is marked "No Public Access", even though it would make a nice way for kids on Sycamore to get to Highland Circle Park or to Arundel. Sure, it isn't ADA-able, but can't the City have a dirt or gravel trail here with some railroad-tie stairs?	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/17/2019	Pedestrian Improvement	Need a crosswalk to connect these two parks	0	0
10/18/2019	Route Improvement	add bike routes to connect to Pulgas Ridge Preserve and Edgewood County Park. Although neither allows bikes, bikes are a great way to get there. Include steep grade warning for bikes on Crestview.	0	0
10/18/2019	Pedestrian Improvement	Increase the number of crosswalks across Laurel St from San Carlos Ave to Arroyo Ave, and reduce the speed limit to 10 mph in the same area. Or remove cars altogether.	0	0
10/18/2019	Pedestrian Improvement	If a traffic circle were placed here, it would make it safer for pedestrians, bikes, and cars, particularly those cars turning left from Hilltop to southbound Alameda. It would also slow the traffic to the posted speed limits (not a freeway). This has DeAnza Ave, Hilltop, Alameda, merging into one high-speed zone.	0	0
10/18/2019	Pedestrian Improvement	There is no crosswalk across either Alameda or St. Francis and we witness people going from 0 mph at the Eaton Ave. stop sign to 60mph to Brittan. This could be a stop sign + crosswalks and be so much safer!	0	0
10/19/2019	Pedestrian Improvement	Crosswalk needed for better visibility/pedestrian awareness. I have seen many, many cars roll through the heavily used stop sign at Garnet and Eaton and near missed with pedestrians and bicyclists at this location.	0	0
10/19/2019	Pedestrian Improvement	Please install a curb bulb out on Orange at Cedar to prevent motorists from turning the corner from Cedar onto Orange at high speed while endangering	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
		pedestrians. Squaring up this intersection will also help left turns from Orange to Cedar.		
10/19/2019	Pedestrian Improvement	Install all-way stop signs here. Motorists speed on this stretch of Cedar, trying to make the light at San Carlos Ave. This endangers pedestrians crossing Cedar and other motorists trying to turn from Cherry onto Cedar. This is a pedestrian casualty or T-bone crash waiting to happen	0	0
10/20/2019	Pedestrian Improvement	Between the tall landscape and drivers going fast around the circle (Morse & elm) it's extremely dangerous to cross at this intersection. Drivers are not looking beyond the entry point to their right. I have seen many people jumping into the circle to avoid getting hit and have had to do it myself multiple times. It is especially dangerous for children due to the tall landscape which is above the driver's eye line. Appreciate the consideration.	0	0
10/20/2019	Route Improvement	On this stretch of Elm cars get very anxious about passing. it would be great to have speed bumps or other road furniture so that a cyclist riding fairly quickly can easily stay ahead of traffic. Or somehow give bikes a head-start at the light at San Carlos?	0	0
10/20/2019	Route Improvement	Can you add flower pots somewhere along here to keep the traffic truly local? Then it would be really like the Bryant Street bike path in Palo Alto, which is the best bike-commute infrastructure on the Peninsula.	0	0
10/20/2019	Route Improvement	It would be great if there was valet bike parking like SFBC offers at Giants' games. We would be far more likely to ride our bikes to downtown and spend money!	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/20/2019	Route Improvement	please improve the bike thru-traffic safety (southbound). the moving lanes and merge are TERRIBLE.	0	0
10/21/2019	Route Improvement	removes section of Eaton with poor visibility and parked cars.	0	0
10/21/2019	Pedestrian Improvement	This intersection is used for several different walking routes but is difficult for walkers even off-peak and with the existing crosswalks. I'd prioritize the north and east crosswalks for improvement.	0	0
10/21/2019	Pedestrian Improvement	Despite the 4 way stop, impatient drivers make this crossing of Brittan unsafe.	0	0
10/21/2019	Route Improvement	Politically difficult, but worth some effort. The alternative is a merge across 3 lanes of Holly to make a left turn whether coming from Twin Dolphin or heading to it.	0	0
10/21/2019	Route Improvement	Reduce speed limit to 25 mph.	0	0
10/21/2019	Route Improvement	Reduce speed limit to 25mph for entire stretch of San Carlos Ave.	0	0
10/21/2019	Route Improvement	Reduce speed limit to 25mph for ADLP	0	0
10/21/2019	Route Improvement	Reduce speed limit to 25mph for this stretch of Brittan	0	0
10/22/2019	Route Improvement	This portion of Cordilleras is a key school route with many children walking, riding bikes, and scootering. In particular, we have had repeated car accidents on the 800 block of Cordilleras and need traffic calming measures. We also need to recognize this as a key pedestrian throughway.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/22/2019	Route Improvement	Shared bike lane on Stanford Lane and on Sunnydale Ave between Stanford Lane and Elm St. This would provide a low-traffic route from Warwick St to Elm St that avoids Eaton Ave which has more traffic and poor sight distance.	0	0
10/23/2019	Route Improvement	Kids want to bike through San Carlos, and a dedicated space would be much safer. We should have a class IV separated paths, perhaps across Cedar, and down Howard. Auto traffic could be converted to one-way to make space.	0	0
10/23/2019	Route Improvement	Most walkers and bikers use Carmelita to get downtown and back. It's quieter and safer. So better to make a medium-term plan to use Carmelita including safer crossing of Alameda and re-opening postman path at the end of Carmelita to cherry street. Even after San Carlos avenue is safer, this Carmelita route will be heavily used so best to make it a better route for walkers and bikers.	0	0
10/23/2019	Route Improvement	Overall comment — This plan presents intersections and lines. Better to start with vision of the plan (a paradise that entices bikes and walkers to leave their cars because it's so safe and enjoyable) and articulate the east-west and north-south routes that we will make safe/wonderful END-TO-END. Total and complete safe routes should be the goal, not points of pain alone!	0	0
10/23/2019	Pedestrian Improvement	Crossing improvement. Flashing sign for cross walk. Currently, most cars are not stopping for pedestrians.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/23/2019	Route Improvement	Please add sidewalks, to improve pedestrian access to Home Depot/Lucky shopping center	0	0
10/23/2019	Pedestrian Improvement	Drivers speed through this intersection without looking since it does not have a stop sign in the Magnolia to Laurel direction.	0	0
10/23/2019	Pedestrian Improvement	Consider a stop sign or speed hump at this intersection to improve pedestrian and vehicle safety.	0	0
10/23/2019	Pedestrian Improvement	Today the sidewalk ends and you must cross traffic without a pedestrian walkway	0	0
10/24/2019	Route Improvement	Elm street should be a viable biking street that connects with RWC biking. To do so requires elimination a significant street parking that makes both walking and biking unsafe and difficult	0	0
10/24/2019	Pedestrian Improvement	I think an entirely pedestrian Laurel St. between San Carlos ave and Arroyo would be great. Currently there is a heavy traffic, lots of kids and pedestrians at stop signs, parking cars, big commotion, especially in busy hours. A pedestrian closure would allow for a way more pleasant restaurant experience, safer walk for families and overall better likability of San Carlos. Additional multi-level public parking could be made along the train at available lots.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/24/2019	Pedestrian Improvement	Needed-a crosswalk and stop signs at the Alameda and DeAnza Ave. Cars are moving too fast and there is no pedestrian crosswalk. It is also difficult to enter the Alameda from DeAnza as southbound cars are obscured by parked cars and northbound cars are obscured by the hill on the Alameda.	0	0
10/24/2019	Pedestrian Improvement	Definitely agree with changing the bike lane so that there is a separate bike lane between the schools (Carlmont, Mariposa, etc.) and downtown. Would like to have children bike to school, but too dangerous with bikes and cars sharing the same lane.	0	0
10/24/2019	Route Improvement	We live on the 1800 block of Carmelita. We were not aware of the proposed reopening of the mailman trail. My understanding is that it was closed due to loitering and "misbehavior" along the path. If the path is reopened, what preventative measures will be made to stop this type of behavior?	0	0
10/25/2019	Route Improvement	There needs to be a safe way to cross 101 and connect to the extensive Redwood Shores bike path system. The current crossing has to go over the highway on and off ramps and is incredibly dangerous and unattractive for bikers, especially kids. Connecting the City of San Carlos bike system to the Redwood Shores bike system would provide a much-enhanced biking experience allowing for multi-hour weekend family excursions without having to cross dangerous locations. We currently don't feel safe there	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/25/2019	Route Improvement	Biking along this stretch of San Carlos ave is incredibly dangerous. There should be a dedicated, protected bike line on both sides of San Carlos Ave, with physical barriers to prevent cars from impinging into the bike lanes. This would allow many students and others to safely bike from the downtown area to Carlmont schools and waterdog lake.	0	0
10/25/2019	Route Improvement	I would highly encourage that the City and it's planning team research best practices from Country's that are considered advanced in terms of bike infrastructure. Specifically, Holland and Denmark. They have dozens of clever and functional designs for bike paths, street crossings etc. that are proven to work and increase bike safety. Why re-invent the wheel when we could import these proven solutions?	0	0
10/25/2019	Pedestrian Improvement	I think a High Visibility Crosswalk Marking or Pedestrian Refuge Island should be added to the corner of Eaton and Cedar. May even consider a stop sign. There are many kids going (mostly biking) to Sequoia down this route and the cars go fast on Eaton.	0	0
10/25/2019	Route Improvement	Sidewalks on both sides of the road and/or wide bicycle lanes would significantly improve safety. This likely means restricting street parking, but given multiple fatalities on San Carlos Ave I believe this is a necessary measure for the safety of children walking/cycling to school.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/26/2019	Route Improvement	Rosewood and Morse is a 4-way intersection that needs a stop sign, currently there are yield signs that are ineffective. It basically an uncontrolled intersection.	0	0
10/27/2019	Pedestrian Improvement	Recommend against opening the long-closed postman's path from Orange, near Central due to serious public safety concerns. This path was originally a walking path for mail carriers, but became dangerous after many incidents of fires (arson and cigarette related), assaults, sexual assaults, burglaries and crimes against children occurred. It was closed in 1989-1990. It is inaccessible to emergency vehicles & runs through very dry trees and ground cover and would be an extreme fire risk.	0	0
10/27/2019	Route Improvement	This closed path is no longer accessible and was closed due to a high number of crimes, including fires, home break-ins, pet poisoning and assaults. SC Police and fire departments responded to many calls and City Counsel closed this permanently in 1990. There are fences and other structures there now. Would be a high fire danger area if opened to public where increased risk of accidental fire from smoking or vaping. Teachers from Central may recall crimes against children when path was open.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
10/29/2019	Route Improvement	Several sections of shoulder uneven due to poor maintenance/tree roots/sinking of the shoulder. Lane painting is severely faded or nonexistent. Large garbage trucks travel at excessive speeds. Street is extremely slippery at entrances to Recology. Flexible traffic delineator posts should be installed along road center-line around entire curve at 375 Shoreway to prevent trucks from crossing over center-line and to aid in slowing them down. Chain link fence damaged encroaches on roadway.	0	0
10/31/2019	Pedestrian Improvement	Need walkway over freeway, please.	0	0
11/1/2019	Route Improvement	Down Devonshire Blvd.	0	0
11/1/2019	Pedestrian Improvement	I see many Arroyo students crossing from School St to go downtown after school, especially mid-day on Wednesdays. From Arroyo, it's a sharp blind turn and I've witnessed several near-misses. We need to have a safer option to cross Elm. (In my experience, kids choose to take the shortest route possible.) I recommend a crosswalk either at School or Olive.	0	0
11/1/2019	Pedestrian Improvement	Need crosswalk at Crestview x Melendy. There are many kids who walk to bus stop and need to cross every morning and afternoon, as well as other pedestrians. There is NO crosswalk between Club Dr & Brittan, and cars go TOO fast down Crestview Dr. There needs to be better options for pedestrians to safely cross Crestview Dr.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/2/2019	Route Improvement	Long Term Future - add PED/BIKE/Scooter undercrossing under Caltrain tracks (can happen with a future Caltrain construction project). Extend bikeway route along Bing to Industrial to give easier access to Bay Trail, etc.	0	0
11/2/2019	Route Improvement	Extend Laurel St route to Belmont City Line. Why? Because the street is wide enough and this is a path to the San Carlos station for residents in this part of the city.	0	0
11/2/2019	Route Improvement	LONG TERM FUTURE PLAN: When shopping center is redeveloped, realign the entrance to Hull Dr and include extending Hull Dr Bikeway across ECR. When Caltrain does construction work in this area, add undercrossing of the tracks and continue Hull Dr route along Taylor Way to Industrial Rd and better/safer Bay Trail access for those in North San Carlos. This is a long-term plan, 20-30 years, but needs shown for future planning.	0	0
11/2/2019	Route Improvement	This ped route exists today	0	0
11/2/2019	Pedestrian Improvement	add missing segment of sidewalk on east side of Laurel St from Oak to mid-block (behind McDonald's.	0	0
11/3/2019	Pedestrian Improvement	flashing ped crossing signal	0	0
11/3/2019	Pedestrian Improvement	flashing ped crossing signal	0	0
11/3/2019	Pedestrian Improvement	flashing ped crossing signal	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/3/2019	Pedestrian Improvement	Make Laurel Street between San Carlos Ave and Olive Street a "walking street" no cars.	0	0
11/3/2019	Route Improvement	Arundel School has a great feature called a front turn around which should be used as such and not a parking lot. Install a traffic signal here and use multiple lanes to allow busses and cars to quickly and efficiently drop off children here which will reduce traffic on local small side streets. Remove the school turn around and parking from Phelps which cannot handle the traffic.	0	0
11/6/2019	Route Improvement	Add Sound wall so folks on the Bay Trail are protected from the freeway noise.	0	0
11/6/2019	Route Improvement	I think most experienced cyclists know this is a good route, but the retaining wall for Caltrain needs to be moved over, in places, to allow the road to be widened. On St. Parking is also a problem/challenge in spots.	0	0
11/6/2019	Route Improvement	Make connection for bikes/peds from Saratoga to Trinity	0	0
11/6/2019	Pedestrian Improvement	Difficult to see around parked cars to cross safely, as driver, cyclist or pedestrian	0	0
11/7/2019	Route Improvement	There is a trail coming out of Devonshire Canyon that used to go through to Crestview, but a corporate landowner put up warning signs to stay out. This trail is on the city's 2030 General Plan in Figure 7-4. This would allow hiker and cyclist access up to and down from Crestview to Devonshire that is not as steep and is completely removed from traffic. This trail bed already exists and could be improved with only a slight realignment.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/7/2019	Route Improvement	This trail appears in the 2030 General Plan and would allow off-road access to and from the lowlands of San Carlos from Crestview, allowing transportation and recreation for hikers and cyclists.	0	0
11/7/2019	Route Improvement	This trail also appears on the 2030 General Plan and would allow foot or bicycle access to the lower slopes of the Melendy hill and to Heather School. This trail, like the others I added nearby, does not need to be paved to provide transportation and recreation benefits to the communities	0	0
11/7/2019	Route Improvement	Trail access to Carlmont High School provides safer access to schools while reducing congestion. There is an existing Trail Alignment that could be improved, no need for pavement.	0	0
11/7/2019	Route Improvement	Trail access to Carlmont High School provides safer access to schools while reducing congestion. There is an existing Trail Alignment that could be improved, no need for pavement.	0	0
11/8/2019	Pedestrian Improvement	This is a blind turn where many children walk on the way to Brittan Acres Elementary school. Adding a sidewalk would make this corner much safer.	0	0
11/9/2019	Route Improvement	Why not continue the Class III Bikeway all the way up Howard, similar to how it is shown on St. Francis?	0	0
11/9/2019	Pedestrian Improvement	This crossing needs the strobe-type pedestrian crossing lights.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/9/2019	Route Improvement	Would be great to have a walking path that connects Crestview to Devonshire, without all the "private property - do not enter" signs at the top.	0	0
11/9/2019	Route Improvement	Add bike safety improvements, including at intersection of San Carlos Ave and Dartmouth, and a left-turn green area at Tierra Linda entrance.	0	0
11/13/2019	Route Improvement	Can we please have a sidewalk on Brittan so we can walk safely especially with kids? Cars drive down at 50mph on Brittan, and it's not safe to walk without a sidewalk. To be honest, I am kind of surprised we don't have a sidewalk already - that's really not that much to ask for?	0	0
11/13/2019	Route Improvement	Taking a turn from Hewitt is really dangerous when cars are hurtling down at much greater than 30mph. Can we please please please have a stop sign at Hewitt and Brittan? There have been so many documented accidents but we are prioritizing people's needs to speed through Brittan to get to 280 over people and children's safety. Or do something else but enforce the 30mph limit on Brittan.	0	0
11/13/2019	Route Improvement	Some of us have been working with the city for 1+ year to no avail so hopefully someone listens here. PLEASE implement measures to restrict speed on Brittan to 30 mph. Folks are using this road as a highway driving at 50mph+. IT IS NOT SAFE. While our city staff has been saying nothing can be done, I see Redwood City implementing safety measures all over. St. Francis Way in San Carlos has also done something similar. Crestview has blocked an entire road so 280 traffic doesn't impact them,	0	0

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Submission time	Type	Comment	Likes	Dislikes
11/13/2019	Route Improvement	Some of us have been working with the city for 1+ year to no avail so hopefully someone listens here. PLEASE implement measures to restrict speed on Brittan to 30 mph. Folks are using this road as a highway driving at 50mph+. IT IS NOT SAFE. While our city staff has been saying nothing can be done, I see Redwood City implementing safety measures all over. St. Francis Way in San Carlos has also done something similar. Crestview has blocked an entire road so 280 traffic doesn't impact them,	0	0
11/13/2019	Pedestrian Improvement	There is a crosswalk at Tamarack and many kids and families use this route to get to Brittan Acres elementary school. I used to cross there every morning with my daughter and many cars would not stop even with the flashing light that is currently in place. There need to be pedestrian beacons like rectangular rapid flash beacons on the ground that can be seen from a distance. I drove in Mountain View where I saw these at many busy crossings and these got my attention.	0	0
11/13/2019	Route Improvement	*Stop sign at Hewitt/Other means to regulate traffic speed on Brittan between Alameda and Crestview* - Cars and trucks drive north of 50mph daily. Most of these folks are likely headed to 280 and using Brittan for thoroughfare. My neighbor spent months late last year working with the city, the police, the surveyors who generally acknowledged yes speeding is an issue here, yes there are accidents because of this but really didn't do anything because they don't want to slow down traffic to 280.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/13/2019	Route Improvement	Remove unlawful barrier on public roadway. This is a salient safety issue in an era of wildfires. I'd like to see the city take some leadership on this as fires are only going to worsen in California.	0	0
11/14/2019	Pedestrian Improvement	Add curb cuts to facilitate accessibility between neighborhood and Highlands park	0	0
11/14/2019	Pedestrian Improvement	Add curb cuts to facilitate accessibility between neighborhood and Highlands park	0	0
11/14/2019	Pedestrian Improvement	Add curb cuts on Hewitt Drive to make the sidewalk more usable.	0	0
11/14/2019	Pedestrian Improvement	Sidewalk or other safe way to get from Alameda/Brittan up to Hewitt/Brittan. Thank you	0	0
11/14/2019	Pedestrian Improvement	Curb cutouts along Hewitt Drive	0	0
11/14/2019	Pedestrian Improvement	<p>A new sidewalk on this part of Brittan (Hewitt to Alameda) would increase foot traffic to all of San Carlos/Burton Park/Schools/Churches.</p> <p>What is currently there is unsafe, broken and forces people to cross Brittan as the sidewalk abruptly ends on one side. If I'm reading this correctly, it looks like this is being proposed and that is fantastic.</p>	0	0
11/14/2019	Pedestrian Improvement	We need a crosswalk to get from one side of the sidewalk to the other side of the sidewalk on Brittan between Alameda and Sunset. There is a church, a preschool and a bus stop there.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/15/2019	Pedestrian Improvement	Sidewalk needed from Brittan up to Crestview. There's no safe way to walk, and I'd love to get to the park without driving and parking!	0	0
11/19/2019	Pedestrian Improvement	A cross walk is needed here for children to cross Crestview Dr. in order to reach the SamTrans bus stop on Melendy, as well as walking to Heather School.	0	0
11/19/2019	Pedestrian Improvement	Not very safe for walking kids to school	0	0
11/19/2019	Pedestrian Improvement	Not always safe for crossing with young kids	0	0
11/19/2019	Pedestrian Improvement	Many people use the hidden stairs here to walk into town and the side walk is so bad on the north side on San Carlos that it would be nice if there was a crosswalk with blinking light at the bottom of vista to cross San Carlos ave safely	0	0
11/19/2019	Route Improvement	Edgewood from Crestview to and from 280 is very hectic for bike riders. A traffic light might help for turning at the Crestview intersection and widening the rode/bike area might help for riding up and down the road on Edgewood.	0	0
11/22/2019	Route Improvement	There needs to be a clearer Path leading to Sequoia High School. several San Carlos bikeways leading into Eaton, Warwick, Duane, Brewster, etc.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
11/22/2019	Route Improvement	Alameda De Las Pulgas. currently ADLP is the only decent North-South Corridor we have along the Peninsula. This could be a great commuter and recreational bikeway leading from Belmont all the way to Palo Alto and Mountain View. But it needs improvement here and San Carlos needs to put pressure on Redwood City and the County to finish their stretches.	0	0
11/22/2019	Route Improvement	At one point Eaton/ADLP need to have a roundabout there. Eaton leads to Clifford Elementary and through Warwick over to Sequoia HS. This stretch along ADLP and Edgewood needs some serious re-design starting with a roundabout on Eaton.	0	0
11/22/2019	Pedestrian Improvement	There are blind spots from Manor Dr/Carmelita where cars zoom by and it is very hard to get on the sidewalk. The sidewalk should be longer and a crosswalk added to give driver awareness of people crossing.	0	0
11/29/2019	Pedestrian Improvement	Need a 4-way STOP sign here. It's a main route to White Oaks school that a lot of families and children use. Cars drive too fast on St. Francis.	0	0
11/29/2019	Pedestrian Improvement	Need a 4-way STOP sign here. A lot of cars and trucks speed through this intersection driving along Park Avenue in both directions. Super dangerous. There has been 1 accident recently due to reckless driving where a car ended up in the front yard of 635 Park Avenue. Lots of children, families, and people walking their dogs through this intersection and the adjacent blocks.	0	0

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
12/17/2019	Pedestrian Improvement	This looks like a waste of time and money for very limited, if any, improvement for pedestrians. It seems like a great place for teens or homeless to hangout.	0	0
1/12/2019	Comment	Convert to cycling comment.		
1/8/2019	Comment	Traffic light to cross to El Camino to the Caltrain Station takes really long which causes people to sometimes dangerously jaywalk in order to catch a train. A bridge/underpass would be a great addition for pedestrians		
1/11/2019	Comment	Would love to find a safe and easy route across 101 to get to the bay trails. One note though: I'd hate to have to spend millions to build a single purpose bridge. There has to be a better and easier way to make a safe route to the bay.		
1/13/2019	Comment	Couldn't agree more. This "bike lane" is a death trap. I detour to the Belmont pedestrian bridge as well. Maybe a short, straight tunnel somewhere? Protected bikeway in the median? Just don't put cyclists between rushed drivers missing their freeway entrances.		
1/9/2019	Comment	I would like to see the overhead flashing light at El Camino and Belmont changed from a yellow flashing light to a red stop light like the ones up and down the El Camino. They should all be the same.		
1/9/2019	Comment	I think our neighbors in Redwood City and Belmont are planning bike lanes throughout their cities on ECR -- we should keep that stretch of bike lane going through San Carlos		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/10/2019	Comment	I agree! And ideally the bike lanes would be protected -- like redwood city		
2/22/2019	Comment	Maybe San Carlos could implement what I've seen in downtown San Mateo - the white 'walk' lights for pedestrians go on first for several seconds before traffic lights in the same direction turn green (so pedestrians get a head start in crosswalks before cars can turn left or right)		
1/9/2019	Comment	San Carlos Avenue needs effective and real traffic control. I never see any traffic control there. Crossing San Carlos Avenue can be dangerous for pedestrians. The intersection of San Carlos Avenue and Prospect St. is a dangerous one. A 4-way stop is needed.		
1/9/2019	Comment	Agree that this area is dangerous -- the whole San Carlos Ave from about Cordilleras to Caltrain should have parking protected bike lanes, meaning [road car parking bike lane sidewalk] so bikes aren't next to moving cars. Additionally, cars move fast because the road here is excessively wide and it feels like a freeway. A road diet (slim the road!) greatly helps safety because it causes drivers to proceed more cautiously (read: slowly).		
1/11/2019	Comment	Westbound biking & walking is problematic as well		
1/14/2019	Comment	100% agree. It is very challenging to cross between parks with cars doing 45 MPH plus on curves		
2/13/2019	Comment	We don't need a crosswalk - it's a sharp hill. People need to park on the correct side and not cross to the park.		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/9/2019	Comment	I didn't know this used to exist, but I'd love to be able to use it!		
1/9/2019	Comment	I've taken to riding in the opposite direction bike lane on San Carlos Ave for the short distance from Cedar St to Cordilleras Ave due to this problem. Definitely need a fix for this situation		
1/12/2019	Comment	An underpass might be more practical and cheaper, but we need a solution to easily bike/walk from San Carlos to Redwood Shores		
1/15/2019	Comment	Bianchini's		
1/15/2019	Comment	My wife and I walk San Carlos up to three times a week between 3-4.5 miles. We walk different routes between Eaton, Laurel St, San Carlos Ave., and The Alameda De Los Plugs		
1/9/2019	Comment	Because of the poor sidewalks on San Carlos Ave itself, I always take Carmelita Dr into the downtown Laurel St area from my house on Plymouth and avoid San Carlos Ave as much as possible. Especially with a stroller or dog.		
2/4/2019	Comment	This area needs more defined crosswalks and bike lanes. This is a major route for kids from the White Oaks area ridding/walking/scooting to Arroyo and Central.		
1/10/2019	Comment	That suggestion seems not quite feasible.		
1/12/2019	Comment	Elevated would be ideal. Barring that, bikes need a physical barrier from the traffic.		
1/12/2019	Comment	It is outright dangerous by bike		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/11/2019	Comment	Would love to find a safe and easy route across 101 to get to the bay trails. I'd hate to have to spend millions to build a single purpose bridge. There has to be a better and easier way to make a safe route to the bay.		
3/1/2019	Comment	While bikers need to obey the rules of the road as if they were cars, bike lanes would actually help solve the very problems that are being mentioned here.		
1/15/2019	Comment	When crossing street to reach the only sidewalk I have been leery of the cars accelerating because of dip in road. This is a place where an accident will happen!		
2/13/2019	Comment	I agree with this comment. we need sidewalks that run on each side continuously from the top of Brittan all the way down.		
1/15/2019	Comment	This stretch of road is very narrow, with fast moving cars. When students are waiting for bus they spill onto the road. The size of the waiting area is too small, dangerously so. Amazing that this was not taken into consideration by the city when the new construction was approved		
1/10/2019	Comment	I'd be surprised if the city owns that property. I agree stairs would be ideal but that might require the Church to provide the land.		
1/12/2019	Comment	There is an informal path here, but it should be improved and made safer in wet weather.		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/11/2019	Comment	It's dangerous to try to cross here. Cars are accelerating in one direction and the opposite traffic has a blind area. My daughter walks this way to school and there needs to be an obvious crosswalk for all.		
1/11/2019	Comment	This flashing pedestrian walkway across El Camino feels very dangerous. It's difficult to see if there is a pedestrian crossing and many vehicles do not stop even when the lights are flashing. With a new parking lot on the other side of El Camino this may become a bigger problem.		
1/12/2019	Comment	It is difficult to walk down old county rd because the drivers don't see you as the exit from the side streets. They are looking for cars coming down the road but they don't look for pedestrians walking down the sidewalk. I have almost been hit several times.		
1/12/2019	Comment	I would not feel safe riding my bike on this road as the road isn't wide enough for both bikes and cars. If you try to pass a bicyclist, then you be driving into oncoming traffic which is another death trap. We need bike lanes that are wide enough and clearly marked with green paint		
1/13/2019	Comment	I biked to work on this road daily for a few months. The road condition is not very good in some places, and the bike lanes disappear in some areas. In the parts without bike lanes, it's stressful and only die-hard cyclists use the route (need to be biking with mirrors, taking the lane, etc.). It's a good stretch of straight road, and would make a great cycling highway, but needs protected bike lanes for that to really work. Then you just need to cross El Camino...		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/25/2019	Comment	Would be ideal to have a clear bike route for kids going to/from Sequoia. So many kids make this trek every day, a safe and clearly marked bike route is really needed.		
3/1/2019	Comment	This problem could be solved by not allowing parking between Holly and the entrance to Trestle Apartments. Cars parked along block visibility into the apartment parking lot and make it extremely difficult for cars exiting Trestle Apartments to see oncoming traffic.		
3/5/2019	Comment	I too would like a pedestrian bridge to redwood shores, but not here.		
1/13/2019	Comment	sorry, wrong location on the map. The pothole is right across the first entry to Carlmont High school when coming from San Carlos		
2/4/2019	Comment	36" isn't enough. That is as wide as the front door on many houses! These sidewalks are for pedestrians. Every encroachment into the sidewalk should be signed by the business telling us why they are allowed to take this space and that non-customers are welcome to bring their own food and sit there. There should be some visible permit issued by the city allowing each encroachment.		
2/27/2019	Comment	Bike lane disappears after Elm street down to train station. Some type of colored paint to indicate bike share would be good here as this is the last crucial part to connect bike to Cal Train.		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
3/4/2019	Comment	This would just push the problem onto Holly, which is more residential and even less able to safely handle the traffic flow. While I absolutely support making downtown more walkable, I don't think that closing off *the* main roads that leads into it (and pushing that traffic onto a more residential street) is the way to do it.		
2/18/2019	Comment	For many of us who live north of San Carlos Avenue, this is the *only* direct walking route to Laurel Street. We should focus on improving pedestrian/bike safety along San Carlos Avenue, not forcing pedestrians to walk somewhere else for the convenience of rude and distracted drivers.		
2/4/2019	Comment	San Carlos Ave should have the stop lights timed to less than 20 mph. Leave the speed limit at 25 but put a sign up. Take away the incentive to speed by making the cars stop at the intersections and run slower at crosswalks because it just doesn't do any good to go faster.		
2/13/2019	Comment	Car left turns at Elm make it nearly impossible to walk cross even with a "walk light" during commute without risking your life.		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
1/18/2019	Comment	I walk daily about 2.5 mi winding around Beverly, Charlton, Plymouth, Cambridge, & Devonshire, & Molton. The lack of sidewalks everywhere except Devonshire makes this extremely dangerous after 3 pm when commuters start racing thru these streets as shortcuts from Edgewood Rd to Ralston. Complaints to the City about traffic enforcement goes on deaf ears. I used to walk up Windsor to Chesham & Faye, but the traffic after 3 pm is just too dangerous now. Many dog walkers in this area.		
1/21/2019	Comment	diagonal intersection walking would be so much fun! Closing down laurel st to decrease stress for everyone will be good for residents and then great for business. Absolutely the best idea ever!!		
3/5/2019	Comment	I agree, we need to start looking at redirecting traffic via one-way streets		
2/12/2019	Comment	I agree - this needs a flashing beacon such as the one at Tamarack. It is very dangerous and the adult folks crossing the street look frightened.		
2/12/2019	Comment	This is a great idea!		
2/12/2019	Comment	And if you gave upper Brittan a sidewalk, they could use it too. I would have loved this when my kids were at St. Charles.		
1/21/2019	Comment	There should be dedicated bike path for bikers and sidewalks should just be for pedestrians.		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/4/2019	Comment	In Addition to Margaret suggestions, we need to add some sort of traffic calming to accommodate the new pedestrian/bike plan. More ADA crosswalks, defined cross walks, and other proven measures to make it safe for bikes/kids/pedestrians.		
2/17/2019	Comment	Would like to see the traffic light respond to the presence of a bicycle waiting on Arroyo to cross El Camino		
3/1/2019	Comment	Agree with this comment however I believe this is a SamTrans issue, not the City. It would be great if SamTrans would do more to make these areas pedestrian and bike friendly.		
3/5/2019	Comment	We desperately need to have a bicycle and pedestrian overpass midway down Holly. or at least a flashing crosswalk when pedestrians are present. We have a ton of strollers, pets, kids, and bicyclists crossing in the middle of the street to get to the other side. You can't blame them. None of the intersections cater to more than 2 people waiting to cross.		
3/1/2019	Comment	Crossing here as a pedestrian can be very difficult. Many cars do not keep an eye out for pedestrians. The walk sign flashes so quickly that if a car cuts you off before you can step into the intersection the light has already switched back to "don't walk" before you can begin crossing. The following cars then believe that they have the right of way and do not stop for pedestrians.		
1/21/2019	Comment	It's a downtown. Not a car-town.		
1/21/2019	Comment	Need a lot a lot more bike parking.		

Input Map Suggestions				
Submission time	Type	Comment	Likes	Dislikes
2/17/2019	Comment	would like traffic light to recognize bicycles on Cedar to allow crossing of San Carlos Ave. Ditto for Chestnut, Elm, and Walnut streets		
3/5/2019	Comment	Absolutely agree... we need to consider making E San Carlos and Holly Street both multi modal. bikes and pedestrians are the answer...cars are the problem.		
2/12/2019	Comment	Me too, but I have no safe sidewalk from Upper Brittan Ave so I'm forced to drive my car.		
3/4/2019	Comment	Agreed. A dedicated bike lane on Cedar Street would help make Burton Park and the elementary schools much more accessible by bike, particularly for children.		
3/6/2019	Comment	please take the city owned r/w to widen the street especially at the turns to potentially add bike lanes or at least wide enough right lane that vehicles and bikes have more space. Better signage (e.g., overhead XX) for parking lanes in use		
3/6/2019	Comment	adding street light near the Yield sign would be helpful; Yield sign hid behind vehicles especially at night		
3/7/2019	Comment	additional enhancements: add a speed bump and/or paint in crosswalks		

Written Comments



Courtney Wood <courtneywood@altaplanning.com>

FW: Bicycle lane thoughts

2 messages

Grace Le <GLE@cityofsancarlos.org>

Fri, Feb 15, 2019 at 11:42 AM

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

FYI on the bike comment.

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

From: [REDACTED]

Sent: Friday, February 15, 2019 10:38 AM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: Bicycle lane thoughts

Dear Ms. Le,

Nice to meet you at the recent input session at Arundale Elementary School. I thought it was a very well organized presentation. I learned about a San Carlos project that I had not been aware of. Though I drive and walk through the project area on occasion, I live on the southwest side of San Carlos and don't always take that route. The proposed improvements seem good to me.

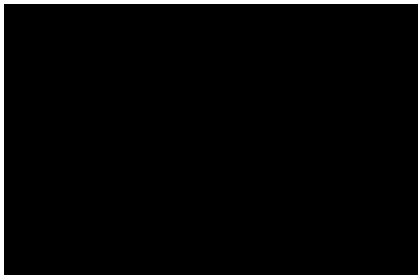
I guess my comments on bike/pedestrian/car interactions take a slightly different angle. As the city moves forward with more bike paths, better pedestrian pathways and such, I would like to advocate for bike rider education. I travel up and down Howard Ave., Brittan Ave., and Alameda De Las Pulgas, and the cross streets like Cedar, Elm, etc, quite a bit. Often I encounter small groups (families, school boys, bike clubs) that also travel along these streets. I am supposed to share the road with these bike riders. OK. But they routinely ride two/three abreast, blow through stop signs and intersections, and have a general disregard for cars. I thought bicycles were subject to the moving vehicle laws and must obey traffic signs.

So, my thought was perhaps bike riding education might be included at our local schools.

I am a bit worried that as we move forward with more bikes (or electric scooters to get up the hills?) we will just compound these interactions.

OK. thanks for letting give my two cents. I do like the direction the city is headed. And I like the way the city management handles community input.

Thanks,



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Courtney Wood <courtneywood@altaplanning.com>

FW: Bicycle pedestrian master plan

1 message

Grace Le <GLE@cityofsancarlos.org>

Wed, Jan 16, 2019 at 4:00 PM

To: Courtney Wood <courtneywood@altaplanning.com>, Hugh Louch <hughlouch@altaplanning.com>

FYI.

Grace Le, PE
City Engineer
City of San Carlos
600 Elm Street, San Carlos, CA 94070
(650) 802-4201

-----Original Message-----

Sent: Wednesday, January 16, 2019 3:50 PM
To: Grace Le <GLE@cityofsancarlos.org>
Subject: Bicycle pedestrian master plan

I wasn't able to make the meeting last night but did complete the attached survey. I want to share several thoughts and some photographs as a long time city homeowner and resident who spends a lot of time in the city on his bicycle and also walks regularly.

There are a number of violations nearly every minute of the day at the intersection of El Camino Real and Holly Street. I live nearby and usually count the number of cars either blocking the intersection or blocking one of the crosswalks. The attached photos will show you that I'm talking about the south east portion of the intersection. I generally see 2 to 4 or more vehicles in violation at any one time. This will only be exacerbated, the danger to pedestrians, as new residence continue to occupy the new apartments.

There's a similar problem under the train tracks at Britton and at Howard Avenues

There's a growing situation where automobile owners feel it's OK to park their vehicles on sidewalks, in particular Eaton Avenue. But other street as well including Howard and Olive.

Drive down Eaton after dark and you will see dozens of cars and trucks parked partially on the sidewalk. Some of them use just a portion of the sidewalk others, as one photo depicts, take up most of the sidewalk denying anyone in a wheelchair the ability to use the sidewalk. Many people on Eaton and elsewhere use the sidewalk to place their garbage and recycling bins further blocking pedestrian access.

Everything I pointed out makes it difficult and dangerous for pedestrians and it's all illegal. Education and enforcement I think is the answer and possibly some signage. I'm looking forward to working with the city to make this a safer place for people who walk and bicycle.

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https://us2.proofpointessentials.com/index01.php?mod_id=11&mod_option=logitem&mail_id=1547682619-YwrY2nbO-Efd&r_address=gle%40cityofsancarlos.org&report=1

9 attachments



image5.jpeg
158K



image4.jpeg
163K



image3.jpeg
135K

image2.jpeg
137K



image1.jpeg
129K

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Courtney Wood <courtneywood@altaplanning.com>

FW: San Carlos Bicycle and Pedestrian Master Plan

1 message

Grace Le <GLE@cityofsancarlos.org>

Wed, Feb 27, 2019 at 11:54 AM

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

FYI


Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201


Sent: Tuesday, February 26, 2019 1:39 PM**To:** Grace Le <GLE@cityofsancarlos.org>**Subject:** San Carlos Bicycle and Pedestrian Master Plan

Hi Grace,

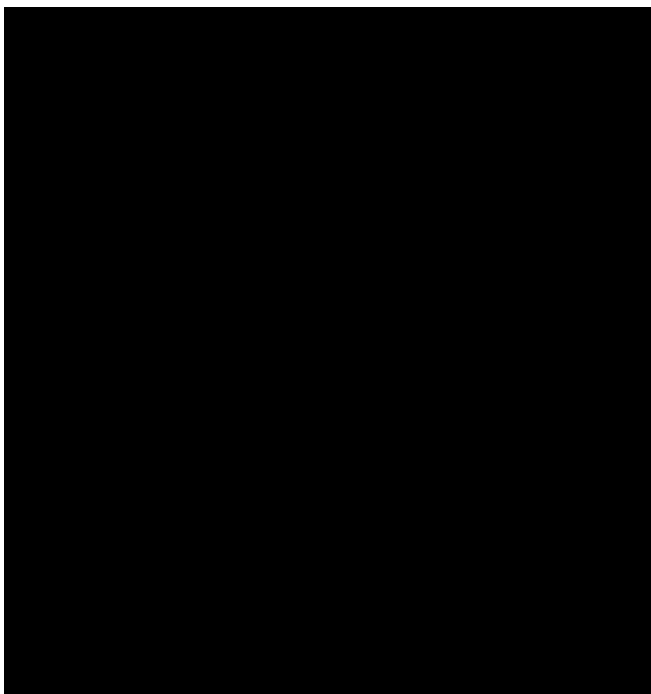
Thank you for creating the [public site](#) with updates on the City's Bicycle and Pedestrian Master Plan, and more importantly, for including your contact information online. Unfortunately, I am unable to attend the March 5th open house, so here's my feedback:

1. The City needs clearly marked green bicycle lanes (like they have throughout Palo Alto and on Alameda de las Pulgas immediately before you get to Woodside Road from San Carlos). This is especially important around schools and busy intersections where there are designated right turn lanes for cars. Cars

crossing bicycle lanes are especially dangerous.

2. The City needs to think about bicycle commuters who live/work in San Carlos and who travel to/from neighboring cities. For instance, I bike from San Carlos to Palo Alto, where I work. I take Alameda de las Pulgas past Edgewood Drive, which is a notoriously dangerous intersection. Please remember to factor in bicycle commuters who need good entry/exit points to San Carlos.

3. If you want to draw lessons from a city that has done an amazingly good job of becoming bicycle-friendly, look at Copenhagen, Denmark. It seems like there are more bicycles than cars in Copenhagen but everyone manages to get along. No need to reinvent the wheel when other cities have already found great solutions.



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Courtney Wood <courtneywood@altaplanning.com>

FW: Bike Boulevard Design Proposal (One-way Cedar/Elm)

Grace Le <GLE@cityofsancarlos.org>

Thu, Mar 14, 2019 at 2:45 PM

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

FYI

Grace Le, PE
City Engineer
City of San Carlos
600 Elm Street, San Carlos, CA 94070
(650) 802-4201

-----Original Message-----

Sent: Thursday, March 14, 2019 9:26 AM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: Bike Boulevard Design Proposal (One-way Cedar/Elm)

Attached is a rough sketch of a design proposal for making Cedar and Elm one-way streets (for cars) while still accommodating 2-way bike/human-powered wheels traffic.

Can you let me know what legal/engineering/safety hazards this design presents, so I can start brainstorming workable solutions? Also, may I present this at next week's traffic circulation meeting?

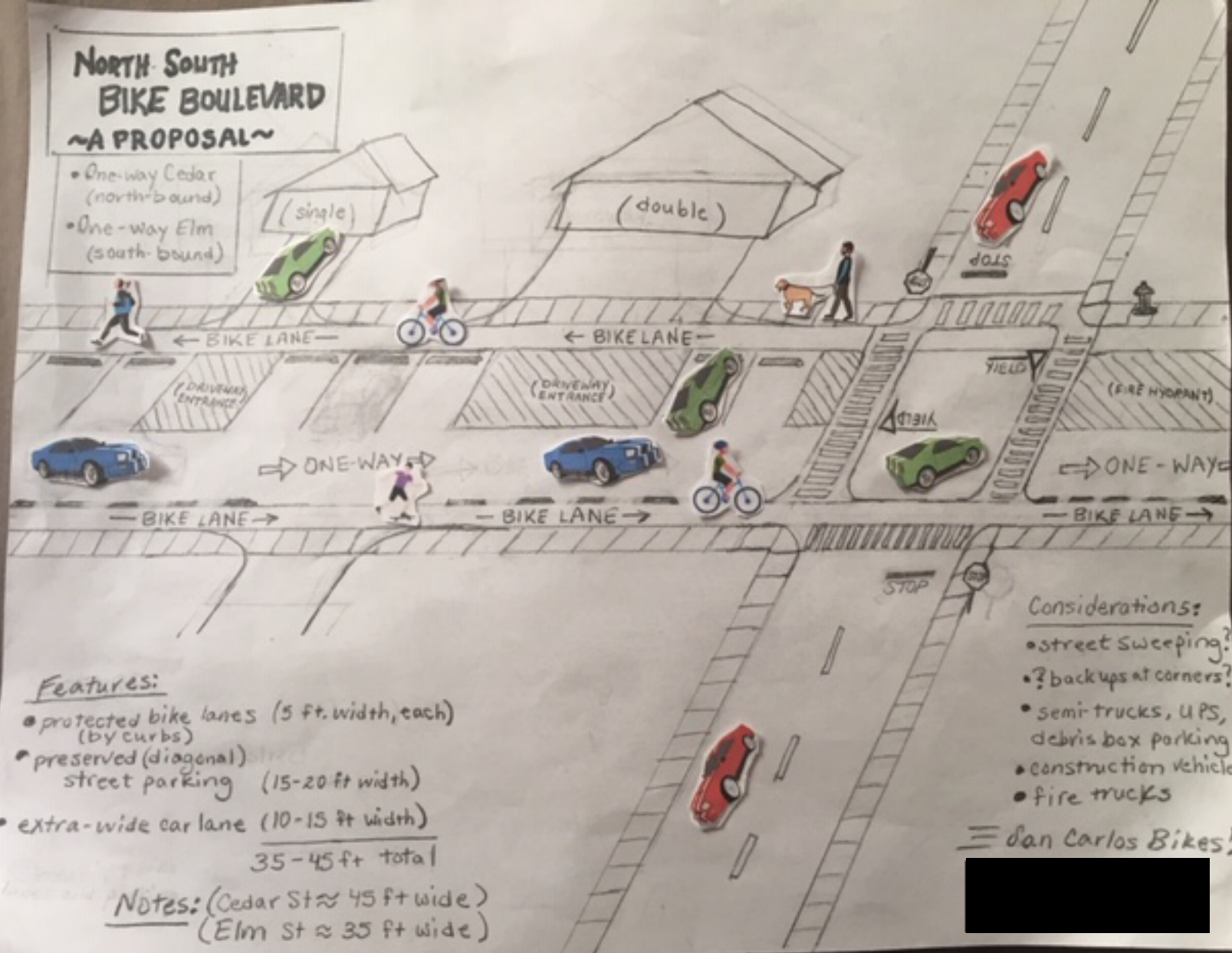
Second, while ideally a bike lane would be at sidewalk level (higher than the street), the way this proposal is laid out, bike lane is at street, not sidewalk, level, with intermittent curb/barriers alongside to protect users from moving vehicles, except at intersections.

Thirdly, in thinking about the problems that driveway aprons represent to pedestrians, wheelchair users, stroller users, etc, I am wondering why more gradual slopes aren't used everywhere, not just at corners? you had mentioned street parking space would decrease, but there is no reason people couldn't park along side a lower, even street level curb if a sign or paint indicated it was ok, is there? If the problem is cars will inevitably drive up on the sidewalk, a possible solution would be to leave curb at same height, but drop the sidewalk level gradually between driveway and street level. That would mean sidewalk slope would be gradual enough not to impede wheelchairs, strollers and sidewalk users, but curb would remain the same to keep cars off sidewalks. Does that separation of curb height and sidewalk height at edge of driveways pose a legal/safety or engineering problem?

PS-I will try also to drop off a copy of this sketch for easier reading.

NORTH-SOUTH BIKE BOULEVARD ~A PROPOSAL~

- One-way Cedar (north-bound)
- One-way Elm (south-bound)



Features:

- protected bike lanes (5 ft. width, each) (by curbs)
 - preserved (diagonal) street parking (15-20 ft width)
 - extra-wide car lane (10-15 ft width)
- 35-45 ft total

Notes: (Cedar St \approx 45 ft wide)
(Elm St \approx 35 ft wide)

Considerations:

- street sweeping?
- ? backups at corners?
- semi-trucks, UPS, debris box parking
- construction vehicles
- fire trucks

San Carlos Bikes!



Courtney Wood <courtneywood@altaplanning.com>

FW: Bike & Ped Mstr Plan - wikimap not saving changes

Grace Le <GLE@cityofsancarlos.org>

Mon, Mar 18, 2019 at 9:22 AM

To: Courtney Wood <courtneywood@altaplanning.com>, Hugh Louch <hughlouch@altaplanning.com>

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Sunday, March 17, 2019 4:03 PM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: RE: FW: FW: Bike & Ped Mstr Plan - wikimap not saving changes

Hi Grace,

Here is my list of bike/ped/scooter/etc. "improvements":

- a general item is to increase street lighting on key routes to make them safer (i.e., more visibility of bikes/peds/scooters and more visibility of the street itself)

- add a sidewalk along Alameda De Las Pulgas next to Tierra Linda Middle School
- complete the missing section of San Carlos Ave from Devonshire Blvd/Wellington Dr to north of Beverly Dr to connect with existing sidewalks and the future improvements on San Carlos Ave
- at the Elm St/Holly St intersection & nearby Elm St/Magnolia Ave
 - remove parking on the east side of Elm St between Holly St and Magnolia Ave to increase the safety of traveling NB/SB on Elm St between these two intersections while dealing with traffic at both intersections.
 - On Holly St, perhaps make a RT/LT lane so it is clearer which way traffic is turning
 - Perhaps on Elm St to the SB direction from Holly St., red curb farther back to increase the sight distance for LT Holly movements
 - the above changes then can help with bike movements traveling through this area
- St. Francis Way (a longer term plan): install "bike lanes" and add a bike/ped undercrossing under the Caltrain tracks that connects to Old County Rd and then Bing St. The "bike lanes" travel down Bing St to Industrial Rd. which has bike lanes on it going either way. Going SB on Industrial Rd one enters RWC which continues the lanes. (RWC has a missing piece on Whipple Ave to connect to the Bay Trail.) Going in the reverse direction is also valuable all way back to Alameda DLP and beyond.
- Work with Redwood City and SamTrans to "extend" the bike lanes on Skyway Rd across Airport Way along Pico Blvd to connect with the trails that go around Redwood Shores. This addition would also make it easier for those traveling over the new Bike/Ped 101 overpass to connect to these trails.
- F St. underpass of the Caltrain tracks: WB side - widen the existing sidewalk from ECR; EB side - improve the crosswalk on Old County perhaps by "cutting off" the wall corners a few back in either direction so there is more visibility in all directions.
- Laurel St between F St. and Hull St: add stripping on Laurel b/w F St and Spring St. Currently, the stripping just stops at Spring St. On the part from Spring St to Hull St, remove the center turn lane and add bike lanes on either side. May need to convert the diagonal parking into parallel parking.
- Add a sidewalk on the east side of Laurel St from Hull St to about 1/2 the way north.

- Add a street light at Hull St/Elm St intersection immediately above the Yield Sign area on Hull St. The yield sign is missed especially at night, and the current lighting doesn't cover this side of the intersection. (Vehicles and bikes will travel through this intersection w/o slowing down many times even at night). This is a risk to bikes/scooters that are traveling along Elm St in this area. I've walked this and have had a few close calls (thought not as bad as Holly/ECR crossings).

- On Warwick St with RWC limits, "better" connection to other bike lanes in the area in San Carlos (e.g., Cedar, Elm, etc.). I'm not sure the options though given the street grid layout in the area.

- A way out there idea and longer term is to install a linear park with bike/ped lanes/sidewalks from Redwood City limits to the station with ped/bike bridges over Brittan and Howard. I've seen parks like this done well in other parts of the world. Ideally, this would have gone all the way to Belmont, but the new Transit Village makes that hard.

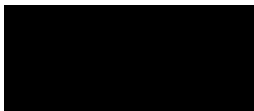
- Some other areas the wikimap showed multiple comments already in the same spots. I won't duplicate those here.

Lastly, I'm Looking forward to new Holly 101 bike/ped overcrossing bridge that is coming.

Thank you,



----- On Mon, 11 Mar 2019 15:46:37 -0700 **Grace Le** <GLe@cityofsancarlos.org> wrote -----



Unfortunately, we don't see any of those routes on the map. Please send me your submissions and we'll manually add.

Sorry for the inconvenience!

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Monday, March 11, 2019 1:10 PM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: Re: FW: FW: Bike & Ped Mstr Plan - wikimap not saving changes

Here are a few examples that I think will be easy to "see":

- a route on Holly St. from ECR to Elm, then on Elm till Magnolia; with some comments including removing the parking on the east side of Elm b/w Holly and Magnolia
- a route continuing St. Francis Way across ECR and Caltrain tracks for a possible undercrossing with a comment about that
- a route along Alameda de las Pulgas from San Carlos Ave/Cranfield Ave to Chula Vista Dr commenting on the need for sidewalk on the east side along the Tierra Linda School property.

This last one above was the first one I had added when I started using the wikimap and noticed early on it didn't save. Then I reentered it again at which point it did save. I went on to add a bunch more all that looked to be saving. I noticed over the weekend none of the

routes had been saved. I entered it a 3rd time as a test. It didn't save. Thus, I decided to email you at that point.

I figure they can see if these routes are there on their end. If yes, then the rest of my submissions are probably there. If not, I'll plan to get you my submissions to forward to them.

Thank you for the follow-up!

----- On Mon, 11 Mar 2019 12:42:03 -0700 Grace Le <GLE@cityofsancarlos.org> wrote -----

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

From: Hugh Louch <hughlouch@altaplanning.com>

Sent: Monday, March 11, 2019 12:40 PM

To: Courtney Wood <courtneywood@altaplanning.com>

Cc: Grace Le <GLE@cityofsancarlos.org>

[Quoted text hidden]

[Quoted text hidden]

[Quoted text hidden]



Courtney Wood <courtneywood@altaplanning.com>

FW: Several pictures of another problem street

1 message

Grace Le <GLE@cityofsancarlos.org>

Wed, Jan 30, 2019 at 9:07 AM

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Wednesday, January 30, 2019 7:58 AM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: RE: Several pictures of another problem street

Hi Grace,

Below is an e-mail expressing concern about biking and walking conditions on Lupin Way.

In the past when the Sheriff's Deputy attended a TCC meeting, I asked about parking on the sidewalk, and my recollection was that he said it is OK as long as people can pass. Do you know how much space must remain on the sidewalk? It looks in these photos like the sidewalk is almost completely blocked.

Thank you.

[REDACTED]

Sent: Saturday, January 26, 2019 9:53 PM

[REDACTED]

Subject: Fwd: Several pictures of another problem street

[REDACTED]

I am forwarding a transportation concern (with photos, below) sent to me by resident [REDACTED] dedicated bicyclist in San Carlos. (Fyi, Elm Street near Howard Avenue has a similar problem—street is scary to bike along because so narrow, and sidewalks are taken up by parked cars.)

[REDACTED] if you haven't, can you add this problem on Lupin (and any other concerns/suggestions you have identified) to the new City of San Carlos Master Pedestrian/Bicycle Plan Survey? Link here: <https://www.cityofsancarlos.org/government/departments/public-works/streets/bicycle-and-pedestrian-master-plan>

For now, consolidating as much feedback as possible through the survey and on the interactive city map I think is the best way to communicate areas of traffic & circulation concerns and suggestions. However, in the future (once survey no longer active) most direct route will be through the traffic commissioners, whose contact information and when their terms expire can be found here:

[transportation-and-circulation-commission](#)

Thanks for the heads up!

[REDACTED]

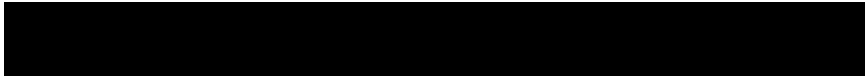
PS to all—There's interest in having a group of bicyclists in the Hometown Days parade this year. [REDACTED]
Can you spread the word and see who else might be interested in being a presence in the parade? Like a "critical mass", but without being a nuisance :) ...Maybe we can carry a banner about the work being done on the Bicycle/Pedestrian Masterplan the City is working on. [REDACTED]
organizers) last year had mentioned interest in potentially keeping Laurel Street closed after the parade for a few hours to encourage biking there, as well as the

1/30/2019

Alta Planning + Design Mail - FW: Several pictures of another problem street

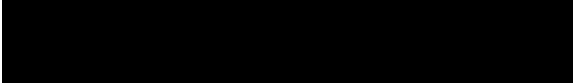
even bolder idea of closing the streets all around Burton Park to traffic, also to encourage more human powered traffic like bikes, skateboards, scooters, joggers, etc. Lmk what you think

Begin forwarded message:



Subject: Several pictures of another problem street

Date: January 26, 2019 at 3:19:03 PM PST







I sent some photos and suggestions of problem areas for pedestrians and bicyclists in the area. Cars parked on the sidewalk. Illegal of course. Clearly the issue was poor design decades ago. I learned to bicycle on the sidewalk of someone disabled in a wheelchair to maneuver.



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Courtney Wood <courtneywood@altaplanning.com>

FW: Suggestion on pedestrian access for downtown San Carlos

1 message

Grace Le <GLE@cityofsancarlos.org>

Mon, Jan 28, 2019 at 11:53 AM

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

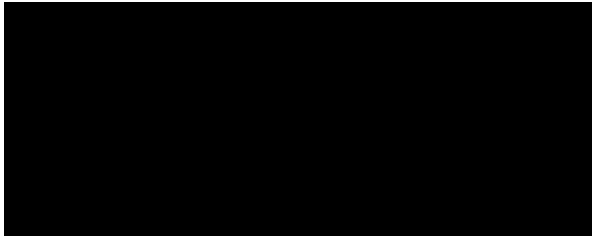
(650) 802-4201

Sent: Monday, January 28, 2019 10:47 AM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: Suggestion on pedestrian access for downtown San Carlos

Hi Grace, I wanted to know if there was any consideration to closing a few blocks of Laurel beginning from San Carlos Ave, going south. If you have ever visited Santa Monica and seen the Third Street Promenade (picture below) it would be something like that. I believe this would be a real upgrade for the downtown area. Of course the roadways east and west of Laurel would have to be upgraded to accommodate additional traffic. This would be unique to the area making San Carlos downtown a destination for the Bay Area.





Courtney Wood <courtneywood@altaplanning.com>

RE: Group Bike Rides

2 messages

Grace Le <GLE@cityofsancarlos.org>

Wed, Feb 27, 2019 at 5:44 PM

Cc: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

[REDACTED] forwarded your email to me. No permit is required as long as there is no need for traffic control and riders obey traffic rules.

Please do inform us when you have a date and route for the ride. As you mentioned, this is a good opportunity for us to collect more information for the Bicycle Pedestrian Master Plan.

Regards,

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

[REDACTED]
Sent: Saturday, February 23, 2019 8:00 AM

Subject: Re: Group Bike Rides

Thank you for your very rapid reply! No concrete plans have yet been made, I wanted to understand the rules before doing anything else. To date, you are the only person to whom I've mentioned the idea of organized bike rides. The questions which I can answer right now are:

* There would be a small group of riders, less than a dozen.

* There is no need for traffic control.

* Riders would be together but would ride single file and behave as individual riders. For example, at a four way stop, each rider would wait their turn, alternating with crossing vehicles as usual. There would be no difference in behavior than if a group of unrelated riders happened to be together on the road by chance.

* I anticipate a number of such rides over the course of the development of the Bicycle and Pedestrian Master Plan, perhaps one a week.

What is the next step? Two alternative ideas spring to mind, but please suggest alternatives that work better for you.

1) The relevant departments (e.g. yours, Public Works, the Sheriff's) could provide general guidelines for group bike rides that do not require a permit, things like no need for city services (e.g. traffic control), and no disruption of traffic.

2) I can tentatively organize a ride, subject to approval. At that point I would have a date, time, and a route if necessary (though it would be better for the purposes of evaluating the Bicycle and Pedestrian Master Plan if we could keep the route flexible.) Hopefully, we could develop a system that is easy for everyone for handling future rides.

I believe that the specific rides I am suggesting will be helpful in the City's development of the Bicycle and Pedestrian Master Plan, and that more generally, the right kinds of group rides would provide ongoing benefit to the cyclists of San Carlos without undue burden on its citizens.

Let me know how you would like me to proceed.

[REDACTED]

[REDACTED]

[REDACTED]

Thank you for reaching out. We would need more details about what you have planned, such as the proposed route(s) for the bike rides, dates and times, and whether you would need any type of traffic control. Do you think the riders would be riding in a pack or spread out throughout the proposed route? assess along with other departments such as Public Works and the Sheriff's Office as to whether a

[REDACTED]

[REDACTED]

[REDACTED]

I am a San Carlos resident contacting you in reference to special event permits required by the City of San Carlos. I got your contact information from the city's webpage.

The webpage states:

"If you are considering holding a special event or organized community activity on public property, including parks and facilities, sidewalks and street areas that will change the typical use of that property, then you will need a special event permit."

Some in the community believe this means any organized bike ride on city streets requires a permit. Reading the above, I would think that a small organized bike ride which does not materially change the typical use of the streets on which it travels would not require a permit. The immediate example I have in mind would be in connection with the current ongoing development of a Bicycle and Pedestrian Master Plan by the city. I think it would be useful to organize bike rides by those of us participating in the process as citizens to ride routes under discussion to better understand the current situation and future needs. I would anticipate that there would be less than a dozen cyclists on any given ride and that we would not impede the normal use of the roads we used. (Of course all laws would be obeyed on these rides.)

What is your opinion on the matter? If I should direct this inquiry to someone else, I would be most grateful if you could point me in their direction.

THANKS!!



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Grace Le <GLE@cityofsancarlos.org>

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

Mon, Mar 4, 2019 at 3:47 PM

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Monday, March 4, 2019 3:42 PM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: Re: Group Bike Rides

Hi Grace,

The ride was both fun and thought provoking. There were four of us, which was a good number for facilitating discussion. We started at Elm and Cherry, took Elm south to Eaton, jogged over to the end of the Peninsula Bikeway on Warwick, then explored the best way to get from there to Cedar. In the end, we decided that the simplest route, west on Eaton to Cedar, was the best. We then took Cedar north, across San Carlos Avenue, to Magnolia. We explored various routes to the border with Belmont. We then went back to San Carlos Avenue and rode west to Cordilleras where we split up and each took our own route home. There was a lot of stopping and discussion along the way, so the whole thing took about 2 hours. One thing we decided is to make this a weekly event. Next week we are thinking of focusing on east San Carlos, crossing El Camino and Caltrain on Arroyo and then exploring Old County Road and East San Carlos Road among others.

What did we accomplish? We got to know each other, we laid the groundwork for future collaboration on a variety of ongoing city projects, and we each were able to expand and refine our ideas about biking in San Carlos. As we continue to attend the meetings the city is having about the new master plan, I think we will each show up at those meetings as better educated citizens. Not that it has any impact per se, but I thought it was interesting that we ended up with a shared opinion as to our favorite idea for how the interim Peninsula Bikeway might best be extended through San Carlos. We came into the ride with different ideas, but as we discussed them, we converged onto the following:

North on Warwick from the end of the current Bikeway in Redwood City to Eaton. West on Eaton to Cedar. North on Cedar to Arroyo. East on Arroyo across El Camino and under Caltrain to Old County. North on Old County to the border with Belmont at Belmont Creek. Except for a few blocks at the very beginning, this is

entirely existing bicycle infrastructure as is appropriate for this interim Bikeway, takes cyclists to locations of interest, and in particular, ends at Belmont at a point where it would be easy to extend north, and which is on the way to the excellent crossing to the east side of 101 at the pedestrian-bike overpass in Belmont near Ralston.

Thank you for your interest, and please let me know if you have any questions or suggestions.

All the best,



On Mon, Mar 4, 2019 at 9:42 AM Grace Le <GLe@cityofsancarlos.org> wrote:



Thanks for letting me know. How was the bike ride and what route was taken?

Regards,

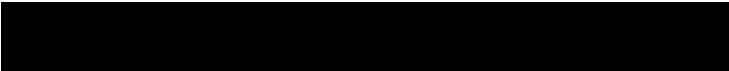
Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201



Sent: Friday, March 1, 2019 8:06 AM

To: Grace Le <GLe@cityofsancarlos.org>

Subject: Re: Group Bike Rides

Grace,

Here is the announcement:

"Join us for a social bike ride to survey some of the roads being discussed in the City of San Carlos' Bicycle and Pedestrian Master Plan. The ride is scheduled for this Sunday, March 3rd. We will assemble at the parking lot of the Library at the corner of Elm and Cherry at 9:15 am to discuss the route and will depart at 9:30 am. At the end of the ride, we can go to the Farmer's Market on Laurel Street. During the ride, we can discuss if we'd like these social rides to be a regular event. This ride will not be at all strenuous and we will all stick together. Riders should be comfortable riding with traffic on the streets of San Carlos."

It was posted on the San Carlos Bikes Facebook page:

https://www.facebook.com/pg/San-Carlos-Bikes-178816082826394/posts/?ref=page_internal

Please feel free to redistribute it if you want.

On Thu, Feb 28, 2019 at 10:41 AM Grace Le <GLe@cityofsancarlos.org> wrote:

Thanks for your input. We've also heard other residents express their interest in having San Carlos participate in Peninsula Bikeway. We'll definitely look into it.

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Thursday, February 28, 2019 10:02 AM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: Re: Group Bike Rides

Hi Grace,

At the February 21 Bike Plan meeting, my memory is that you announced yourself as the best contact for Bike and Pedestrian Plan related questions. Based on that, I am changing the subject to raise a different Bike-related issue, and that is the Peninsula Bikeway (<http://peninsulabikeway.com/>). In response to an announcement from San Carlos Bikes (<https://sancarlosbikes.org/>), I participated in the launch of the interim Peninsula Bikeway last September. This interim bikeway goes from the border between San Carlos and Redwood city on Warwick street to the southern border of Mountain View. An obvious question is if that bikeway could be extended north to include San Carlos, to the border between San Carlos and Belmont? It would appear that the cost of doing so might be reasonable in that all that appears to be involved is some additional signs. At the meeting, I asked this question of the contractors, and they seemed favorably inclined, but in retrospect thought it might be better addressed to the city, e.g. you. I don't know enough to have an opinion in the matter, but wonder what the city's thoughts might be.

Thank you for your help!

[Quoted text hidden]

[Quoted text hidden]

[Quoted text hidden]



Courtney Wood <courtneywood@altaplanning.com>

FW: Updated Peninsula Bikeway Route(s) & Monday Bike-About info

1 message

Grace Le <GLE@cityofsancarlos.org>

Mon, Oct 29, 2018 at 10:28 AM

To: Hugh Louch <hughlouch@altaplanning.com>, Courtney Wood <courtneywood@altaplanning.com>

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Saturday, October 27, 2018 2:57 PM**Cc:** Grace Le <GLE@cityofsancarlos.org>**Subject:** FW: Updated Peninsula Bikeway Route(s) & Monday Bike-About info

First a question – Can I borrow a big map of San Carlos, to take to the San Carlos Bikes “Bike About” event Monday evening? And is there a map of any size that shows the current City-designated bike routes? The map in the 2012 Bicycle Transportation Plan is illegible.

Second, there are some good suggestions below for suggested Peninsula Bikeway routes and videos showing bike-friendly Dutch intersection designs. Let’s keep these in mind for the upcoming Bike/Ped Master Plan.

Please let me know if a map is available. Thank you.

Sent: Friday, October 26, 2018 11:50 PM**Subject:** Updated Peninsula Bikeway Route(s) & Monday Bike-About info

Hello All,

I biked the routes I had proposed. Taking Cedar north all the way to Magnolia, and then winding (up) to Laurel Street & E Street we found not ideal.

It’s a little hilly, but mostly it doesn’t lend itself to crossing El Camino very safely, nor does it link up well to San Carlos Caltrain station. Elm is good but very narrow, so we found Cedar still better because it’s wider, already designated as a bike route with the sharrows, goes by two schools (White Oaks and Cedar), and gets close to two more (St Charles and Brittan Acres).

[Peninsula Bikeway Suggested Route\(s\)](#)

suggestion is that our part of the **Peninsula Bikeway** that starts at Warwick go west on Eaton, north on Cedar to Arroyo, and then forks to two options: The **Downtown/Train Station** option goes east on Arroyo, crosses El Camino, goes through the nice wide tunnel under the tracks, and drops right onto Old County Road, where it goes past the train station and north to Belmont. The **North San Carlos** option continues on Cedar St. all the way to Magnolia, heads east through the roundabout, to Laurel Street, follows Laurel north all the way to Hull, then turns east on Hull to cross El Camino there. From there I'd suggest bikes and pedestrians share the *sidewalk* on the east side of El Camino for 1 block (along the plaza between My Gym and CVS), to hang a right just past CVS where there is another tunnel underneath the Caltrain, which then also drops a biker back on to Old County Road. Either option takes a biker roughly equidistantly to Old County Road, however the North San Carlos is probably better for more experienced bikers because it has a stretch on Laurel that combines a narrowing of the street on a hill with poor visibility and densely parked cars on both sides of street. (Traffic Commission probably should flag that street for future safety consideration regardless.)

Proposed Maps of Peninsula Bikeway—the **North San Carlos** option: (<https://goo.gl/maps/FP4aCGs8RKy>), **Downtown/Train Station** option: (<https://goo.gl/maps/6JkNxxhEKS12>)

East-West adjoining Arterials

East-west Arterials to improve access to downtown and train station for north San Carlos, (and in particular to keep as many bikers as possible off of San Carlos Ave since a right-side bike lane and the right car turn-lane onto El Camino are in pretty significant conflict), we suggest include:

- Cherry Street, between Cedar and El Camino Real
- Arroyo and/or Belle between Cedar and Tamarack (for Brittan Acres and St. Charles access)
- Other suggestions??

Improved Intersection Safety

Tentatively identified key intersections that would benefit from the following video examples of Dutch awesomeness, (as shared by local biker extraordinaire with great ideas

- Arroyo @ El Camino Real
- Cherry @ El Camino Real
- Hull @ El Camino Real

Video 1) Bikes stop ahead of cars for better visibility and separated crossing lights stagger traffic (<https://vimeo.com/177962114>), and

Video 2) Intersection medians carve out paths for safer right AND left turns (<https://www.youtube.com/watch?v=FIAbxLz6pA>)

Pedestrian Zone Downtown

yet another biker extraordinaire in San Carlos with excellent ideas for sensible bicycle infrastructure in our fair town. He spoke about the idea of a 'Superblock', a downtown Pedestrian Zone that is blocked off so a) either *only* pedestrians and non-motorized wheels are allowed, or b) cars move one-way, racetrack-style up Laurel and down Walnut, with plenty of space for bike & human-powered wheels lanes. Personally I would like to see those two blocks between Arroyo and Cherry totally closed to non-human powered traffic, meaning the Superblock would be flanked on both sides by streets with safe biking infrastructure.

Planning for Monday, 5pm Bike-About

Current plan for Monday's Bike-About is still to ride along Elm, up Eaton, and down Cedar back to Burton Park for the kid-friendly portion. Kids can ride on the sidewalk next to us if traffic is really bad. The second portion will head east on Arroyo, cross El Camino and check out the tunnel, though we won't plan to bike on Old County Road because I agree with it's just not safe enough currently for a big group at rush hour. We'll probably spend some time at that intersection and then maybe bike along the sidewalk on the east side of El Camino to Cherry to also look at that intersection, before we head back to Arroyo and back to Burton Park.

I have lots of glow in the dark stickers, so encourage your friends with kids, even if they don't want to do the Bike about, to meet us at 6pm for pizza and glow in the dark in the park (and make sure they RSVP on Facebook or Nextdoor or [Meetup.com](https://www.meetup.com) or with an email to sancarlosbikes@gmail.com)

Finally, I'd like to have a large map of San Carlos, ideally with the current state of bike lanes, on hand when we discuss on Monday evening. I guess I is there a map like that already? I remember the old one was nearly impossible to read. If not, I might try to get a google.map with existing bicycle routes printed.

The Future of San Carlos is BIKING—who wants to open a Bike Shop?

If someone knows anyone who wants to open a bike store in San Carlos, that could carry electric bikes (for our hill-challenged neighbors), cargo bikes, kid, grownup & family biking setups, plus trailers for hauling groceries and farmers market goodies, I think it could do a brisk business. Currently San Carlos' only bike shop is REI, and Beeline Bikes which is repair only.

Thanks all! Suggestions welcome. My specialty is uncommon sense, not common sense, so I often miss obvious things—please don't hesitate to suggest them or correct me. Also feel free to share this email/info with anyone you know who cares and wants to be involved, whether for or against this project, ALL input is valuable.

Thanks,

Caltrain and SamTrans: Comments and San Carlos Bike/Ped Plan

Bikes

- Generally, Caltrain is looking to triple ridership in the next 20 years and is supportive of safety improvements that encourage people to walk and bike to San Carlos Station.
- Conversion of on street car parking into space for high quality bicycle facilities leading to the train station, especially on Old County Road, should lead to many more people arriving by bike.
- Concept C is the preferred choice for San Carlos Avenue to maximize the number of people coming to the station by bike.
- It would be ideal to coordinate with Belmont and Redwood City so routes on El Camino and Old County Road provide users with a high quality, consistent design without gaps.
- With any road diet, it's important to keep in mind that our standard buses are 10.5 feet wide mirror to mirror. For 10' and 11' road widths, a bus will occasionally intrude into adjacent lanes. Buffers between mixed-traffic lane and the bike or parking lane (such as in the proposed bikeway on ECR) is ideal.
- If two-way facilities on one side of the street are proposed, special consideration should be given to how people access the facility, especially where it transitions to bike lanes on each side of the street (see Shoreline/La Marina intersection in Santa Barbara for an example)



- If there is only room for a bike lane in one direction of a flat north-south street, a bike lane in the northbound direction should be prioritized due to wind.
- A Cherry Street bike facility should address crossing improvements at El Camino.
- Lighting improvement on San Carlos Avenue east of the tracks would make the street much more inviting to use at night.
- Caltrain is in the process of greatly increasing the amount of secure bike parking at San Carlos Station and system-wide.

- Consider bus boarding islands where bike lanes and cycle tracks are proposed on streets with bus routes (see example in San Francisco at North Point/Polk). [NACTO has some good design considerations.](#)



Pedestrians

- There is general support for shortening pedestrian crossing distances, tightening turn radii (as long as bus turns are accommodated), and other improvements to increase pedestrian safety and comfort. Turn radii for our 40' and 60' buses: <https://nacto.org/publication/transit-street-design-guide/intersections/transit-route-turns/turn-radii/>
- Bus bulbs are supported since they shorten crossing distances for pedestrians and reduce delays with buses reentering a traffic lane.
- There is general support for intersection improvements along El Camino but more information is needed to comment on intersection specific improvements.
- Several employees at 1250 San Carlos Avenue have had close calls at the San Carlos Ave/Laurel intersection. Implementing some of the bike designs should help address concerns at that intersection.
 - We would also like to see pedestrian crossing improvements at this intersection. See letter below (we drafted this back in the spring but I'm not sure if it ever made it into the hands of San Carlos staff).

General

- More information about the shared path over 101 would be helpful but Caltrain is generally supportive of the idea.

Grace Le, City Engineer
City of San Carlos
600 Elm Street
San Carlos, CA 94070

Dear Ms. Le:

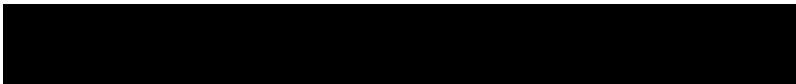
The purpose of this letter is to recommend that the San Carlos Bicycle and Pedestrian Master Plan prioritize pedestrian safety improvements at the San Carlos Ave and Laurel St intersection. The SamTrans central office is located at the northeast corner of this intersection and has over 250 full-time employees. SamTrans employees cross this intersection multiple times every day to access restaurants on Laurel St, the Caltrain station on San Carlos Ave, among other local destinations. Many SamTrans employees have witnessed or been involved in collisions and near-misses at this intersection. It is particularly dangerous because cars yield on left-turns and high traffic volumes incite cars to make aggressive left turns when there is a gap in oncoming traffic. However, the oncoming traffic often blocks the driver's view of the crosswalk, putting pedestrians at risk.

According to police reports from 2011 to 2017, nine collisions have occurred at the San Carlos Ave and Laurel St intersection. Five of these collisions involved a vehicle hitting a pedestrian, four in which the vehicle was making a left turn.¹ There are likely many unreported collisions and countless near-misses at this intersection.

It is evident that the yielding left turn at this intersection is dangerous for all roadway users and it is unacceptable for injuries to continue to occur. Collisions at this intersection are preventable through pedestrian-oriented roadway design and facilities. Potential improvements include designated left turn lanes and signals, high-visibility crosswalks, automatic pedestrian detection (rather than push button activated), a pedestrian scramble, leading pedestrian intervals, and left turn phasing.

SamTrans strongly recommends that the San Carlos Bicycle and Pedestrian Master Plan prioritize this intersection for pedestrian safety improvements. Please reach out if you have any questions.

Sincerely,



¹ Transportation Injury Mapping System (TIMS), *Safe Transportation Research and Education Center, University of California, Berkeley*. 2019.

EDAC Meeting Notes

1. Look at adding additional pedestrian improvements in the Industrial Arts District
2. Bike improvements are needed on Arroyo near the schools
3. Concern regarding changing to parallel parking in downtown – how many spots would be removed?
4. Dedicated bike lanes on San Carlos Avenue near the schools would be the only option for students near 4 corners

FW: Postman's path (Proposed Bike and Pedestrian Master Plan)

2 messages

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

██████████ thank you for the info in your email reply. We are happy about the overall Bike and Pedestrian Master Plan for San Carlos. However, we only learned today that the proposed plan includes a short section of an abandoned, closed path from Carmelita that previously proved very dangerous, especially to children from Central. We are very concerned about the foreseeable negative impacts if the City were to open this particular section of the proposed path. We will attend future meetings and respectfully request that you forward our email to the 2+2 committee and the City Counsel.

██████████ who are San Carlos residents who own the property on a portion of the path near Orange and Cedar. The area has long been fenced by the property owners and the path is no longer accessible. She and ██████████ both told us that they lived there when the path was closed for all the reasons that ██████████ mentioned in their emails. They also told us that they (or their parents) and other property owners agreed to allow the City to install a path across their properties for the specific purpose of allowing mail carriers to shorten their route. They said that some property owners granted the City a written easement, but others only gave the City verbal permission to install the path. Unfortunately, the path later became a popular place for criminal activity. ██████████ personally witnessed multiple incidents of disturbing, sometimes criminal behavior and they led the effort to close the path due to threats to health and public safety. Their main concern was the danger posed to Central students and other children who used the path. According to ██████████ she was present at the meeting when the San Carlos City Counsel voted unanimously to close the path after a number of alarming and dangerous incidents.

We join them in strongly opposing opening this abandoned path for the following reasons:

(1) Negative Impact on public health and safety. The path runs through multiple people's yards where there are numerous trees creating a shaded "tunnel" where people, especially children will be vulnerable to criminal behavior. This is effectively a blind alley that is inaccessible to patrol cars and other fire safety vehicles. It is foreseeable that people will walk through this area and discard cigarettes, other smoking products or vaping devices that could cause a serious fire. It is our understanding from long-term San Carlos residents who have lived along the path for many years that there were a couple of cases of arson (and small fires) along the path and that was one of the many reasons it was closed. They also reported personally witnessing assaults, fights, vandalism, drug use, sexual activity and public lewdness. Its' close proximity to a school invites children to congregate in an area that is unsupervised, dark and inaccessible to police and emergency vehicles. In addition, it borders the Pulgas creek and other properties with very steep banks that could lead to falls. This will expose the City to unnecessary liability and legal costs.

(2) Negative Impact on Riparian and wildlife habitat. This path borders the Pulgas Creek. Some of us whose properties border the creek sometimes observe people, especially children and teens, walking in the creek and we have to remove discarded water bottles, cans and other trash they leave behind on a regular basis. Opening this path so close to the creek will invite more trespass into the creek and onto private property and increase the amount of trash that property owners are legally obligated to remove. The City should not be able to take advantage of property owners' legal obligation to keep the creek clean by allowing public access to an area that directly increases that burden.

We will formally request that the City of San Carlos, the Department of Fish and Wildlife and other federal, county and state agencies conduct an impact study to determine what effects a highly trafficked bike and pedestrian path in a riparian corridor will have on local flora and fauna. This area is one of the few non-trafficked areas in Brittan Acres that is accessible to a large herd of deer and other wildlife. There are at least 12 deer in this particular herd that use this area for breeding and access to the creek for water. Most of us have photos and videos of the deer and will be happy to send them as proof of their frequent and uninterrupted year round usage and their use of the area for birthing fawns.

(3) The City has not used this path for many years and to the extent that it obtained legal easements from the multiple property owners at that time, it appears to have abandoned the easements. Based on our conversations with several property owners along the path, they contend that the City **does not have legal easements** across all the properties that will allow it to use the path for this new and expanded purpose. Much of the former path no longer exists in many places. Several owners have fenced in their adjoining yards (on their property) to minimize trespass and vandalism. It is also our understanding that some property owners never granted the City a formal written easement. (We were told that they, or their attorneys, intend to object to the path on those grounds.) In addition, even if the City's easements are determined to be legal after the appropriate judicial proceedings, the easements were granted with the understanding that the intended use was a path specifically for mail carriers. This proposed new use was not contemplated by the property owners who may have granted legal easement rights at that time. We request that the City provide documentation that establishes the legality of its' alleged easements across the multiple properties along the length of the path.

(4) The cost of installing this small section of a bike and pedestrian path, including lighting and patrolling it for public safety is not the highest and best use of taxpayer funds. It will be even more expensive if the City intends to fence in the path to prevent trespass onto the neighboring properties and direct access to the creek. In addition, some property owners will have to take down their existing fences and install new ones. It is frustrating that the City is considering this project when it has refused to improve the storm drainage on this end of the block on Carmelita despite years of letters, emails and meetings. Several of us continue to have to use sandbags and other barriers to keep our yards and garages from flooding during the rainy season. We constantly have to replace the sandbags when heavy vehicle traffic runs over them. Increasing the traffic on this block will make this issue even worse. It is also frustrating that we all pay a separate tax specifically for storm drains, yet San Carlos does not have a dedicated budget for storm drain repair. We formally request that the City consider allocating scarce resources to maintaining storm drains, Pulgas creek and other San Carlos waterways instead of investing so much time, money and resources into such a small section of the overall bike and pedestrian master plan.

(5) Opening this section of the path to the public greatly increases liability for the City and adjoining property owners. As indicated earlier, the City should expect litigation concerning the legality of its' purported easements across multiple parcels of property. Our email replies provide the City with formal notice of the prior criminal and civil complaints that arose when the City opened the path to the public. City records should demonstrate that the City Counsel voted unanimously to close the path based on those complaints years ago. This will increase the City's liability if it proceeds with plans to open this section of the path despite having actual notice of what transpired before. A repeat of those types of criminal behavior is not only foreseeable, but may be more likely than it was years ago.

[REDACTED] thank you for considering our concerns and for forwarding our reply to the appropriate City and Planning representatives.

[REDACTED]

p.s.

Neighbors, will you please forward this email to any other neighbors who may be interested, especially those who own the properties that the the path crosses. We appreciate everyone's input! Thank you!

[REDACTED]

Thank you for your email.

There will be a Joint City/District 2+2 Committee meeting scheduled on Friday, November 1, 2019 at 8:30 a.m. in the Enterprise Room, 2nd Floor of City Hall. The 2+2 Committee is comprised of two City Council Members and two School Board Members who discuss common issues

between the agencies and make recommendation to their respective Council/Boards members on matters from this Committee.

At the November 1, 2019 meeting, there is a New Business Item – Prepare a Walk Audit for SCSD Schools and Analyze Feasibility of Restoration of Postman Paths. This item is for discussion and it is intended to discuss the merits of the project. If the 2+2 Committee decides that it wishes to pursue this project, they will recommend to the full City Council to consider the project at a future Council meeting. As of now, Staff has not been directed or is proceeding to move this project forward.

I am happy to share your concerns and your email with the 2+2 Committee and you and your neighbors always welcome to attend the meeting to share your thoughts and concerns.

Please let me know if you have any questions.

[REDACTED]

Sent: Thursday, October 24, 2019 3:08 PM

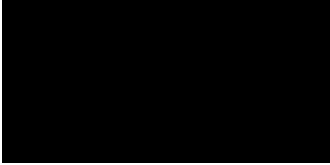
[REDACTED]

Subject: Postman's path

Dear San Carlos Planning Department,

We recently heard that the city is thinking about reopening postman's path (from the end of Carmelita to Orange Ave). We live at [REDACTED] surprised to learn this, and even more surprised that the homeowners living along and near the path have not been consulted or even contacted. There are many good reasons why the path was closed, and we are surprised the city would consider re-opening it, especially considering the potential liability to the city. There was a homeless person ultimately living in the path. Since the path is long and not lighted, and almost certainly there will be fences built along both sides, it will be a long, dark tunnel. This is the way it was when it was closed, and it became a gathering spot for children (middle school and high school) who would gather in the middle of the path/tunnel to party, and middle schoolers would plan fist fights there. We continue to find broken beer bottles after winter storms. Our

neighbor who lived here at that time (and still does) said the cul-de-sac at the end of Carmelita became a thoroughfare with a constant line of cars dropping kids hool. We strongly oppose this path reopening. Would someone contact the neighbors on this end of Carmelita as we should have ars will cause a hazard to those living on this street.



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Brett Hondorp <bhondorp@altaplanning.com>
To: Grace Le <GLE@cityofsancarlos.org>, Courtney Wood <courtneywood@altaplanning.com>

Fri, Oct 25, 2019 at 10:28 AM

Thanks for the update. My take is this is a planning study and identifying any particular path alignment does not require the city to construct a facility, it simply identifies a potential corridor and gives the city options to study it in the future. Any future consideration of a path would require additional study and outreach including any ROW, engineering, environmental and safety impacts.

[Quoted text hidden]



Courtney Wood <courtneywood@altaplanning.com>

FW: Bike and Pedestrian Master Plan (Opposition to Opening Postman's Path from Carmelita to Orange) Transportation and Circulation Meeting on November 19th

1 message

Grace Le <GLE@cityofsancarlos.org>

Fri, Nov 15, 2019 at 3:23 PM

To: Courtney Wood <courtneywood@altaplanning.com>, Brett Hondorp <bhondorp@altaplanning.com>

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Friday, November 15, 2019 3:23 PM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: FW: Bike and Pedestrian Master Plan (Opposition to Opening Postman's Path from Carmelita to Orange) Transportation and Circulation Meeting on November 19th

Hi Grace,

d the neighbor may attend the T&C meeting next

[REDACTED]

[REDACTED]

November 15, 2019

[REDACTED]

Members of the Transportation and Circulation Commission,

[REDACTED] and are writing in advance of the upcoming Transportation and Circulation Commission Meeting on Tuesday, November 19th. We strongly oppose re-opening the abandoned Postman's path between Carmelita and Orange. We have lived on Carmelita since 1959. The path was closed in the late 80s or early 90s due to many problems. Here are some of the problems we see with opening the path:

--Caused unsafe traffic--cars and motorcycles would turn down this street thinking that the path crossed through. There would be a line of cars trying to turn around and some even drove onto the path and got stuck in the brush and had a hard time getting out. When the SCPD later put up posts to block the path, motorcycles drove around them. We think this will happen again if opened.

--Caused unsafe conditions for people who used it. There were a lot of fights and people rode very fast on bikes, sometimes running into walkers. We never let our kids use the path alone. If fenced in like it was years ago, it would be a long, concealed area where people would be vulnerable.

--Caused littering and trash accumulation. People dropped a lot of trash along the path, into the creek and over the fences, especially beer cans, glass and cigarette butts.

--Was a fire safety risk. The grass, brush and trees along the path are are very dry and if set on fire by cigarettes or other things, that would threaten the neighboring houses. Would also be hard for fire department to get their engines back there to put it out. Also, one of my neighbors mentioned in their email that a PG&E line that ran under the creek was uprooted by trees in a storm in 2012. That caused the gas line to break and it took hours for them to stop it. One of the trees also fell on my property and damaged a shed and my fence at the same time.

--There is a large herd of deer who congregate near the opening of the old, abandoned path and they can be very aggressive when people approach them. I have had problems with the deer jumping into my yard over my fence and I can't get them out without help from my neighbors. When they jump the fences they often break a leg. I have had to call the Humane Society on a couple of occasions to come and help

them. If you open the path, they could be hurt even more when they try to jump over the fence when people are present. We have also seen the deer attack people's dogs when they are walking them. They are especially aggressive when they have fawns.

--This street floods and I have to use sandbags in front of my house next to the storm drain to keep water from flooding my garage during the winter--even in drought years. Public Works is well aware of the flooding on this street. We've been asking them to fix the street gutters and storm drain for years.

It took a long time for us to get the City to close this path when they did. It makes no sense to re-open it after all those problems. It is also unsafe to direct more pedestrian, bike and car traffic down a street with all these documented problems.



FW: San Carlos Bikes Focus Group Feedback

1 message

Grace Le <GLE@cityofsancarlos.org>

Tue, Oct 15, 2019 at 11:07 AM

To: Courtney Wood <courtneywood@altaplanning.com>, Brett Hondorp <bhondorp@altaplanning.com>

FYI

Grace Le, PE

City Engineer

City of San Carlos

600 Elm Street, San Carlos, CA 94070

(650) 802-4201

Sent: Tuesday, October 15, 2019 7:03 AM

To: Grace Le <GLE@cityofsancarlos.org>

Subject: San Carlos Bikes Focus Group Feedback

Dear Grace,

We are excited about the vision that the plan from Alta lays out, grateful that you sought out San Carlos Bikes' input, and we have laid out the following considerations and additions we think would make the plan more inclusive and comprehensive:

Prioritizing safe routes to schools

- A San Carlos School District Safe Routes to School survey from within the past year received 441 respondents, of which 50% of those who responded were concerned parents/guardians of children at Tierra Linda and Arundel, arguably the least bike/ped-friendly areas for school children of our town, due to hills and traffic. (The survey included SIXTEEN pages (small print!) of bike/ped-related safety concerns, which **San Carlos Bikes is examining, and will summarize those concerns in a separate, detailed school-specific safe routes letter.**)
- We are very glad to see San Carlos Ave being prioritized, but one concern is that the **feeder streets bringing school children to our northern/western-located schools do not indicate much planned bike/ped improvements**. Unless we provide ways for students to come safely/efficiently down from the hills, whether that's biking, bussing, walking or a combination, parents will *still* end up driving their children to San Carlos Ave, which is better, but might not end up decreasing traffic congestion on San Carlos Ave adequately.
- **Showing elementary school attendance boundaries** would be really valuable to improve people's reference frames when looking at the map. If adding them might unduly clutter the main "proposed bikeways" map, Alta and staff might decide to show school boundaries on a separate map.
- Given the high priority of safe routes to school in our community as a huge driving force (no pun intended) in the desire for safer, less-congested streets, we'd like to **see the high schools our San Carlos students attend daily indicated on the map**: Sequoia High, Carlmont High, Design Tech High, Summit Prep, Junipero Serra High, etc. This allows people to more concretely see which routes would get them there most safely according to the Plan. Prioritizing efficient and safe routes for children to get to school as *independently* as possible is going to be key to catalyzing San Carlos's transition to a more 'bikes/wheel-friendly' riding culture than it currently is.
- Similarly, we think the map ought to include all **preschools on the map** as well (perhaps on the school-boundary version) because they are also important safe routes-necessitating locations

- We would like to see an **additional east/west bike-friendly route, between Alameda and El Camino Real on the south side of San Carlos, either Eaton Ave or St Francis Blvd**, which is particularly important for students commuting back and forth to Sequoia High School

Inter-community connectedness

- Given the interconnectedness of San Carlos with Belmont, Redwood City (including Redwood Shores and Emerald Hills) and beyond, a **map of San Carlos Bikeways should we think include the existing and planned bikeway routes of our *neighboring* towns as well.** (For pedestrian access, a map that ends at San Carlos borders makes sense, but for bicycle access, a more detailed reflection of our interconnectedness seems crucial.)
- We printed the Blkeways map Alta provided out to 3' x 4' which would easily accommodate making the whole map larger to encompass our town AND our neighbors to the north (Belmont), south (Redwood City), east (Redwood Shores), and southwest (Emerald Hills). We would like to see it include street names but most specifically **help people see how our current and planned bikeways intersect with our neighbor cities' current and planned bikeways.**
- We would like to see exactly **where the Bike-Pedestrian Bridge is planned to be built over 101**, and how that will connect East San Carlos to the Bay Trail and Redwood Shores, as well as to Industrial Ave and East San Carlos Ave. (Is this a separate structure from the car traffic overpass, or is it planned to be attached to the overpass?)
- There are routes of considerable value to San Carlos bicyclists that lie just outside the city limits that we think should be included, for example:
 - Just northeast of the SC Airport, Skyway veers east toward Redwood Shores, where it meets up with the south end of Twin Dolphin Drive (albeit by going through the hotel parking lot.) This used to present a **safe, efficient way for cyclists traveling north from the Bay Trail to use Skyway to get onto Twin Dolphin Drive, not only NOT having to use the high-traffic Redwood Shores Parkway, but also not having to get into the left lane across traffic that just came off Highway 101.** Unfortunately however, a chain link fence/gate now blocks Skyway's connection to that Twin Dolphin Drive access. (It is right near the Sam Trans bus parking--why was that gated off?)
 - Especially with the coming Bike/Ped-accessible bridge, which should dramatically improve connection between East San Carlos and Redwood Shores, **putting a bike/pedestrian access opening in the gate** would make cycling to Oracle and that side of Belmont significantly safer and appealing enough to current drivers who will be happy to trade driving for efficient bike access.
 - Additionally, a feature that would make turning left from Redwood Shores Parkway onto Twin Dolphin Drive safer would be a **left turn "wait box" on the right side of the intersection** so that cyclists can wait safely to make their left turn.

This begs the question, to what extent does Alta Planning refer to the (San Mateo County) C/CAG county bike map that shows undesigned routes that bicycle users find useful (and presumably be provided to C/CAG during map development). Link: http://ccag.ca.gov/wp-content/uploads/2014/07/San-Mateo-County-Bike-Plan-Map_new.pdf

- What about the Strava 'heat maps' that show where the majority of Strava-using cyclists are currently riding? It would be really helpful to know that the neighbor-city linework took into account these undesigned but frequently-ridden segments.

Downtown

- San Carlos Bikes supports the improvements to downtown, in particular **wider sidewalks**. Can a pedestrian zone such as recommended center around only one block (like a 'superblock' between Cherry and Olive), while the rest of Laurel accommodates (slow) car traffic?
- Can Alta mock-up a "one-way couplet" option for Laurel and Walnut Streets? A couplet is a pair of streets, typically one block apart, each of which is one-way. In SF, Howard and Folsom function this way. This could allow a *mostly* pedestrian zone with one lane of (slow) car traffic + bike lanes going down it? Would rear/back-in angle parking work there?
- What kind of traffic calming strategies are planned, block by block, on Elm and Cedar? What kind of parking configurations?
- Reopen and pave pathway between east end of Carmelita Dr and Orange coming out on Cedar.

Business Patronage

- Regarding implementing a two-way cycle track on the west (train track) side of the street on Old County Road, what provisions would be incorporated to facilitate access to/from the cycle track's intersecting streets and also mid-block. Can a mid-block refuge be created in a segment of center-turn lane that would allow cyclist/wheel users to exit the cycle track and patronize the businesses along Old County Road? Can traffic lights be staggered, for bikes/peds separately from cars?

Steepness/Grade

- Indicating grade (steepness) seems particularly important for San Carlos Streets. Is this in the Bikeways Map and we didn't notice? The C/CAG county bikeways map uses 1, 2 or 3 red chevrons (" > ", " >> ", " >>> ") to indicate moderate to steep grades and direction of grade. This would be useful on the San Carlos map because it could help make the case for politically challenging street transformations on, say, steep routes to certain schools, by illustrating what kind of bike/ped/bus/car traffic a given route segment will need to support.
- Due to Belmont's hilly topography close to El Camino, extending San Carlos bikeways into Belmont directly is challenging, so one concept which would dramatically impact the efficiency with which San Carlos-based cyclists can get around would be to provide a short segment of two-way bikeway (cycle track?) along the west side of El Camino to close the gap between Hull Ave (San Carlos) and 5th Ave (Belmont). Could Alta Planning mock up a feasible connection from Hull Street in San Carlos, along the west side of El Camino Real for a couple blocks that would intersect back up with 5th Ave in Belmont?

Bike Parking

- Lack of bike parking is a significant deterrent to riding to downtown. Laurel Street already has large curb extensions at corners, however, many of them have massive planters. We would suggest that consideration be given to modifying some of the existing curb extensions, or reconsidering/rearranging the "street furniture" located on them (benches, planters, trash/recycle containers) to accommodate bike parking clusters.
- One of our San Carlos Bikes members has extensive experience in design and furnishing bike parking areas — both outdoor and inside buildings and parking garages — and has detailed background knowledge of national (APBP) Bicycle Parking Guidelines. **We would like to offer to do a "Walk-about" with City staff to brainstorm options along Laurel and close-by on intersecting streets.**

And finally, one observation of the recommended plans is that the most desirable layouts for bike & wheel + pedestrian accommodations that are recommended tend to cut a lot of street parking. Our city needs to make an *intentional/strategic* move to become less dependent on street parking for our cars, especially in residential neighborhoods. Street parking is a) highly theft-prone, which takes police time to investigate, and b) takes up precious 'moving vehicle real estate'.

What about inspiring a contest for home/landscape designers in/around San Carlos that encourages them to submit ideas and sketches for creative and innovative ways to design fronts of (especially smaller) homes that can accommodate two cars (or however many they have) *without* requiring street parking. Perhaps creative use of semi-circle driveways, with artful landscaping that screens the cars from the street, while accentuating the house would be encouraged. It was an important move to recently pass the approval for a wider apron in front of a single car garage space that allows two cars to park in a driveway with a one-car garage, but I think it's time to take it to the next step.

This is because short-sighted issues like 'curb appeal' and 'home resale value', don't consider the fact that climate projections put our region UNDER WATER in the not-that-distant future, so it makes sense to refocus the conversation about street parking altogether toward a more sustainable vision.

Thanks for considering our feedback,

School District Comments	
Comment	Response
San Carlos Ave. needs a buffered bike lane the entire way up to the Club/Dartmouth intersection.	Buffered bike lanes are proposed from Devonshire to Dartmouth
We need to prioritize plans to get students who live in the <u>Crestview</u> /Club/Beverly/Devonshire hills out of single occupancy vehicles and onto safer sidewalks and bike routes. Right now I don't see any opportunities presented on the current plan.	
We want to designate Old County road as A primary north/south bike route and create more underpass access to it. Similar to Arroyo underpass. We especially need underpass access further south near Edgewood Drive so we can get the students who most need to be biking independently (high schoolers) to and from Sequoia High School.	
We like the Howard improvements near Laurel/El Camino and suggest that bike/ped traffic signals are specifically timed to allow bikes/peds through the intersection before cars. I don't know the technical term for that type of staggered signal, but the idea is to give bikes and pedestrians time to get cross El Camino before cars are allowed to move. There would have to be 'No right on red' while bikes/pedestrians are moving.	
It's difficult to understand the options for Cedar Ave. improvements but that is most certainly our most travelled school route and we'd like to see options for limiting vehicle traffic during the 7:30-8:30am and 3:00-3:45pm hours.	
One option to consider at Arroyo and Cedar is limiting the cars coming east on Arroyo in the morning. Perhaps a blockade (a la Bryant St. in Palo Alto) that would force cars coming east on Arroyo to either turn right (south) on Orange, or left (north) on Orange instead of coming all the way to Cedar. I know that the city is still looking to fund the Arroyo/Cedar project that would square up that intersection, but in the meantime, we need to do more to make this intersection safer by reducing the number of vehicles.	

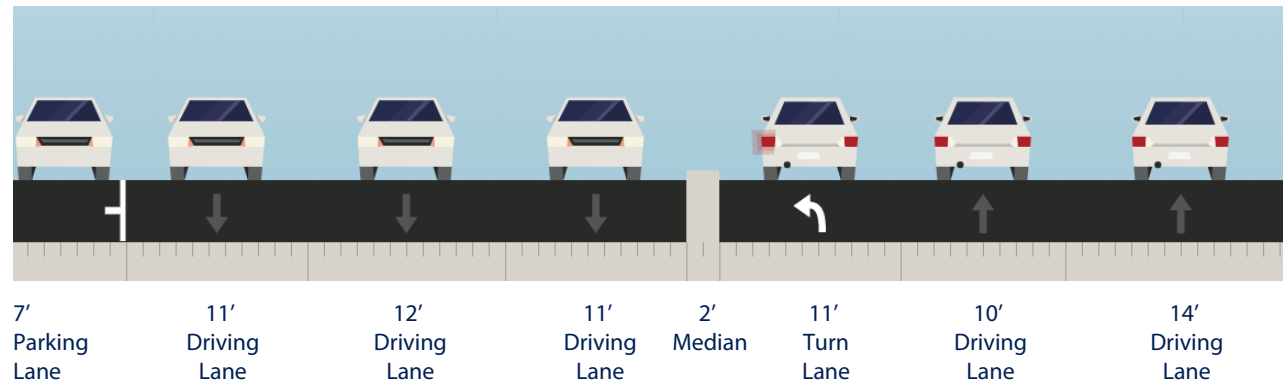
School District Comments	
Comment	Response
We recommend a buffered bike lane the entire distance of Alameda de las Pulgas	This is something we can consider. It would require removal of parking.
Brittan Ave. needs a sidewalk all the way to the top of Crestview.	Sidewalk gap closure is proposed for Brittan Ave.
Really great to see the proposed separated bike lanes on El Camino and Old County Road, that will connect nicely with planned improvements here in Redwood City. Additionally, I am glad to see the bike boulevard route along Chestnut Street to Warwick Avenue as I hear a lot of Sequoia students use that route when coming from San Carlos. For Clifford Elementary, we have a lot of families coming from the neighborhood south of Brittan Avenue and west of Alameda de las Pulgas, though those are fairly calm streets with sidewalks. While I don't see any improvements there aside from the crosswalk improvement on Alameda, I feel that those streets are calm enough that improvements aren't warranted. In the future we may do a walk/bike audit for Clifford, in which case I will definitely reach out to you.	No action

El Camino Real

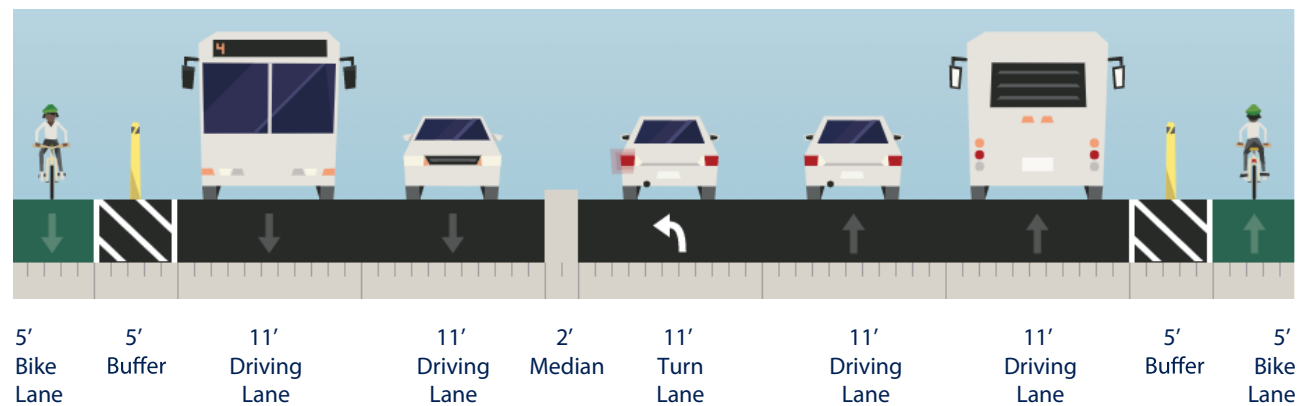
78' Width

El Camino Real, South of San Carlos Avenue

Existing Conditions



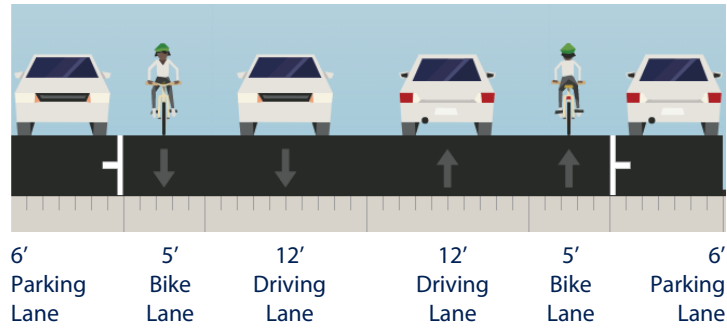
Proposed Separated Bikeway



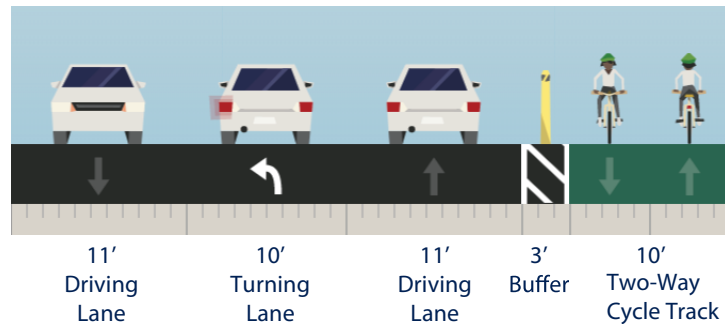
The proposed is only 77'. You could use that 1' to widen one of the lanes

Old County Road 45' Width

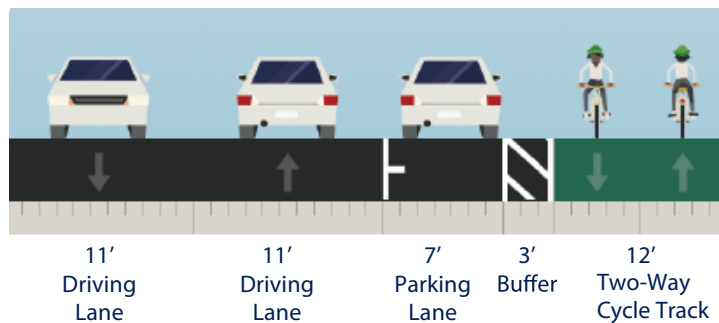
Existing Conditions



Concept A Separated Two-Way Cycle Track

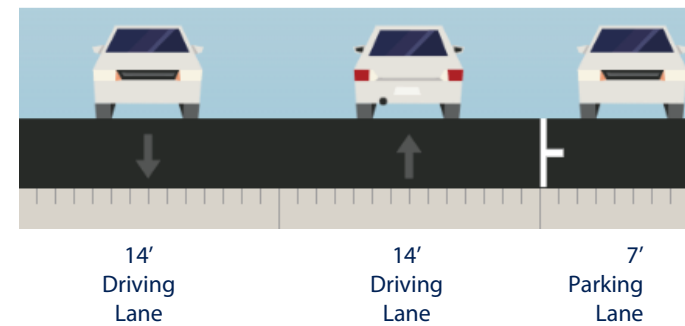


Concept B Protected Two-Way Cycle Track

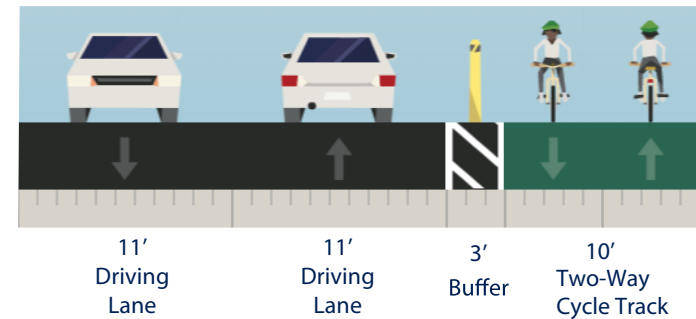


Old County Road 35' Width

Existing Conditions



Concept A Separated Two-Way Cycle Track



Where feasible, it might be good to have a wider parking lane on streets with speeds above 25mph (typical)

This option is only 44' in width

APPENDIX D

Prioritization

PRIORITIZATION





































Table D1: Bikeways

ROADWAY	START	END	CONNECTIVITY	SAFETY	COMFORT	ROUTE TO SCHOOL	PRIORITIZATION CATEGORY
Cherry St	Cedar St	El Camino Real	●	◐	●	●	High
Elm St	Magnolia Ave	Eaton Ave	◐	●	●	●	High
Arroyo Ave	Tamarack Ave	El Camino Real	◐	●	●	●	High
San Carlos Ave	Vista Ave	El Camino Real	●	●	●	○	High
San Carlos Ave	Club Dr	Wellington Dr	◐	◐	●	●	High
San Carlos Ave	Cranfield Ave	Club Dr	●	○	●	●	High
Cedar St	Eaton Ave	San Carlos City Limits	◐	◐	●	●	High
Old County Rd	San Carlos City Limits	San Carlos City Limits	◐	◐	●	●	High
Alley	Elm St	Laurel St	◐	◐	●	●	High
Saint Francis Way	Alameda de las Pulgas	El Camino Real	◐	◐	●	●	High
El Camino Real	San Carlos Limits	San Carlos Limits	●	●	◐	○	Medium
San Carlos Ave	El Camino Real	Old County Rd	◐	●	●	○	Medium

PRIORITIZATION









Alameda de las Pulgas	San Carlos Ave	San Carlos City Limits					Medium
Laurel St	San Carlos Ave	Cherry St					Medium
Laurel St Paseo	Laurel St	Walnut St					Medium
Crestview Dr	Brittan Ave	Club Dr					Medium
Eaton Ave	Alameda De Las Pulgas	El Camino Real					Medium
Howard Ave	El Camino Real	Industrial Rd					Medium
Howard Ave	Laurel St	El Camino Real					Medium
Howard Ave	Alameda De Las Pulgas	Laurel St					Medium
Industrial Rd	San Carlos City Limits	Bing Street					Medium
Brittan Ave	Cedar St	Elm					Low
Brittan Ave	Milano Wy	Crestview Dr					Low
Magnolia Ave/ Laurel St	Hull Dr	Cedar St					Low
Hull Dr	Cedar St	El Camino Real					Low

PRIORITIZATION

Melendy Dr	Alameda De Las Pulgas	Crestview Dr					Low
Club Drive	San Carlos Ave	Crestview Dr					Low
Pedestrian Pathway	Industrial St	Old County Rd					Low
Railroad Bed	Old County Rd	Industrial Rd					Low
Cordilleras Ave	Brittain Ave	San Carlos Ave					Low
Taylor Way	Old County Rd	Industrial Rd					Low
E San Carlos Ave	Industrial Rd	Holly St Overpass					Low
Brittan Ave	Milano Ave	Alameda de las Pulgas					Low
Commercial St	Old County Rd	Industrial Rd					Low

PRIORITIZATION

Table D2: Sidewalks

ROADWAY	START	END	CONNECTIVITY	SAFETY	COMFORT	ROUTE TO SCHOOL	PRIORITIZATION CATEGORY
San Carlos Ave	Upland Ave	Cordilleras Ave					Medium
Brittan Ave	Milano Way	Rogers Ave					Low

PRIORITIZATION
































Table D3: Pedestrian Improvements

ROADWAY	CROSS STREET A	CROSS STREET B	CONNECTIVITY	SAFETY	COMFORT	ROUTE TO SCHOOL	PRIORITIZATION CATEGORY
P14	Industrial Ave	Center St	●	●	●	●	High
P13	Industrial Ave	Howard Ave	●	◐	●	●	High
P7	El Camino Real	San Carlos Ave	●	●	●	○	High
P10	Brittan Ave	Cordilleras Ave	◐	◐	●	●	High
P21	El Camino Real	Belmont Ave	●	●	●	○	High
P33	Kenton Ave	San Carlos Ave	◐	◐	●	●	High
P5	Arroyo Ave	Chestnut St	●	◐	○	●	High
P6	El Camino Real	Hull Dr	●	◐	●	○	High
P8	El Camino Real	Arroyo Ave	●	◐	●	○	High
P9	El Camino Real	Brittan Ave	●	◐	●	○	High
P12	Brittan Ave	Crestview Dr	●	◐	●	○	High
P19	Holly Ave	El Camino Real	●	◐	●	○	High

PRIORITIZATION

P27	San Carlos Ave	Laurel St	●	◐	●	○	High
P28	Alameda De Las Pulgas	Howard Ave	●	◐	●	○	High
P36	Olive Ave	Elm St	●	◐	○	●	High
P4	Alameda de las Pulgas	St Francis Wy	●	○	●	○	Medium
P16	El Camino Real	Cherry St	●	○	●	○	Medium
P17	El Camino Real	White Oak Way	●	○	●	○	Medium
P18	Holly Ave	Old County Rd	◐	◐	●	○	Medium
P22	Cedar St	Arroyo Ave	●	○	○	●	Medium
P29	Alameda De Las Pulgas	Carmelita Dr	◐	◐	●	○	Medium
P30	Chestnut St	Cherry St	●	○	○	●	Medium
P31	Orange Ave	Arroyo Ave	●	○	○	●	Medium
P32	Cordilleras Ave	Arroyo Ave	●	○	○	●	Medium
P34	Brittain Ave	Trail Connecting Parks	●	○	●	○	Medium

PRIORITIZATION

P35	Brittain Ave	Greenwood Ave					Medium
P37	Cherry St	Cedar St					Medium
P11	Brittan Ave	Woodland Ave					Low
P25	Cordilleras Ave	Belle Ave					Low
P15	Magnolia Ave	Elm St					Low
P23	Woodland Ave	Chestnut St					Low
P24	Tamarack Ave	Belle Ave					Low
P20	Holly Ave	Laurel St					Low

PRIORITIZATION